

AGENDA FOR

PLANNING CONTROL COMMITTEE

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To: All Members of Planning Control Committee

Councillors : J Black (Chair), A Matthews, Y Wright, R Skillen, C Preston, E O'Brien, J Harris, M D'Albert, R Caserta, T Cummings, S Haroon, S Kerrison and I Schofield

Dear Member/Colleague

Planning Control Committee

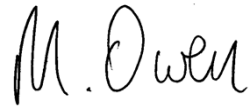
You are invited to attend a meeting of the Planning Control Committee which will be held as follows:-

Date:	Tuesday, 28 February 2017
Place:	Peel Room, Bury Town Hall
Time:	7.00 pm
Briefing Facilities:	<p>If Opposition Members and Co-opted Members require briefing on any particular item on the Agenda, the appropriate Director/Senior Officer originating the related report should be contacted.</p> <p>The Head of Development Management will brief the Committee on any changes to be made to the Planning Applications since the issue of the Agenda. This information will be circulated to Members and made available to the public on the Council's Website on the day of the meeting.</p>
Notes:	<p>Food will be available from 5.00 pm – Lancaster Room. Pre-meeting Briefing/virtual Site Visits at 6.00 pm – Lancaster Room.</p> <p>Details of Site Visits/Member Training will be circulated separately for the information of Members and Officers.</p>

The Agenda and Reports for the meeting are attached.

The Agenda and Reports are available on the Council's Website at www.bury.gov.uk – Council and Democracy.

Yours sincerely

A handwritten signature in black ink, appearing to read 'M. Owen'.

MIKE OWEN
CHIEF EXECUTIVE

AGENDA

1 APOLOGIES FOR ABSENCE

2 DECLARATIONS OF INTEREST

Members of the Planning Control Committee are asked to consider whether they have an interest in any of the matters on the Agenda and, if so, to formally declare that interest.

3 MINUTES OF THE MEETING HELD ON 24 JANUARY, 2017 *(Pages 1 - 6)*

4 PLANNING APPLICATIONS *(Pages 7 - 182)*

a SUPPLEMENTARY AGENDA *(Pages 183 - 188)*

5 DELEGATED DECISIONS *(Pages 189 - 202)*

A report from the Head of Development Management on recent delegated Planning decisions since the last meeting of the Planning Control Committee

6 PLANNING APPEALS *(Pages 203 - 210)*

A report from the Head of Development Management on recent planning appeal decisions since the last meeting of the Planning Control Committee.

7 PLANNING OUTCOMES REPORT *(Pages 211 - 222)*

A report from the Head of Development Management providing a summary of visits undertaken and analysis provided by Members on the outcomes tour in August 2016.

8 URGENT BUSINESS

Any other business which by reason of special circumstances the Chair agrees may be considered as a matter of urgency.

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Minutes of: PLANNING CONTROL COMMITTEE

Date of Meeting: 24 January, 2017

Present: Councillor J Black (In the Chair)
Councillors R Caserta, A Cummings, M D'Albert,
S Haroon, J Harris, S Kerrison, E O'Brien, C
Preston, I Schofield, and Y Wright

Public attendance: 65 members of the public were in attendance

Apologies for absence: Councillors A Matthews and R Skillen

PCC.501 DECLARATIONS OF INTEREST

There were no declarations of interest made at the meeting.

PCC.502 MINUTES

Delegated decision:

That the Minutes of the meeting held on 20 December, 2016 be approved as a correct record and signed by the Chair.

PCC.503 PLANNING APPLICATIONS

A report from the Development Manager was submitted in relation to the applications for planning permission. Supplementary information was also submitted in respect of application numbers: 60045, 59412, 60708, 60784, 60836, 60929.

The Committee heard representations from applicants and/or objectors in respect of the applications submitted. This was limited to three minutes for each speaker.

Prior to the Committee meeting, Site Visits had taken place in relation to Planning Applications 60045 and 60708.

Councillors Silbiger and Quinn spoke as Ward Councillors in respect of Planning Application 60045. Councillor Tariq spoke as a Ward Councillor in respect of Planning Application 60732.

Delegated decisions:

That **Approval** be given to the following applications in accordance with the reasons put forward by the Head of Development Management in the report and supplementary information submitted and subject to the conditions included:

60527 Unit 5, Croft Lane Industrial Estate, Croft Lane, Bury – Whitefield & Unsworth – Unsworth Ward

Change of use from light industrial (Class B1) and general industrial (Class B2) to personal training studio (Class D2)

60567 Prestwich Heys A.F.C, Adie Moran Park, Sandgate Road, Whitefield, Manchester – Prestwich – Holyrood Ward

Variation of conditions 4, 6 & 11 of planning permission 59412

60688 29 Bolton Road, Bury – Bury West – Church Ward

Change of use from A1 (retail) to A5 (hot food takeaway) with addition of flue to rear elevation

The decision to Approve with Conditions this application is subject to the addition of the following Condition, agreed by the Committee at the meeting:-

Condition 6 : No development shall commence unless and until a scheme for the storage of bins and refuse has been submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be constructed and completed in accordance with approved plans prior to the commencement of the development and maintained thereafter.

Reason. No details of bin and refuse storage has been submitted to ensure adequate provision for the storage and disposal of refuse and customer litter pursuant to Policy S2/6 - Food and Drink.

60708 Former Ramsbottom Police Station, Bridge Street, Ramsbottom, Bury – Ramsbottom & Tottington – Ramsbottom Ward

Demolition of former police station and erection of building to include 2 no. shop units with 8 no. apartments above

60732 Hatfields, Manchester Road, Bury – Bury East – Redvales Ward

Demolition of showroom section of existing facility and erection of new showroom, refurbishment and extension of workshop to include new MOT and valet bays and amendments to site parking and display layouts

60784 41 Bury Old Road, Ainsworth, Bolton – Radcliffe North Ward

Change of use of stables to dwelling with single storey extension

60810 Land at Bolton Street/Holly Bank Street, Radcliffe, Manchester – Radcliffe West Ward

Erection of 9 no. garages

60836 7 Fairfax Road, Prestwich, Manchester – Prestwich – St Mary's Ward

Change of use from Citizens Advice Bureau (Class A2) to wine bar (Class A4) and alterations to front/rear elevations

The decision to Approve with Conditions this application is subject to an amendment to Condition 3, to read as follows:-

Condition 3: Before the Class A4 use hereby approved commences, a noise assessment, including sound insulation testing of the party walls in accordance with Regulation 41 (Sound Insulation Testing) of Building Regulations Approved Document E, shall be submitted to and approved in writing by the Local Planning Authority. Any acoustic treatment recommended in the assessment shall be implemented in full before the use commences.

Reason. To protect the amenities of the neighbouring sound sensitive use at No.5 Fairfax Road pursuant to UDP Policy EN7/2 Noise Pollution.

60878 Land adjacent to 12 Est Bank Road, Ramsbottom, Bury – Ramsbottom & Tottington – Ramsbottom Ward

Change of use of land for the extension of residential curtilage and erection of boundary fencing

60870 Land adjacent to 361 Bolton Road West, Ramsbottom, Bury - Ramsbottom & Tottington – Ramsbottom Ward

Change of use of land for the extension of residential curtilage and erection of boundary fencing

60929 478 Bury Old Road, Prestwich, Manchester – Prestwich – Holyrood Ward

Alterations to shop front, conversion from 1 no. unit to 2 no. units and change of use from retail (Class A1) to retail and hot food takeaway (Class A1 & A5) with flue at rear

60939 Land adjacent to 32 Nuttall Street, Bury – Bury East Ward

Erection of 1 no. attached dwelling

60957 Fairfield Primary School, Rochdale Old Road, Bury – Bury East Ward

Classroom extension and covered corridor; Replacement parking spaces at front

That the following Planning Application be **Deferred** for further discussions to take place between the Applicant and relevant Officers of the Council in relation to parking/highways issues :-

60045 Texaco Petrol Station, Bury Old Road, Prestwich, Manchester – Prestwich – Sedgley Ward

Demolition of existing structures and erection of 2 retail (A1) units and synagogue with associated car parking, landscaping and access arrangement

(Note: This planning application had an Officer recommendation - to Refuse the application. During consideration by the Committee, the meeting was adjourned for a short time following a proposal to defer the application to enable further discussion and investigation of the parking issues raised. The Applicant agreed to this proposal which was formally proposed by Councillor Caserta and seconded by Councillor O'Brien. The Committee voted unanimously in favour of the proposal to defer the application and no further discussions took place at the meeting in respect of the planning application.)

PCC.504 DELEGATED DECISIONS

A report from the Development Manager was submitted listing all recent Planning application decisions made by Officers using delegated powers. Councillor Harris sort clarification as to the decision relating to planning application 60896 which was provided at the meeting. Councillor Wright requested further details relating to the delegated decision in respect of planning application 60779.

Delegated decision:

That the report be noted.

PCC.505 PLANNING APPEALS

A report from the Head of Development Management was submitted listing all recent Planning Appeal decisions since the last meeting of the Planning Control Committee.

Delegated decision:

That the report be noted.

PCC.506 PLANNING ENFORCEMENT CHARTER

The Assistant Director (Localities) submitted a report seeking to update the Planning Enforcement Charter, both to bring it up to date with current legislation and National guidance and also to better reflect the processes and procedures of the Enforcement Team. The Chair informed the meeting that this was a Council decision, delegated to the Planning Control Committee.

Delegated decision:

That the Planning Control Committee approve the Charter's standards and adopt the updated Planning Enforcement Charter.

CHAIR
COUNCILLOR J Black

(Note: The meeting started at 7.05 pm and ended at 9.10 pm)

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Title	Planning Applications
To:	Planning Control Committee
On:	28 February 2017
By:	Development Manager
Status:	For Publication

Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

This report has the following implications

Township Forum/ Ward: Identified in each case.

Policy: Identified in each case.

Resources: Not generally applicable.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:
The elimination of discrimination, harassment and victimisation;
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

Human Rights: All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

Development Manager

Background Documents

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

01	Township Forum - Ward: Radcliffe - East	App No. 60673
	Location: Eton Hill Business Park, Eton Hill Road, Radcliffe, Manchester, M26 2US	
	Proposal: Erection of 18 no. small industrial units for Class B1 (Business), Class B2 (general industry) and Class B8 (storage and distribution) with associated car parking and service yard	
	Recommendation: Approve with Conditions	Site Visit: N
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02	Township Forum - Ward: Ramsbottom + Tottington - Tottington	App No. 60762
	Location: Victoria Mill, High Street, Walshaw, Bury, BL8 3FS	
	Proposal: Proposed extension and conversion of existing mill to provide 13 no. 1 & 2 bedroom residential apartments with 16 no. car parking spaces and associated works	
	Recommendation: Minded to Approve	Site Visit: N
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03	Township Forum - Ward: Bury West - Church	App No. 60767
	Location: Site adjacent to 24 Bolton Road, Bury, BL8 2QR	
	Proposal: Replacement of externally illuminated 48-sheet advertising display with 48-sheet digital display	
	Recommendation: Approve with Conditions	Site Visit: N
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04	Township Forum - Ward: Prestwich - Holyrood	App No. 60812
	Location: Land at side of 46 Merton Road, Prestwich, Manchester, M25 1PL	
	Proposal: Demolition of existing garage and erection of new dwelling	
	Recommendation: Approve with Conditions	Site Visit: N
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05	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 60870
	Location: Bast House Farm Barns, Manchester Road, Ramsbottom, Bury, BL9 5LZ	
	Proposal: Conversion of two redundant agricultural outbuildings to create 6 no. dwellings	
	Recommendation: Approve with Conditions	Site Visit: N
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06	Township Forum - Ward: Whitefield + Unsworth - Pilkington Park	App No. 60901
	Location: Land at Bank Street, Whitefield, Manchester, M45 7JF	
	Proposal: Demolition of existing buildings and erection of two storey office (Class B1) building and associated car parking, hard and soft landscaping works and associated works	
	Recommendation: Approve with Conditions	Site Visit: N
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07	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 60974

Location: Side of 10 Scotland Place, Ramsbottom, Bury, BL0 9BD
Proposal: Erection of 1 no. attached dwelling

Recommendation: Approve with Conditions

Site Visit: N

08 Township Forum - Ward: Prestwich - St Mary's **App No.** 60997

Location: Land to the rear of Sandy Lane, Prestwich, Manchester, M25 9PS
Proposal: Demolition of existing garages and erection of 2 no. semi detached houses

Recommendation: Approve with Conditions

Site Visit: N

09 Township Forum - Ward: Prestwich - Sedgley **App No.** 61022

Location: 45 Bury Old Road, Prestwich, Manchester, M25 0EY
Proposal: Extension and change of use to existing dwelling to form first floor self contained flat with post graduate study centre on ground floor; Formation of new vehicular access from Kings Road

Recommendation: Approve with Conditions

Site Visit: N

10 Township Forum - Ward: Whitefield + Unsworth - Unsworth **App No.** 61048

Location: 57 Parr Lane, Bury, BL9 8JR
Proposal: Second floor extension to form three storey building

Recommendation: Approve with Conditions

Site Visit: N

Ward: Radcliffe - East

Item 01

Applicant: Hamilton Heath Estates

Location: Eton Hill Business Park, Eton Hill Road, Radcliffe, Manchester, M26 2US

Proposal: Erection of 18 no. small industrial units for Class B1 (Business), Class B2 (general industry) and Class B8 (storage and distribution) with associated car parking and service yard

Application Ref: 60673/Full

Target Date: 24/02/2017

Recommendation: Approve with Conditions

Description

The site forms part of an Employment Generating Area and is located close to the junction of Bury Road and Eton Hill Road. The site consists of a hard standing, with trees and vegetation on the perimeter. There is a low brick wall along the boundary with Bury Road and 2 metre high palisade fencing to all other boundaries. Access to the site is from the existing vehicular access off Eton Hill Road, which is shared with the adjacent property.

There is open grazing land to the north of the site and Hutchinson's Goit forms the boundary to the east. To the south and south east are industrial buildings and residential dwellings to the west across Bury Road.

Planning permission was granted in September 2011 for the provision of 194 self-storage containers, which would be stacked at 2 storeys in height. The units would be let to commercial and members of the public for both short term and long term storage. However, this was not implemented due to viability concerns.

Planning permission was granted in March 2016 for the provision of 18 dwellings and this consent has not been implemented to date, but is extant.

The proposed development involves the erection of 18 small industrial units for Class B1 (business), Class B2 (general industry) and Class B8 (storage and distribution). The units would be arranged in two blocks of three units, and a third block of 12 units. The block of 12 units would be located centrally in the site with three units along the boundary with Bury Road and three to the eastern part of the site.

The proposed units would measure 12 metres by 8 metres and would be a maximum of 8 metres in height. The proposed units would be constructed from profiled metal cladding. Vehicular access would be taken from Eton Hill Road via the adjacent site, which is in use for industrial purposes. There would be no means of access between the site and Bury Road.

Relevant Planning History

35433 - External storage of the steel work and cladding of a dismantled prefabricated building to a maximum height of 2 metres for a period of no more than 2 years at land adjacent to Remploy factory at junction of Bury Road and Eton Hill Road, Radcliffe. Approved with conditions - 29 July 1999

54131 - Erection of 194 self-storage container units stacked in 2 storeys at Eton Business Park, Eton Hill Road, Radcliffe. Approved with conditions - 21 September 2011

58324 - Residential development of 18 dwellings at land adjacent to Eton Business Park, Bury Road, Radcliffe. Approved with conditions - 18 March 2016.

01877/E - Pre-application enquiry for proposed small industrial/storage units with associated yard and parking at Eton Business Park, Bury Road, Radcliffe. Enquiry completed - 15 September 2016.

Adjacent site

45808 - Temporary sales cabin at land at Bury Road, adjacent to Remploy Ltd, Radcliffe. Approved with conditions - 29 March 2006.

49849 - Change of use to include the reception, storage and treatment to recycle electrical and electronic equipment at remploy, Eton Hill Road, Radcliffe. Approved with conditions - 21 July 2008.

51808 - Change of use of industrial unit from electrical and electronic equipment recycling (sui generis) to vehicle repair and MOT testing centre (Class B2) use; addition of mezzanine floor; new vehicular entrance and alterations to car park at Remploy, Eton Hill Road, Radcliffe. Approved with conditions - 16 December 2009.

52569 - Application for non-material amendment to approved application 51808 to reduce size of approved vehicle repair and mot testing scheme; deletion of exhaust/extraction flues; elevational alterations at Eton Business Park, Eton Hill Road, Radcliffe. Approved - 12 July 2010

Publicity

The neighbouring properties were notified by means of a letter on 25 November and a press notice was published in the Bury Times on 8 December 2016. Site notices were posted on 6 December 2016.

6 letters have been received from the occupiers of 269, 271, 277, 283, 289 Bury Road, which have raised the following issues:

- Object to the development because of noise and traffic problems. The area is not suitable for articulated lorries.
- Eton Hill Road has been a traffic problem for 20 years with many ideas being put forward but no workable solutions found.
- The plan shows that a HGV would require the full width of the road to turn in/out of the site. This is a very busy road and would not take long for traffic to back up onto Bury Road.
- Some months ago, an articulated lorry reversed up the 40ft private drive of No. 275 Bury Road in order to turn around.
- The turning circle indicated on the plan is too close to the residential properties due to the noise and air pollution this will create.
- The area of land is far more suited to housing and probably more profitable for the land owner. It has been approved once for housing, but unfortunately these were not built.
- There are restrictions on working hours and I would expect these to apply to any new development.
- There are plenty of empty units all over the borough and there is no need for any more.
- Concerns about what materials would be stored in these units.
- The proposed development will affect property prices in the area.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections, subject to the inclusion of conditions relating to access alterations at Eton Hill Road, a construction traffic management plan, measures to prevent mud from passing onto the highway, turning facilities, car parking and means of vehicular access.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Environmental Health - Pollution Control - Comments awaited.

Environmental Health - Commercial Section - Comments awaited.

Waste Management - Comments awaited.

Environment Agency - No objections, subject to the inclusion of conditions relating to surface water drainage, contaminated land and controlled waters.

Design for security - No objections.

GM Fire Service - Comments awaited.

GM Ecology Unit - No objections, subject to the inclusion of conditions relating to invasive species and nesting birds.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and surface water drainage.

Unitary Development Plan and Policies

EC2/1	Employment Generating Areas
EC3/1	Measures to Improve Industrial Areas
EC4/1	Small Businesses
EC6/1	New Business, Industrial and Commercial
EN1/2	Townscape and Built Design
EN1/5	Crime Prevention
EN5/1	New Development and Flood Risk
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/2	Woodland and Tree Planting
HT2/4	Car Parking and New Development
HT4	New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (Employment) - The proposed development is located within the Eton Hill Road Employment Generating Area (EGA) and Policy EC2/1 is relevant.

Policy EC2/1 states that the Council will only allow development for business (B1), general industrial (B2) and warehousing (B8). Other uses will only be permitted where they constitute limited development or do not substantially detract from an area's value as an Employment Generating Area.

The application site is located within an Employment Generating Area and the proposed development would involve the provision of industrial units on site, which would fall within use classes B1, B2 and B8. As such, the proposed development would be acceptable in principle and would be in accordance with Policy EC2/1 of the adopted Unitary Development Plan.

Design and layout - The design of the proposed units would match that of the existing industrial buildings to the south of the site. The proposed buildings would be constructed

from profiled metal cladding, which would be acceptable and would match the existing units. The proposed development would be no higher than the existing buildings on the site. Landscaping would be provided along the boundary with Bury Road, which would screen the proposed units from view. Therefore, it is considered that the proposed development would not be a prominent feature in the streetscene and would be in accordance with Policies EN1/2 and EC6/1 of the Bury Unitary Development Plan.

Impact upon residential amenity - Whilst there are no set standards in terms of separation distances between these types of developments and residential properties, SPD6 is used as a guide to assess relationships and aspect standards between properties and new built development.

There would be a minimum of 30 metres between the proposed units on Bury Road and the existing dwellings, which would be in excess of the 16 metre aspect standard. There would be a minimum of 20 metres between the existing and proposed industrial units, which would be acceptable and comply with policy. Therefore, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties.

Ecology - The application site lies in close proximity to a Site of Biological Interest (SBI) including Swan Lodge. An Ecology Survey and Assessment was submitted as part of the application and has found the site to be of generally low ecological value, which supports common habitats and species. GM Ecology Unit agree with the conclusion of the report and has no objections, subject to the inclusion of conditions relating to invasive species and nesting birds. Therefore, the proposed development would not cause harm to any protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan and the NPPF.

Highways issues - The proposed site plan indicates that the proposed units would be accessed from Eton Hill Road and would share the same access as the existing industrial units. The proposed access route has been tracked using a 12 metre rigid truck. The Traffic Section has no objections to the proposal, subject to the inclusion of conditions relating to access alterations at Eton Hill Road, a construction traffic management plan, measures to prevent mud from passing onto the highway, turning facilities, car parking and means of vehicular access. Therefore, the proposed development would not be detrimental to highway safety and would be in accordance with Policies EN1/2 and EC6/1 of the Bury Unitary Development Plan.

Parking - SPD11 states that the maximum parking standards for B1 use is 1 space per 40 square metres, for B2 use is 1 space per 60 square metres and for B8 use is 1 space per 100 square metres. This equates to 27 spaces for the proposed units.

The proposed development would provide an additional 29 spaces providing a total of 142 spaces for the whole site. Whilst the proposed development would over-provide for the proposed development, the additional 2 spaces would create additional capacity for the existing industrial units. Therefore, in this instance, the level of parking provision is acceptable and would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

Response to objectors

- The issues relating to highway safety have been addressed within the report above.
- There is a valid consent for housing and the applicant would have the option of implementing either proposal, should the current application be approved.
- A condition relating to hours of operation and deliveries has been included.
- The impact upon property prices is not a material planning consideration and cannot be taken into account.
- Any hazardous materials would require the appropriate permits from bodies such as the Environment Agency and Health and Safety Executive.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 3440 01 A, 3440 02, 3440 03 A, 3440 06, 3440 07, A099163-TR001, A099163-TR002 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human

health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. No development shall commence until full details of a scheme for the eradication and/or control of Japanese Knotweed (*Fallonia Japonica*, *Rouse Decraene*, *Polygonum Cuspidatum*) and Himalayan Balsam (*Impatiens Glandulifera*) is submitted to and approved in writing by the Local Planning Authority. The approved management plan shall include a timetable for implementation. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority.
Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. No removal of or works to any trees, shrubs or brambles shall take place during the main bird breeding season 1st March and 31st July inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds' nests immediately before the vegetation is cleared and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority prior to undertaking any works.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
8. A landscaping scheme shall be submitted to, and approved in writing by, the Local Planning Authority prior to the commencement of the development. It shall be implemented not later than 12 months from the date the building(s) is first occupied; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.
Reason. The application does not contain details of landscaping and to secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
9. The use hereby permitted shall not be open outside the following times:
08.00 to 20.00 - Mondays to Fridays
09.00 to 16.00 - Saturdays
Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to the following Policies of the Bury Unitary Development Plan:
Policy EC6/1 - New Business, Industrial and Commercial
Policy EN1/2 - Townscape and Built Design
10. Notwithstanding the details shown indicatively on approved plan references 3440 03 Revision C & A099163-TR003, no development shall commence unless and until full details of the proposed vehicular access alterations at the junction of the site access with Eton Hill Road, incorporating a larger northerly kerb radius, the replacement of existing pedestrian facilities on the northerly side of the access and all associated works to the boundary walls and gate and any necessary highway/highway drainage remedial works, have been submitted to and agreed in

writing with the Local Planning Authority. The details subsequently approved shall be implemented in full before the development hereby approved is brought into use.

Reason. The application does not contain details and to ensure good highway design, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EC6/1 - New Business, Industrial and Commercial

Policy EN1/2 - Townscape and Built Design.

11. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved in writing by the Local Planning Authority and shall confirm/provide the following:

- Access route for construction traffic from the highway network;
- Hours of operation and number of vehicle movements;
- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site or on land within the applicant's control of operatives' and construction vehicles together with storage on site of construction materials.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.

Reason. The scheme does not provide full details of the layout and to mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EC6/1 - New Business, Industrial and Commercial

Policy EN1/2 - Townscape and Built Design.

12. Before the development is commenced, details shall be submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of construction.

Reason. The scheme does not provide full details of the measures to prevent mud from passing onto the highway and to ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy EC6/1 - New Business, Industrial and Commercial of the Bury Unitary Development Plan.

13. The turning facilities on the approved plans shall be provided before the development is brought into use and shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy EC6/1 - New Business, Industrial and Commercial

Policy EN1/2 - Townscape and Built Design.

14. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the development hereby approved being brought into use and thereafter maintained available for use at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policy HT2/4 - Car Parking and New Development of the

Bury Unitary Development Plan.

15. There shall be no direct means of vehicular access between the site and site access served from Bury Road.
Reason. To ensure good highway design in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:
Policy EC6/1 - New Business, Industrial and Commercial
Policy EN1/2 - Townscape and Built Design.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints

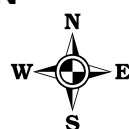


PLANNING APPLICATION LOCATION PLAN

APP. NO 60673

**ADDRESS: Eton Hill Business Park
Eton Hill Road
Radcliffe**

Planning, Environmental and Regulatory Services



Bury
COUNCIL

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60673

Photo 1



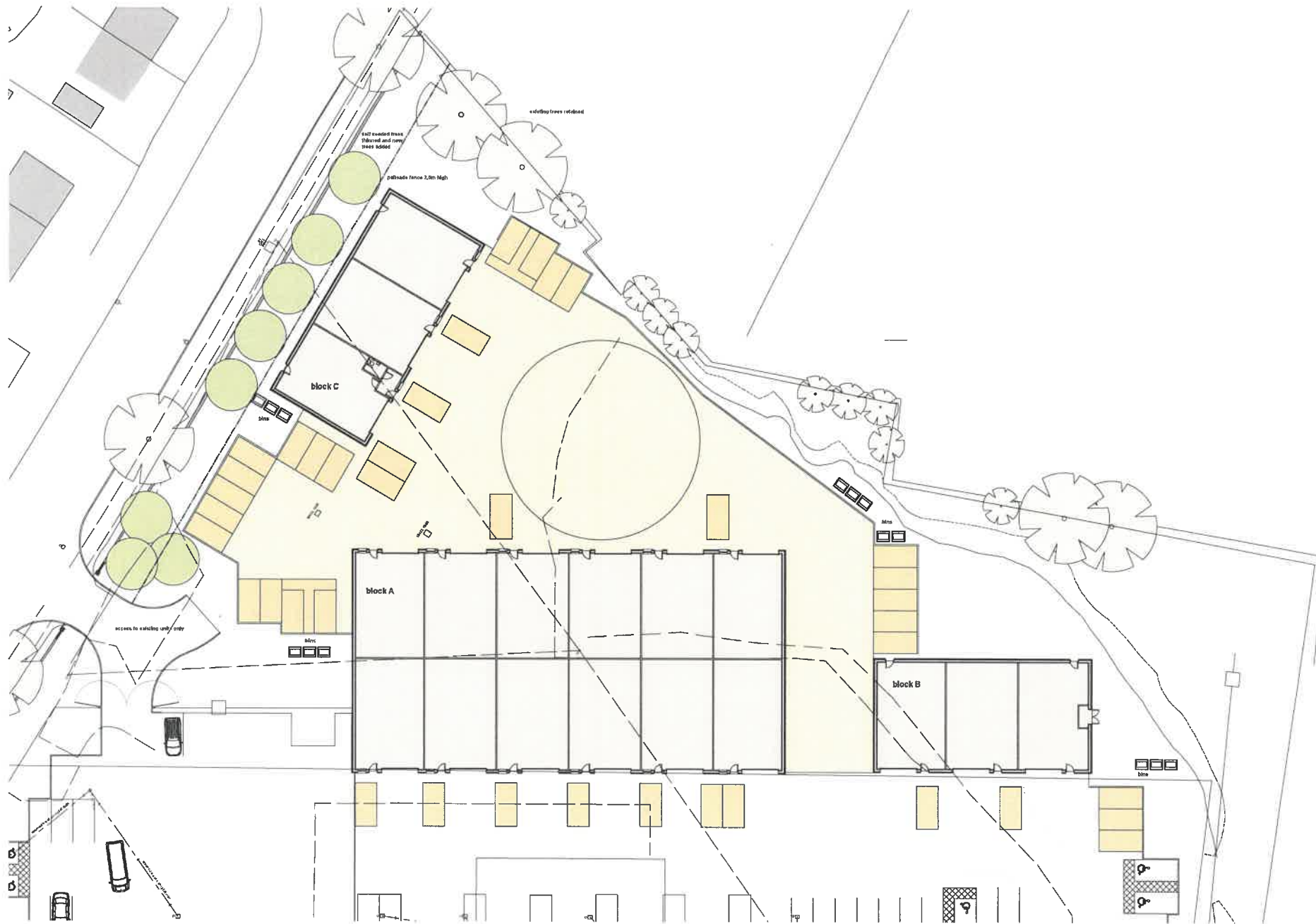
Photo 2



60673

Photo 3







south elevation

east elevation

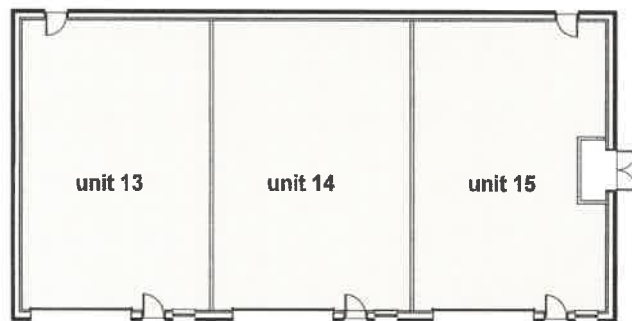


north elevation

west elevation

planning

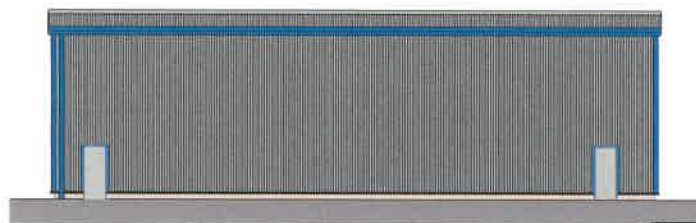
<p>Hamilton Heath Estates Proposed New Units Land at Eton Business Park Eton Hill Road Radcliffe</p>	<p>Plans as Proposed Unit A Sheet 02</p> <p>work to figured dimensions</p> <p><small>© C.J Partnership owns the copyright of this design / drawing which must not be reproduced in whole or in part without the written permission of C.J Partnership</small></p>	<p>contract no</p> <p>3440</p> <p>drawn by</p> <p>FC</p>	<p>drawing no</p> <p>05</p> <p>date</p> <p>September 2016</p>	<p>revision</p> <p>scale @ A3</p> <p>1:200</p>		<p>Blue Pit Mill Queensway Castleton Rochdale OL11 2PG</p> <p>+44 (0)1706 631347</p> <p>info@cjpartnership.com</p> <p>www.cjpartnership.com</p>	<p>CJPARTNERSHIP</p> <p>AN INDUSTRIAL ARCHITECT</p> 
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proposed layout



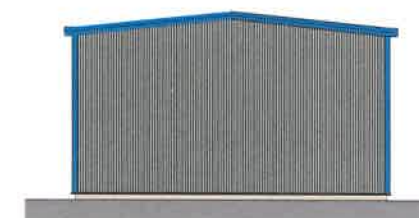
south elevation



north elevation



east elevation



east elevation

planning

Hamilton Heath Estates
Proposed New Units
Land at Eton Business Park
Eton Hill Road
Radcliffe

Plans as Proposed Unit B

work to figured dimensions

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contract no

3440

drawn by
PDC

drawing no

06

date
September 2016

revision

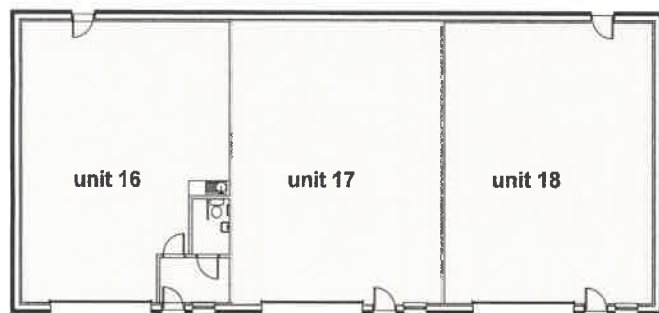
scale @ A3
1:200

Blue Pit Mill Queensway
Castleton Rochdale OL11 2PG

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CJ PARTNERSHIP
ENVIRONMENTAL ARCHITECTS

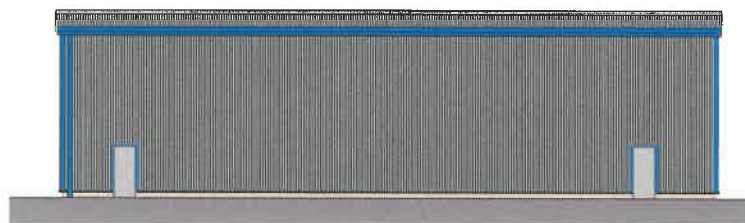




proposed layout



south east elevation



north west elevation



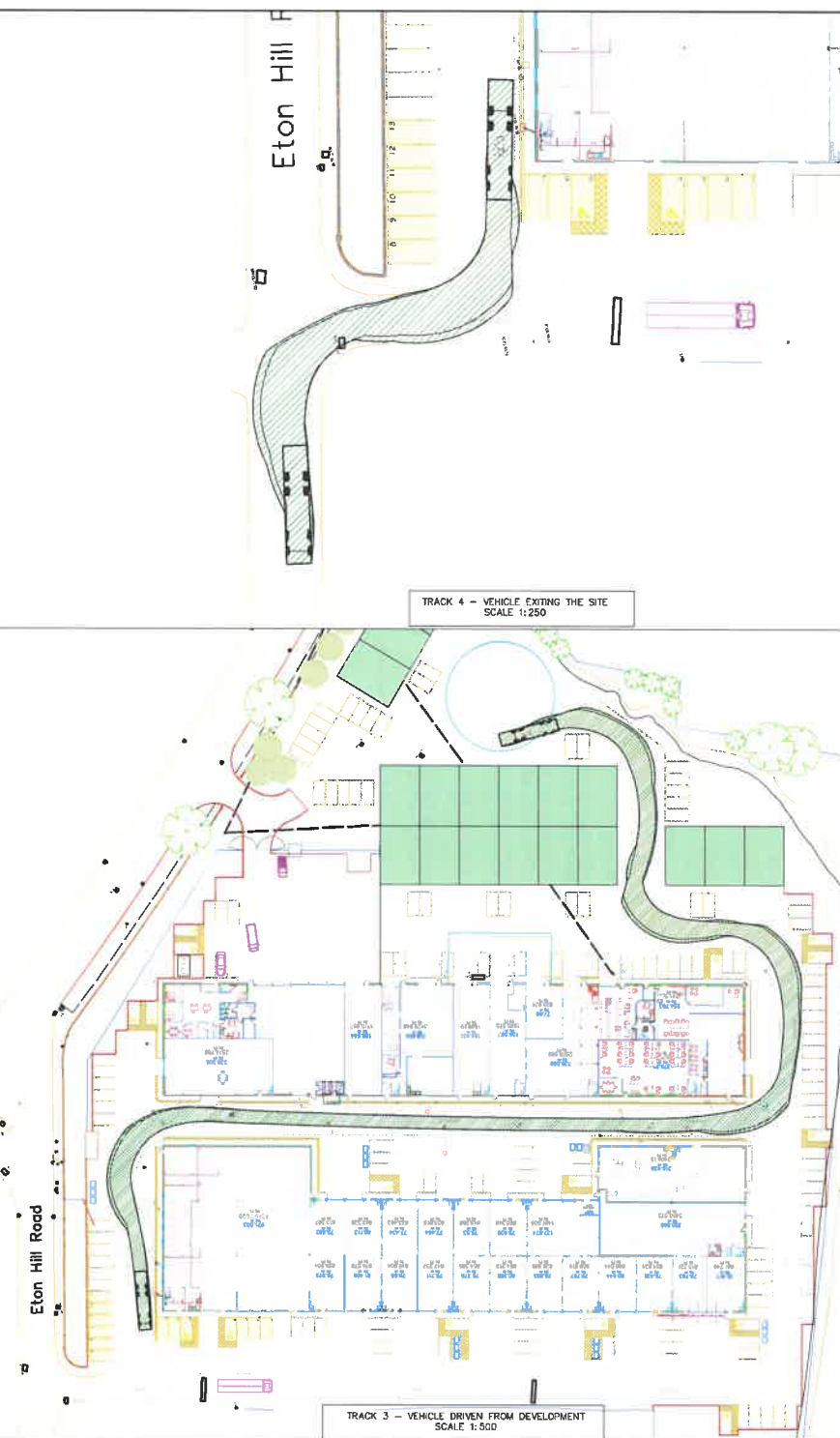
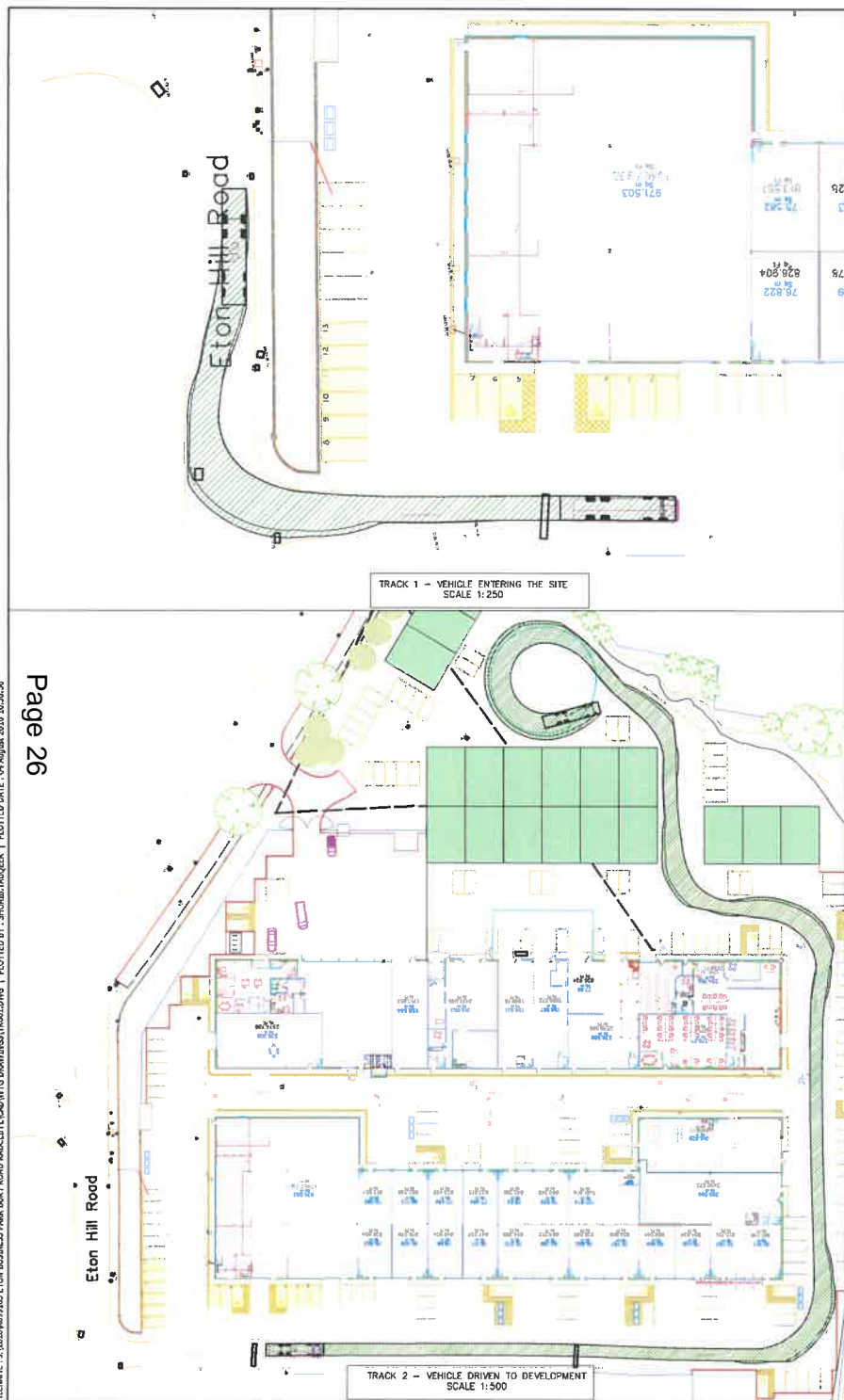
south west elevation



north east elevation

planning

Hamilton Heath Estates Proposed New Units Land at Eton Business Park Eton Hill Road Radcliffe	Plans as Proposed Unit C	contract no	drawing no	revision	Blue Pit Mill Queensway Castleton Rochdale OL11 2PG +44 (0)1706 631347 info@cpartnership.com www.cpartnership.com	CJPARTNERSHIP ENVIRONMENTAL ARCHITECTS
	work to figured dimensions	3440	07			
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DO NOT SCALE; CONTRACTOR TO CHECK ALL DIMENSIONS AND
REPORT ANY OMISSIONS OR ERRORS

1. This drawing should be read in relation to the subject of the title only. Other information shown on the drawing is to be considered indicative only.
2. All dimensions are in metres unless specified otherwise.



Rigid Truck	
Overall Length	12.000m
Overall Width	2.500m
Overall Body Height	3.828m
Min Body Ground Clearance	0.412m
Track Width	2.471m
Lock to Lock Time	6.00s
Kerb to Kerb Turning Radius	11.900m

DRAFT ISSUE

REV	DESCRIPTION	BY	CHK	APP	DATE
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QUAY WEST at MediaCity UK
TRAFFORD WHARF ROAD
TRAFFORD PARK
MANCHESTER
M17 1HH

TEL: +44 (0)161 872 3223
FAX: +44 (0)161 872 3193
e-mail: manchester@wyg.com

Project:
ETON BUSINESS PARK
BURY ROAD, RADCLIFFE

Drawing Title:
SWEPT PATH ANALYSIS
12m RIGID TRUCK

Scale @ A1	Drawn	Date	Checked	Date	Approved	Date
1:250	ST	05.08.16	LR	05.08.16	LR	05.08.16
Project No.	Office	Type	Drawing No.	Revision		
A099163	27	C	A099163-TR001	-		

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Ward: Ramsbottom + Tottington - Tottington

Item 02

Applicant: Cobe Properties Ltd

Location: Victoria Mill, High Street, Walshaw, Bury, BL8 3FS

Proposal: Proposed extension and conversion of existing mill to provide 13 no. 1 & 2 bedroom residential apartments with 16 no. car parking spaces and associated works

Application Ref: 60762/Full

Target Date: 20/02/2017

Recommendation: Minded to Approve

It is recommended that this application is Minded to Approve subject to the signing and completion of a S106 Agreement for recreation provision in accordance with Policy RT2/2 of the adopted Unitary Development Plan and SPD1. If the agreement is not signed within a reasonable timeframe, then delegated authority is sought by the Development Manager to determine the application.

Description

The application relates to a vacant red brick mill building which was last used for employment purposes in the 1990's. It is located on a main road frontage at the head of a 'T' Junction and opposite one of the main routes through from the north west of the Borough to Bury town centre. The site is located within a village setting, and there are a mix of uses nearby comprising housing, shops and public facilities.

Directly to the north of the site are residential properties on Campbell Close. To the east is a main arterial road and war memorial which defines the road junction, and a restaurant to the south. To the west are flats on Grassington Court.

The mill building is three storey in height and sits directly at the back edge of the pavement fronting High Street. It has been extended on the northern side by a part clad/part block 2 storey build with metal shutters on the front. There is a service yard area on the southern side of the building and a yard at the rear.

The mill is generally in good condition and structurally sound, with the external brickwork and stone lintels intact. There are large window openings on the frontage set in a linear and symmetrical style. At the rear, the building is less attractive, and has a cement render which is patchy and started to fall away revealing some brickwork below. There are no windows on this part of the building.

The application seeks the conversion and extension of the mill to provide 3 x No. 1 bed and 10 x 2 bedroom apartments together with 16 parking spaces.

The apartments would be provided over the three floors, with a roof extension accommodating two of the apartments. It is proposed to add a two and half storey extension on the southern side of the building in the existing yard area which would be utilised for the parking of 14 cars. The ground floor would be accessed via an opening off High Street and the first floor via Grassington Court at the rear with 2 spaces allocated for visitors at the rear. On the roof of the extension, it is proposed to provide outdoor amenity space in the form of a roof top terrace enclosed by opaque glazed panels. A bin store would be provided at the rear for communal waste collection.

The main entrance to the apartments would be located at the front off High Street via a newly created glazed foyer and reception area. Other external improvements are proposed

to the external facade of the building which includes cleaning, repointing and repairing, replacement of all windows and new window insertions in the rear elevation and re-rendering the rear of the property.

Relevant Planning History

01736/E - Pre application Enquiry - Completed 20/10/2015

Publicity

82 letters sent on 22/11/2016 to properties on Leigh Street, Bolholt Industrial Estate, Hall Street, Bolholt Villas, Walshaw Road, High street, Campbell Close, Grassington Court.

One letter of objection received from No 33 Campbell Close, with the following issues raised

-

- Vehicular access from the main road leading to a small car park area, now used as a loading bay - has the amount of traffic been measured at different times of the day?;
- Parking at the rear would impact on Grassington Court area, especially as most individuals have 2 cars per household;
- The site would be better having just 10 apartments reducing the impact with regards to vehicles;
- Residents already struggle to park, and have to park on the pavement which is a breach of the highway code - concern that this could impact residents with visitors to the site who would expect to park on our small estate;
- The site needs to be developed but need to look at the bigger picture for the surrounding area and the huge impact on extra traffic to the area.

The objector has been informed of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to conditions.

Drainage Section - No response received. A condition would be included to require the submission of a drainage scheme.

Environmental Health Contaminated Land - No objection subject to conditions.

Greater Manchester Ecology Unit - No objection subject to condition.

United Utilities (Water and Waste) - No response received. Drainage conditions recommended.

Greater Manchester Police - designforsecurity - No objection in principle. Advise the development to be carried out in accordance with the recommendations of the Crime Impact Statement.

Transport for Greater Manchester - No objection. Conditions recommended.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/4	Conversions
EN1/2	Townscape and Built Design
EN1/5	Crime Prevention
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
EC2/2	Employment Land and Premises
EN5/1	New Development and Flood Risk
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
SPD1	Open Space, Sport and Recreation Provision
RT2/2	Recreation Provision in New Housing Development

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policies - Following revocation of the North West Regional Strategy on 20th May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable land.

UDP Policy H1/2 - Further Housing Development considers factors including the need to direct development towards the urban area, availability of infrastructure, need to avoid the release of peripheral open land, unless consistent with urban regeneration, the suitability of the site in land use terms with regard to amenity, nature of the local environment and surrounding land uses and other policies and proposals of the Plan.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

UDP Policy H2/4 - Conversions considers factors of:

- effect on the amenity of neighbouring property;
- general character of the area;
- amenity of occupants;
- effect on the street scene and any external changes

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

EC2/2 - Employment Land and Premises Outside the Employment Generating Areas seeks the retention of existing employment land and premises outside the Employment Generating Areas except where it can be demonstrated that the site/premises are no longer suited to continued employment use.

Principle - The building has been vacant since the early 1990's, and has been identified in the Employment Land Review as unsuitable for retention as an Employment Site in land use terms. In principle, redevelopment for residential purposes is considered to be acceptable.

The proposal would redevelop a vacant and neglected building which is located in the centre of Walshaw. There is existing infrastructure in place to support the scale of the development, and the proposals demonstrate that the property could be successfully converted to provide residential accommodation of an acceptable scale within this location.

The redevelopment would aid the regeneration of the building and contribute to the provision of much needed housing.

As such, it is considered that the principle of the proposal would comply with the NPPF and UDP Policy H1/2 - Further Housing Development and EC2/2 - Employment land and Premises Outside the Employment Generating Areas.

Layout - It is proposed to extend the building to provide 13 No. apartments, 3 No. x 1 bed and 10 No. x 2 bed units, and 16 car parking spaces. This would involve a two and a half storey extension to the south of the building in the yard area adjoining the adjacent restaurant, and the replacement of the existing roof with a roof top extension. The side extension would accommodate the parking at ground and first floor level.

At ground floor, there would be two apartments with the main entrance to the building on the front elevation directly off High Street, which would lead to a reception and lobby area. A bin and cycle store would also be provided. Six parking spaces would be accommodated at ground floor level in the extension, which would be accessed off High Street.

At first floor, there would be 3 units, with 6 units at second floor level utilising the side extension. An outdoor amenity area would be provided on the roof of the side extension and this would be enclosed by 1.2m high opaque glazed panels. The two remaining units would be located in the extended roof space.

Access to the other 8 parking spaces would be from Grassington Court at the rear, with two visitor spaces and the external bin store provided at adjacent to the rear entrance.

The accommodation proposed would provide adequate living space to be considered acceptable for a conversion development, and whilst outdoor amenity space would be fairly limited, the existing built footprint already utilises much of the site area and it is considered that the roof top provision would maximise the potential use of the site.

Parking would be completely contained within the building, and by providing two separate access points to the building at the front and rear, traffic to the site would not intensify vehicular movement to one particular area of the site.

As such, it is considered that the proposed scheme would deliver a good standard of residential development in terms of its layout and facilities, and would comply with UDP Policies H2/1 - The Form of New Residential Development, H2/2 - The Layout of New Residential Development and H2/4 - Conversions.

Design and appearance - In its current state, whilst the mill building has been vacant for a long period of time, the external facade is in relatively good condition. However, it does require some upgrading and improvement works. It is the intention to preserve and retain the character and features of the original building and this is proposed by careful restoration work involving cleaning, re-pointing and repairing of the facade.

The brickwork and stone quoins would be cleaned and the size and positions of the existing window openings would be retained, with the frames replaced by dark grey aluminium frames which would be of a slimline appearance and similarly replicate the existing windows.

The rear of the building is less attractive and the brickwork patchy in parts and it is proposed to apply insulated render to the rear elevation. There are currently no windows in the rear elevation so new openings would be inserted to the ground and first floor which would be of the same design and appearance as those on the frontage. Timber clad panelling between the windows would be minimal but would add an element of interest to the rear elevation.

The 2 storey build to the north elevation is in a poor state and the external facade would be repaired, re pointed and rendered. The extension to the south would be built of facing

brickwork with similar windows to match the main building. The rooftop extension would be recessed and it is proposed to use a dark grey cladding or of equal appearance, which can be subject to a conditional approval.

The existing roller shutter door on the front elevation which currently provides the main access to High Street would be replaced with a large glazed window and door to clearly denote the entrance into the apartment block.

It is considered that the proposed works to the external facade of the building, including the new additions, would be an appropriate and fitting solution to upgrade and improve its appearance, whilst maintaining its historical origins as a mill building. As such the proposal would comply with UDP Policies H2/1 - The Form of New Residential Development, EN1/2 - Townscape and Built Design and H2/4 - Conversions.

Impact on residential amenity - Whilst there are no set standards in terms of separation distances between these types of developments and residential properties, SPD6 is used as a guide to assess relationships and aspect standards between properties and new built development.

SPD6 states that a minimum distance of 20m should be maintained between habitable room windows, plus 3m for every additional storey or difference in levels, and 13m between habitable room windows and a 2 storey blank wall.

Relationship to No 23 Campbell Close - The nearest property is No 23 Campbell Close, which is 7.5m from the 2 storey side elevation and 11m from the 3 storey side elevation of the mill. Separation distances are therefore already sub-standard. There are no windows proposed in the side elevation of the mill facing this property and as such, there would be no difference in the relationship between the two buildings than the existing situation in this respect.

In terms of the roof extension, this would be set back and recessed, and would be no higher than the existing roof ridge. There are no windows proposed which would overlook No 23, the main aspect from this apartment being at the front. The position of the roof extension would be such that given its set back, views from the 1st floor windows of No 23 would be obscured by the edge of the mill building.

As such, it is considered that the relationship to No 23 would be acceptable and there would be no detrimental impact to the amenity of the occupiers of this property.

Relationship to Grassington Court - At the rear are flats on Grassington Court, arranged in a 3 storey block. Nos 55 to 63 which directly face the existing mill building are more than 50m away and given the distance away, would not be affected by overlooking from the building, and aspect standards would be acceptable.

Nos 35 to 45 do not currently have a direct interface with the mill building and face the restaurant. The siting of the proposed side extension would be opposite these flats and would be visible over the top of the restaurant. However, there would be a separation distance of 29m between the extension and the flats on Grassington Court and aspect standards would be satisfied.

Whilst there would be a roof top terrace to the side extension, it would be partly obscured by glazed panelling and again given the distance away, the position of this in relation to the flats on Grassington Court would comply with SPD6 and be acceptable.

As such, it is considered that the proposed development would not have a detrimental impact on residential amenity in terms of proximity, overlooking or privacy and would comply with H2/1 - The Form of New Residential Development and SPD6.

Parking and access - SPD11 states that maximum parking provision required would be 1

space and 1.5 spaces in high access areas and 2 spaces and 2.5 spaces in low access areas for 1 and 2 bed units respectively. This would equate to a total of 18 spaces in high access and 31 spaces in a low access area, for a development of this scale. It is proposed to provide 14 spaces plus 2 visitor spaces.

The applicant has submitted a supporting case for the amount of parking provided in their Planning Statement, which is re-enforced by the submission of a Transport Statement. Whilst the site may not be regarded as the most highly accessible, it is within a semi urban area, with access to local facilities and services within walking distance. The site is also ideally positioned on a bus route with services to Bury town centre and outside the Borough. The Transport Statement has also carried out survey work which demonstrates that the site has a good level of accessibility on foot and by cycle, to a wide range of employment, education, retail and recreation facilities.

The NPPF advocates the principles of sustainable development and encourages sustainable modes of transport and travel when considering new development, and it should be taken into account that the parking standards advised in SPD11 relate to maximum standards, with schemes considered on a site by site basis.

The Highway's team have raised no objection to the proposals, subject to conditions. Given the location of the site within a relatively accessible area, the links to public transport and local amenities, and that the proposal would provide more than 100% parking provision, entirely within the confines of the building, the parking provision for this scheme in this location is considered to be acceptable, and would comply with the principles of sustainable development and the NPPF.

TfGM - Raise no objection to the proposed development. Whilst the proposed car park access appears not to be affected by the position of the lighting column where the bus stop plate is mounted, the applicant is advised to liaise with the Shelters Team to confirm this in the first instance.

TfGM have recommended that if possible, a condition be attached requiring the applicant to fund a scheme towards upgrading the bus stop outside the site on High Street. Paragraph 206 of the NPPF states that planning conditions should only be imposed where they are necessary, relevant to planning and to the development permitted, enforceable, precise and reasonable in all other respects.

In this instance, it is considered that given the scale of the development, its location, the existing bus stop facility and the level of parking proposed for the development, a condition requiring upgrading works to the bus stop would not be considered reasonable or justifiable to make the development acceptable in planning terms.

In terms of ensuring there would be adequate provision made for pedestrian movement, TfGM would recommend that a condition be attached requiring the applicant to reinstate the redundant vehicle access points which served the former site on High Street as these create a poor pedestrian environment and encourage indiscriminate parking. This would form part of the condition imposed by the Local Planning Authorities Highway Team.

Ecology

Bats - A bat assessment which was carried out November 2016 was submitted with the application. Whilst no evidence of bats were found, the building was assessed as having moderate bat roosting potential and further emergence surveys were recommended.

Following consultation with GM Ecology Unit (GMEU), it was advised that as the assessment occurred in November 2016, the emergence surveys should occur prior to determination of the application.

Subsequently, the applicant commissioned an alternative Ecologist to conduct a daytime bat

survey, which was carried out in January 2017 on which GMEU were re-consulted. This report had the benefit of knowing where to inspect in more detail, and provided a case as to why the various potential access points do not provide roosting potential because of water ingress.

On balance, GMEU accept that whilst access for bats is possible, the building is unoccupied and also damp, reducing the risk. However, the scale of the building is such that there would always be some risk. GMEU have therefore assessed the building as low risk, and that precautionary measures recommended by the Ecology Report carried out in January 2017 be conditioned along with a precautionary emergence survey if works would be delayed.

A condition to this effect would therefore be recommended.

Nesting birds - Evidence of nesting birds were found during the bat assessment. A condition is recommended to restrict the timing of works unless a bird nesting survey has been carried out.

Ecological mitigation - The only likely negative impacts are loss of bird nesting and bat roosting habitat. This can be mitigated through provision of artificial bird nesting and bat roosting habitat by condition.

Security - A Crime Impact Statement has been submitted with the application and the design for security team have been consulted. There is no objection in principle to the development with advice that the recommendations of the CIS be incorporated into the scheme. The applicant has addressed the recommendations made, aside from restricting access to the underground parking area at the front with a roller shutter, which would compromise highway and pedestrian safety as a vehicle would not be able to clear the highway when waiting to enter the car park. There would be no access to the apartment block from the underground car park and the applicant would provide CCTV and security lighting to this area. The applicant has also opted not to put railings to the low level windows but instead omit opening lights to these levels and include laminated glass which would secure these lower areas.

The proposal is therefore considered to comply with UDP Policies H2/2 - The Layout of New Residential Development and EN1/5 - Crime Prevention.

Planning obligations

A contribution for recreation would be required of £20,305.35 in accordance with Policy RT2/2 of the Bury Unitary Development Plan and SPD1. This would be secured through a Section 106 Agreement.

Projects have been identified as:

- Improvements to the play area at Town Meadow Park (Phase 1)
- Refurbishment of the tennis courts at Town Meadow park (Phase 1)
- Improvements to the infrastructure at lodges in Kirklees Valley (Phase 1)

Response to objectors - The issues raised regarding proposed access and parking have been covered in the above report. Given the development would be located within a relatively accessible area, the provision of two vehicular accesses into the site and the amount of parking proposed for this scale and type of development, it is considered that there would be sufficient and adequate access and parking provided for the scheme.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal

comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Minded to Approve

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered C1539-P01; P02; P03; P04; P05; P06; P07; P08a; P09a; P10a; P11a; P12b; P13b; P14a; Daytime Bat Survey at Victoria Mill, Walshaw, January 2017, by Rachel Hacking Ecology Crime Impact Statement Version A:11th October 2019 ref 2016/0730/CIS/01 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Reasonable Avoidance Measures for bats shall be carried out in accordance with the details contained in the Daytime Bat Survey at Victoria Mill, Walshaw, January 2017, by Rachel Hacking Ecology, section 4.3 submitted to the Local planning Authority. Should works be delayed beyond April 2017, precautionary emergence surveys shall be carried out by a suitably experienced ecologist and submitted for approval to the Local Planning Authority.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 –

Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

6. No building works shall commence between 1st March and 31st August inclusive in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been approved by the Local Planning Authority.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. No development shall commence unless and until details of mitigation measures through the provision of artificial bird nesting and bat roosting habitat to compensate for the loss of bird nesting and bat roosting habitat have been submitted to and approved in writing by the Local Planning authority. All mitigation measures shall be fully implemented prior to the first occupation of the development hereby approved and thereafter maintained.
Reason. Insufficient information submitted at application stage. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
8. Notwithstanding the details indicated on the approved plans, no development shall commence unless and until full details of the following have been submitted to and agreed in writing with the Local Planning Authority:
 - The reinstatement of all redundant footway crossings along the High Street site frontage to a specification to be agreed, lifting of the footway to meet existing back of footway levels at the westerly and easterly site boundaries and provide adequate falls to the carriageway for the purpose of discharging surface water, proposed level thresholds to the apartment lobby entrance at this increased height above existing levels and all associated highway and highway drainage remedial works;
 - The provision of a vehicular access onto Grassington Court to a specification to be agreed, incorporating the replacement of the redundant footway crossing, provision of adequate falls to the carriageway on the adopted section of the access for the purpose of discharging surface water, provision of adequate levels of visibility at the back edge of the footway and at the junction with Grassington Court in accordance with the guidance in Manual for Streets and all associated highway and highway drainage remedial works and works affecting existing boundary party walls;
 - Foundation and surface water drainage details for the proposed development demonstrating that no encroachment of or discharge onto the existing adopted highway will occur.

The details subsequently approved shall be implemented to an agreed specification and programme.

Reason. Insufficient information submitted at application stage. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design, maintain the integrity of the adopted highway and ensure the intervisibility of the users of the site and the adjacent highways, all in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, H2/4 - Conversions, EN1/2 - Townscape and Built Design, HT6/2 - Pedestrian/Vehicular Conflict and EN5/1 - New Development and Flood Risk.

9. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
- Access route for construction traffic from the highway network;
 - Hours of operation and number of vehicle movements;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
 - Parking on site or on land within the applicant's control of operatives' and construction vehicles;
 - Method statement to cover the delivery and storage of materials and all works abutting the adopted highway, including the provision, where necessary of temporary pedestrian facilities/protection measures.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. Insufficient information submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of road safety pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and Servicing.

10. No development shall commence unless and until details have been submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details only shall be implemented and maintained thereafter during the period of demolition/construction.

Reason. Insufficient information submitted at application stage. To ensure the adopted highways are kept free of deposited material from the ground works operations pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

11. The turning facilities indicated on the approved plans shall be provided before the development is first occupied and the areas used for the manoeuvring of vehicles in each parking area shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, H2/4 - Conversions, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking.

12. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to first occupation of the development hereby approved.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policies H2/2 - The Layout of New Residential Development, H2/4 - Conversions, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

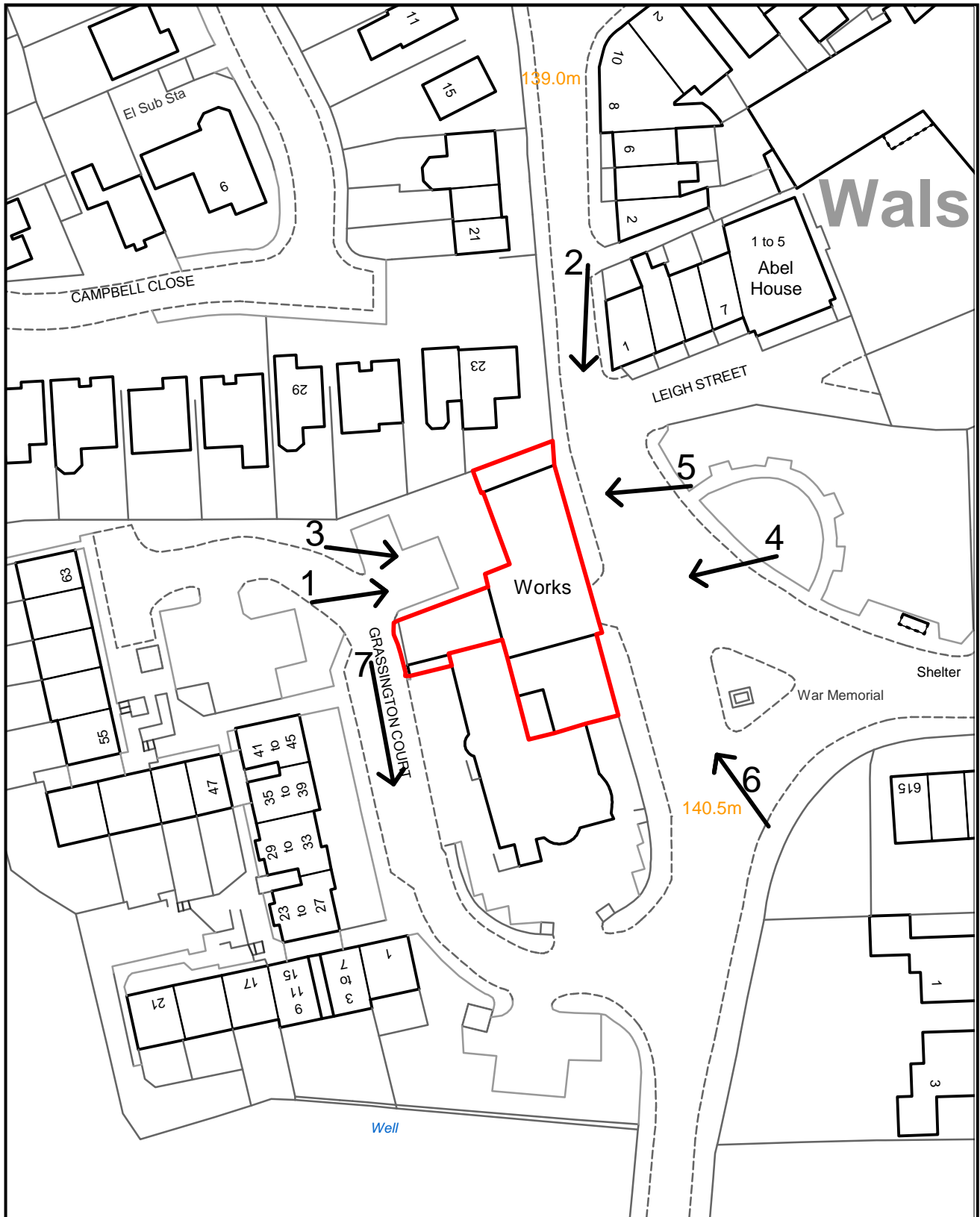
13. Details/Samples of the all materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the

development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Visual Amenity, H2/1 - The Form of New Residential Development and H2/4 - Conversions.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60762

**ADDRESS: Victoria Mill
Walshaw**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60762

Photo 1



Photo 2



60762

Photo 3



Photo 4



60762

Photo 5



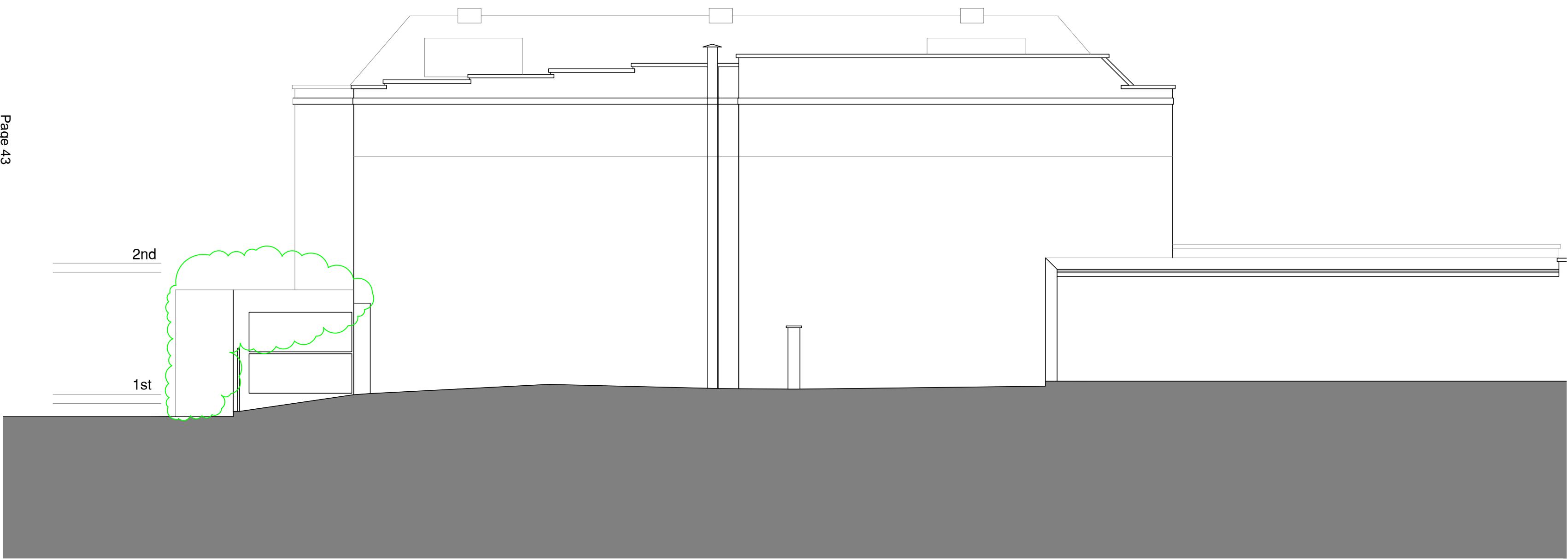
Photo 6



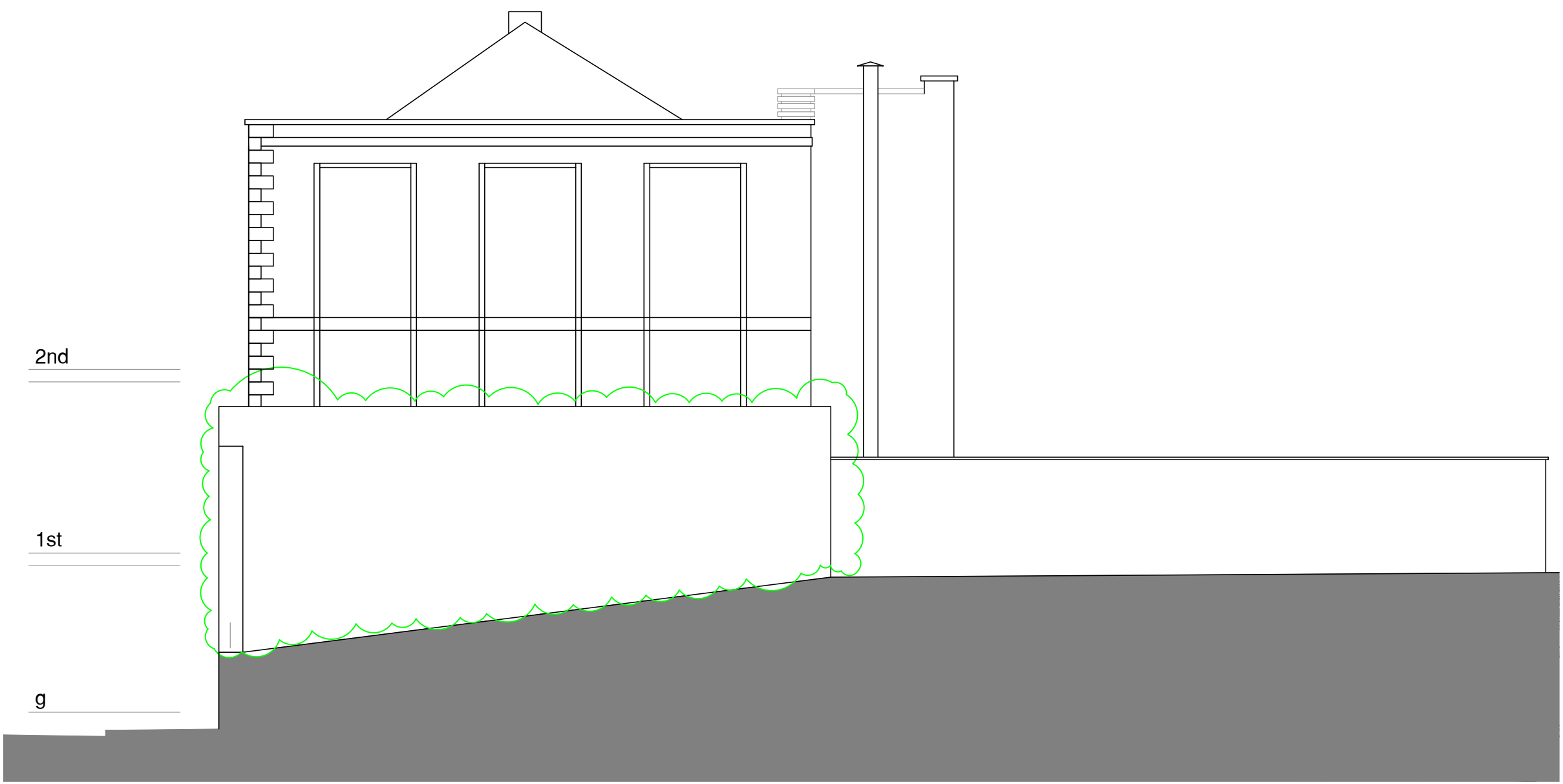
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Photo 7





Existing Rear Elevation to Grassington Court 1:100



Existing Side Elevation to Campbell Close 1:100



Existing Front Elevation to High Street 1:100

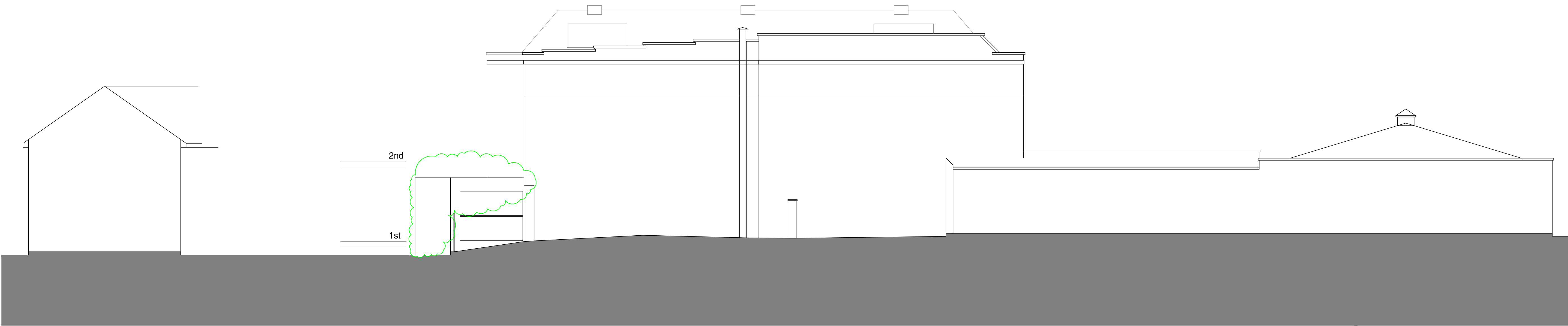


Existing Side Elevation to San Rocco 1:100



Scale bar 1:100

Rev.	Initials	Date	Rev.	Initials	Date	Notes:	PRELIMINARY INFORMATION ONLY FOR CONSTRUCTION FINAL / AS BUILT REVISION	Project	Client	Cobe Consulting Ltd
						This drawing is subject to copyright and is not to be reproduced in part or whole without approval. Do not scale this drawing - Check all dimensions on site.	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	Victoria Mill, High Street, Walshaw	Cobe Properties	
							Checked by:	Proposed extension & conversion to residential	drawn JMT date July 16	
							Existing elevations	scale 1:100@A1 drawing number C1539-P05	
										Derwent House Waterford Business Park Waterford W09 7BR Tel: 0161 762 9920 Fax: 0161 762 9914 E: Melhadmin@cobe.co.uk



Existing Street scene Elevation to Grassington Court 1:100

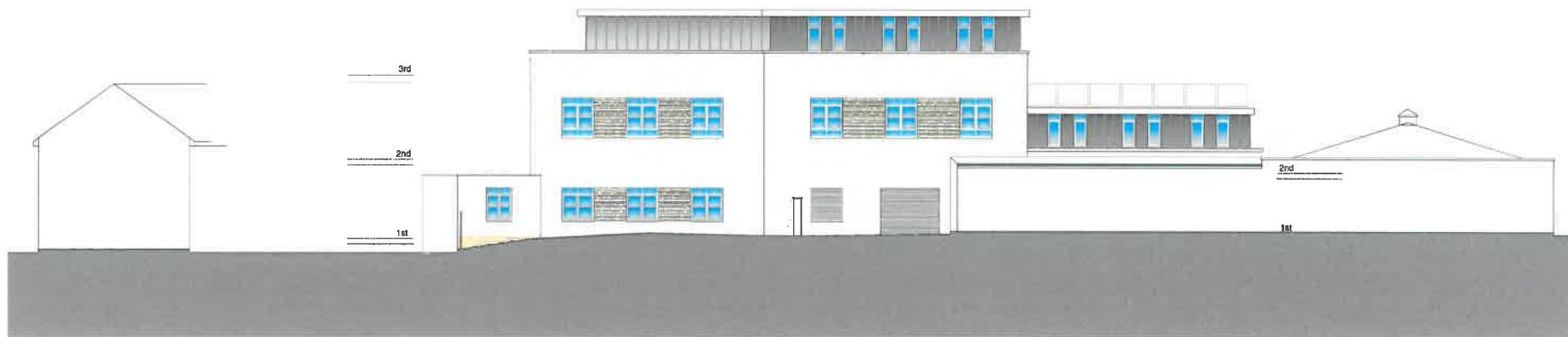


Existing Street Scene Elevation to High Street 1:100



Scale bar 1:100

Rev.	Initials	Date	Rev.	Initials	Date	Notes:	PRELIMINARY INFORMATION ONLY FOR CONSTRUCTION FINAL / AS BUILT REVISION	Project	Client
						This drawing is subject to copyright and is not to be reproduced in part or whole without approval. Do not scale this drawing - Check all dimensions on site.	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	Victoria Mill, High Street, Walshaw	Cobe Properties
							Checked by:	Proposed extension & conversion to residential	drawn JMT
							Existing street scene elevations 1of 2	date July 16
									scale 1:100@A1
									drawing number C1539-P06
									<div>Derwent House Waterford Business Park Waterford W9 7BR Tel: 0161 762 9920 Fax: 0161 762 9914 E: M.hadwin@cobe.co.uk</div>



Proposed Street Scene Elevation to Grassington Court 1:100

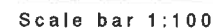
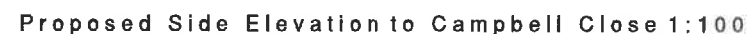
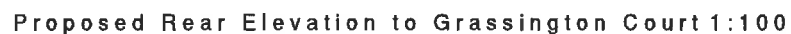



Proposed Street Scene Elevation to High Street 1:100



Scale bar 1:100

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Rev. 1. 0.1.0.1. Following comments received from the LPA deviation treatment to 2 storey section amended			initials Date Rev. JMT 09.02.17			initials Date Notes: This drawing is subject to copyright and is not to be reproduced in part or whole without approval. Do not scale this drawing - Check all dimensions on site.			PRELIMINARY INFORMATION ONLY FOR CONSULTATION FINAL CONSULT PL. BORN Checked by:			Victoria Mill, High Street, Walshaw Proposed extension & conversion to residential Proposed elevations			Cobe Properties JMT July 16 1:100@A1 C1539-P12b					
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Ward: Bury West - Church

Item 03

Applicant: Insite Poster Properties Ltd.

Location: Site adjacent to 24 Bolton Road, Bury, BL8 2QR

Proposal: Replacement of externally illuminated 48-sheet advertising display with 48-sheet digital display

Application Ref: 60767/Advertisement

Target Date: 29/12/2016

Recommendation: Approve with Conditions

Description

This application is being presented to the Committee as an application that has raised a novel planning issue as it would be the first advert application of its kind in the Bury Area.

The site relates to an existing advertising hoarding located alongside a boundary wall facing into a car tyre business at the junction of Bolton Road and Waterloo Street. The wall extends along to the north where there are two other hoardings and to the rear an industrial building. The site lies at one of the boroughs main and busiest traffic junctions to and from the town centre.

The application proposes the replacement of the advertising hoarding with an 48 sheet digital display of the same height and width and 0.3m deeper. It would have LED tiles containing static images changing every 10 seconds. The illumination would be 300cdm2 both day and night. Units are operated remotely removing the need for frequent site visits except for maintenance.

Consent is sought for 10 years as an accompanying statement with the application notes that the proposal is a significant investment at the site.

The LED tiles represent a modernisation of the illumination method for this type of advertising. There has been another application received for replacement hoardings in the borough and it is expected that there will be further applications.

Relevant Planning History

None.

Publicity

None required.

Consultations

Traffic Section - No objection subject to conditions for luminance and operation.

Unitary Development Plan and Policies

EN1/9 Advertisements

EN1/7 Throughroutes and Gateways

NPPF National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning

considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

NPPF Paragraph 67 states that poorly placed advertisements can have a negative impact on the appearance of the built and natural environment. Control over outdoor advertisements should be efficient, effective and simple in concept and operation. Only those advertisements which will clearly have an appreciable impact on a building or on their surroundings should be subject to the local planning authority's detailed assessment. Advertisements should be subject to control only in the interests of amenity and public safety, taking account of cumulative impacts.

UDP Policy EN1/9 relates to adverts and signage and states that proposals should have regard to the character of the locality, scale of the existing building and land use and be considered on their impact on amenity and safety.

UDP Policy EN1/7 - Throughroutes and Gateways is concerned to improve the quality of development along throughroutes and at gateways, and will require new proposals fronting major throughroutes and at the identified gateways to display a high standard of design and landscaping.

Amenity - The area is commercial and a main traffic junction where there are a number of advertising hoardings on Bolton Road, and to the south side of Bury Bridge, which have been in situ for many years. The proposal would replace the traditional poster type advert with a modern digital display which is noted at 300cdm² to be a lower level than the existing one. It would be of the same height and width and slightly deeper by 0.3m. With the exception of the illumination method, it would be of a similar appearance to the existing structure and as such would not appear out of place or intrusive in a predominantly commercial area where there is existing advertising.

There are no immediate residential properties. Opposite the site are terraced shops and therefore the potential for residential above. They are however approximately 38m away and across a main junction therefore there would be no serious impact from the proposal.

The application seeks consent for a 10 year period due to the investment at the site. In this case it is considered that it is reasonable to grant consent for the standard five year period after which the advert would benefit from deemed consent. It would only be required to be removed under a discontinuance notice should circumstances consider it necessary.

Safety - The sign would be located where it would not cause hazard to pedestrians. In terms of its impact on motorists a report has been provided which considers the highway safety aspects of the scheme. Providing a detailed assessment of the site, including road safety records and case studies of similar hoardings at sites within Manchester, it concludes that there are no highway related reasons to withhold consent. The Traffic Section has no objections to the proposal.

With the addition of the recommended conditions to control the luminance and operation of the unit it is then considered that the proposal is acceptable.

The proposal complies with UDP Policy EN1/9 - Advertisements.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan.

These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. Standard Conditions

Any advertisements displayed, and any site used for the display of advertisements, shall be maintained in a clean and tidy condition to the reasonable satisfaction of the Local Planning Authority.

Any structure or hoarding erected or used principally for the purpose of displaying advertisements shall be maintained in a safe condition.

Where an advertisement is required under these Regulations to be removed, the removal shall be carried out to the reasonable satisfaction of the Local Planning Authority.

No advertisement is to be displayed without the permission of the owner of the site or any other person with an interest in the site entitled to grant permission.

No advertisement shall be sited or displayed so as to obscure, or hinder the ready interpretation of, any road traffic sign, railway signal or aid to navigate by water or air, or so as to render hazardous the use of the highway, railway, waterway or aerodrome (civil or military).

Reason for standard conditions: In the interests of amenity and in accordance with the requirements of the Town and Country Planning (Control of Advertisements) Regulations 2007.

2. The luminance of the digital display shall not exceed 300 cd/m2.

Reason. To avoid undue distraction to traffic in the interests of road safety, and to protect the amenity of adjoining occupiers pursuant to policy EN1/9 - Advertisements of the Bury Unitary Development Plan.

3. (a) No individual advertisements displayed on the LED panel shall contain moving images, animation, video or full motion images or any images that resemble road signs, traffic lights or traffic signs.

(b) No individual advertisements shall be displayed for duration of less than 10 seconds.

(c) Controls shall be in place to ensure smooth uninterrupted transition of images displayed on the panel.

(d) The display panel shall be fitted with a light sensor, designed to adjust the brightness to changes in ambient light levels.

(e) A mechanism shall be in place so that if the installation breaks down, it defaults to a black screen to avoid any flashing error messages or pixilation.

Reason. To avoid undue distraction to traffic in the interests of road safety, and to protect the amenity of adjoining occupiers pursuant to policy EN1/9 - Advertisements of the Bury Unitary Development Plan.

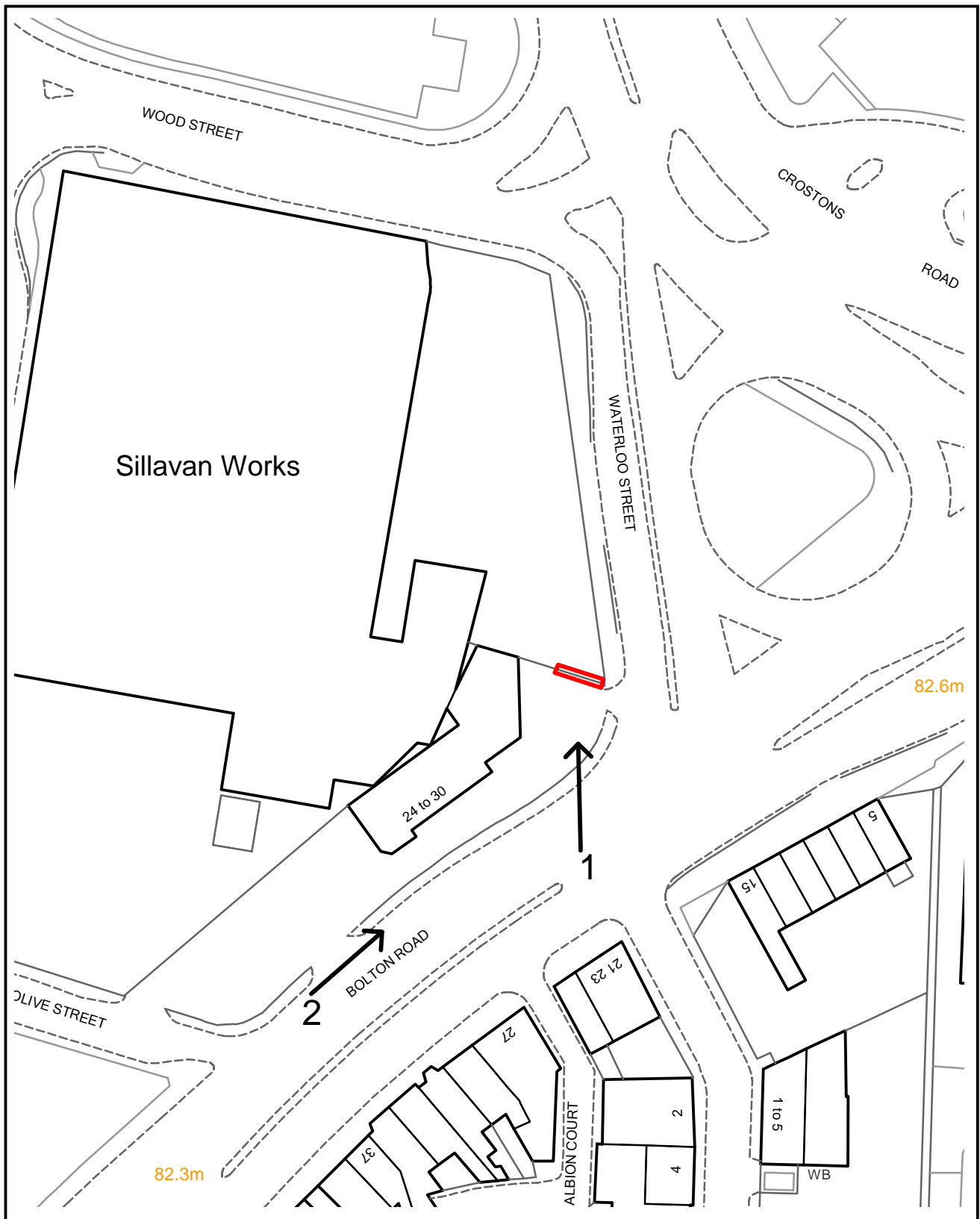
4. This decision relates to drawings numbered T4061 A4 050, T4061 A4 051, T4061 A4 060 and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

5. The Express Consent hereby granted expires 5 years from the date of this decision.
Reason. The development is of a temporary nature only pursuant to Bury Unitary Development Plan Policy EN1/9 - Advertisements.

For further information on the application please contact **Jane Langan** on **0161 253 5316**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60767

ADDRESS: Site adjacent to 24 Bolton Road
Bury

Planning, Environmental and Regulatory Services

(C) Crown Copyright and database right (2015). Ordnance Survey 100023063.



Bury
COUNCIL

60767

Photo 1

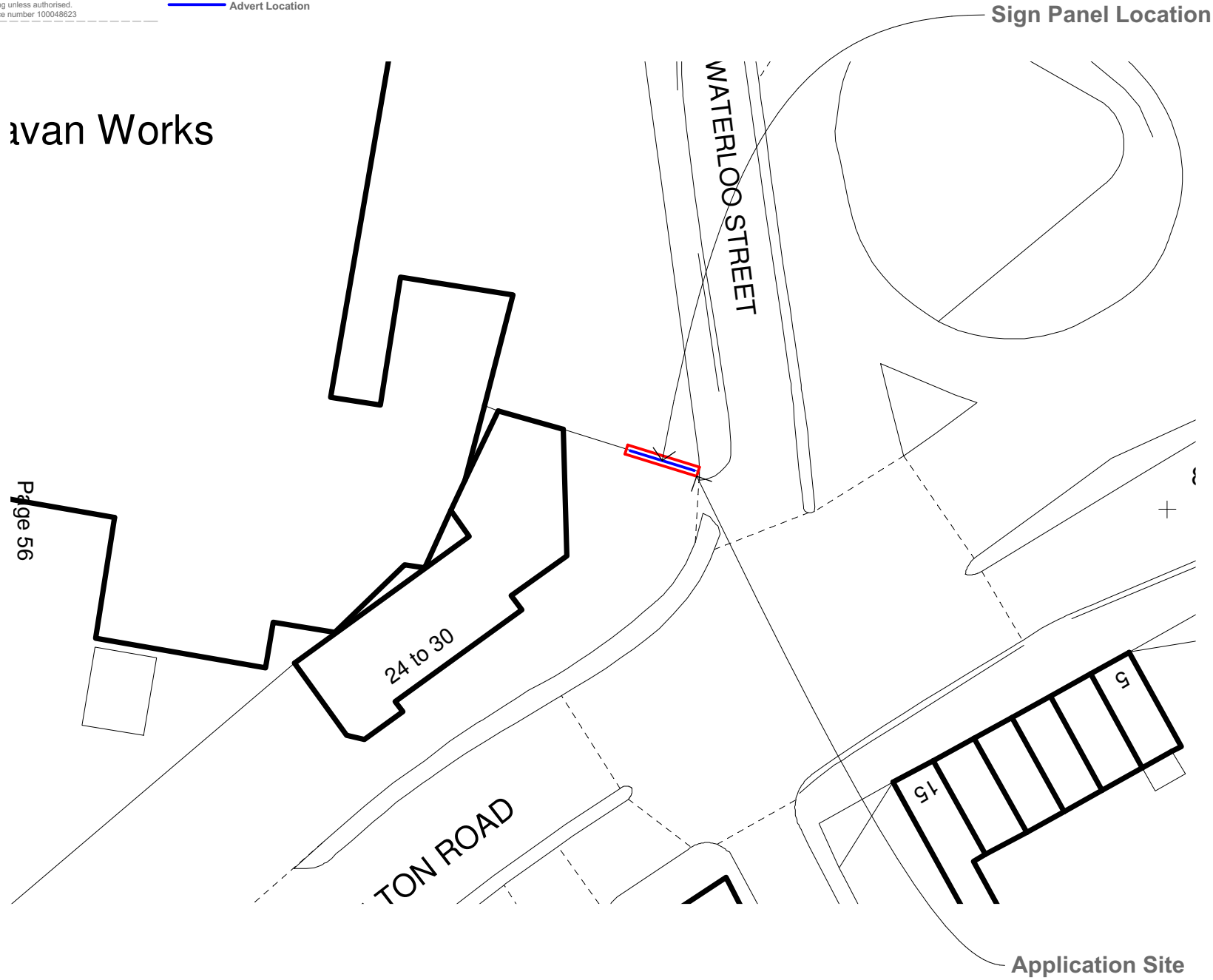


Photo 2



Notes.
All details existing, unless marked proposed
Original drawing size A4
Scaled dimensions should not be taken from this
drawing unless authorised.
Licence number 100048623

Site Boundary
Advert Location



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insite

Insite Poster Properties Ltd
Barton Hall
29 Gloucester Street
Cirencester
Gloucestershire, GL7 2DJ
Tel. 01285 658666 www.insite.co.uk

INSITE Ref:
6646

Project

Land at
24, Bolton Rd
Bury
BL8 2QR

Drawing Name
Site Plan - Proposed

Drawing Status
PLANNING

Revision
- First Formal Issue RW

Date
12 October 2016

Drawing Scale @ A4
1:500

Layout ID Revision
T4061 A4 051



INSITE Ref:
 6646

Project

Land at
 24, Bolton Rd
 Bury
 BL8 2QR

Drawing Name

**Elevations - Existing
 and Proposed**

Drawing Status

PLANNING

Revision

- First Formal Issue RW

Date

12 October 2016

Drawing Scale @ A4

1:100

Layout ID **Revision**

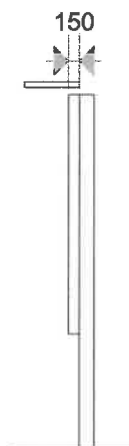
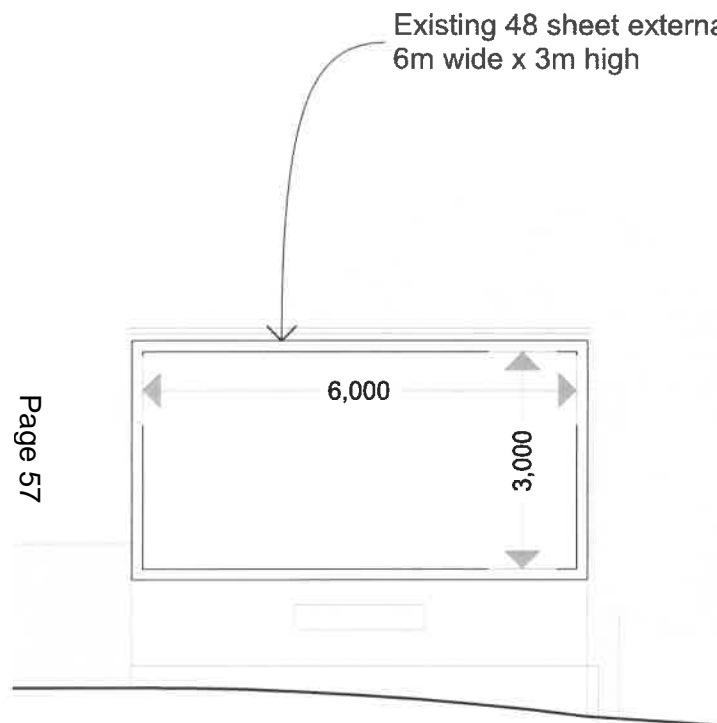
T4061 A4 060



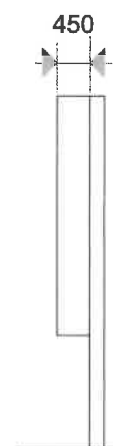
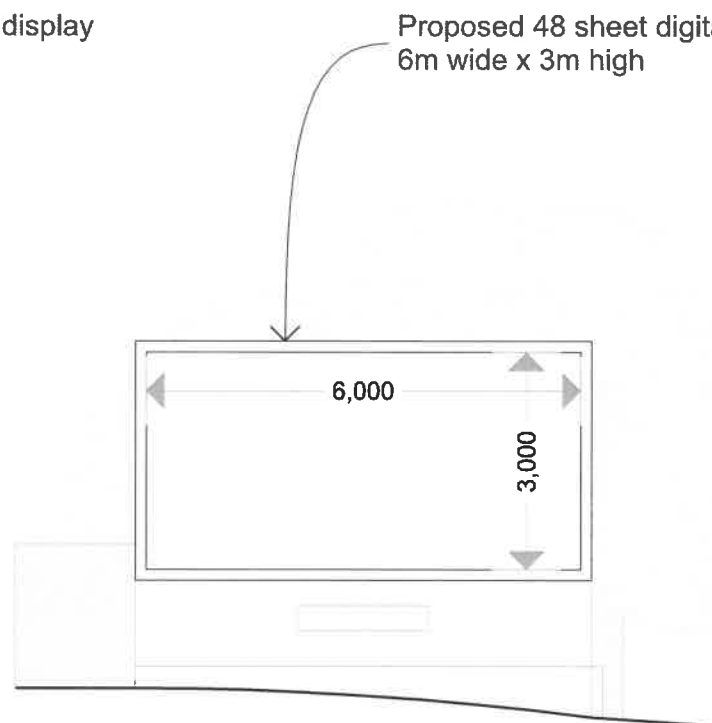
Page 57

Existing 48 sheet externally illuminated display
 6m wide x 3m high

Proposed 48 sheet digital display
 6m wide x 3m high



Section



Section

Existing
 South West Elevation

Proposed
 South West Elevation



Example of digital LED unit at Oldham Road, Manchester



Example of digital LED unit at Oldham Road, Manchester



Ward: Prestwich - Holyrood

Item 04

Applicant: Mr Paul Whiteman

Location: Land at side of 46 Merton Road, Prestwich, Manchester, M25 1PL

Proposal: Demolition of existing garage and erection of new dwelling

Application Ref: 60812/Full

Target Date: 27/01/2017

Recommendation: Approve with Conditions

The Development Manager has recommended a site visit take place prior to the Planning Control Committee Meeting.

Description

The site forms part of the side garden of No 46 Merton Road. This property is a 2 storey semi detached dwelling located at the end of a cul de sac and accommodates a large plot. There is a side garden with a detached double garage and 2 parking spaces in front, and a rear garden which is bounded by a timber fence and conifer trees.

Opposite are terrace houses fronting onto Merton Road, to the rear are houses on Malvern Close and to the side is an unadopted access road which runs between the side boundary of the site and the rear back yards of the terrace houses which front onto Orange Hill Road. The application is for the demolition of the existing garage and the erection of a 2 storey detached dwelling. The dwelling would be set 1.5m from the shared boundary with No 46, and 2.5m from its side elevation and would follow the front building line of this property and the rest of the row of houses on this street. The new dwelling would project 8.17m with a minimum rear garden depth of 7.5m to the boundary with No 25 Malvern Close. It is proposed to retain the existing boundary fence and trees between these two properties.

At the front, it is proposed to keep the dropped kerb which would provide access to a front driveway and in curtilage parking for 2 cars for the new dwelling. The remaining part of the front garden would be lawned.

The proposed dwelling would be designed in a similar context to the adjacent houses and those on the row, in terms of its period appearance, use of red brick, roof design and height and window size and fenestrations. At the rear on the southern side, part of the first floor would be set back 2.4m. Accommodation would comprise living areas at ground floor and four bedrooms at first floor.

In terms of No 46, there would be a boundary fence separating the new dwelling and one in-curtilage parking space would be provided in front of the house.

Relevant Planning History

01001/E - One new residential dwelling - Enquiry completed 03/02/2012

01791/E - Proposed detached dwelling house within the grounds of 46 Merton Road - Enquiry completed 13/01/2016

01866/E - Pre application enquiry for proposed splitting of existing plot into two separate with one retaining the existing house with small extension to the rear, and a new detached house being built on the second plot. - Enquiry completed 19/08/2016

60811 - Single storey extension at rear; New vehicular access/driveway at front - Approved 21/12/2016.

Publicity

26 letters sent on 2/12/2016 to properties at 85-91 (odds), 42,44 Merton Road, 23, 25 Malvern Close, 6-36 (evens) Orange Hill Road.

Letters of objection received from No 91 Merton Road, 24, 26 Orange Hill Road; 25 Malvern Close which raise the following issues:

- Concerned our view and light would be affected;
- The front view which is the main light source was a deciding factor in purchasing the property, as we have afternoon sun and partial sunset view - a building in that space would eliminate that view completely and restrict the sunlight;
- Proposed house is too large for the site and will cause overlooking issues and cast a shadow over the garden;
- Would make the lane at the side dark;
- The coniferous trees are overgrown, unstable and unsightly and should be replaced by proper planting - damage to properties has been caused by the trees
- The root system of the trees to the rear of the plot may in the future interfere with the proposed dwelling or otherwise become a casualty during the construction process;
- Increase the density of buildings around my property;
- The layout is a concern and how it extends towards the rear of the plot and the 'block' style design, is not in keeping in the area which will impact on the character of the area;

Revised plans received to show a set back at first floor level, together with amendments to the design of the rear elevation and roof of the proposed dwelling.

Neighbours affected by the proposed amendments at Nos 23 and 25 Malvern Close were re-notified of the revisions by letter on 6/2/2017.

Further letter of objection received from No 25 Malvern Close with the following issues:

- Cannot view the revised plans and images;
- The application does not conform to Bury Council's UDP particularly in relation to H2/1 - The Form of New Residential development, H2/2 - The Layout of New Residential Development and H2/6 - Garden and Backland Development;
- Key points of objection are detrimental impact upon residential amenities, loss of privacy and overlooking, avoidance of town cramming;
- A detailed letter of objection is to follow.

Further objection received from No 25 Malvern Close, dated 13/2/17, summarised as follows:

- Detrimental impact on residential properties - As a proposed detached property it would not be in keeping with the area, would extend 2.4m further back than other houses on the row, closer to our boundary. Design would be out of keeping and visually impose on No 25. A 4 bed house is out of scale and not in keeping with the area;
- Loss of privacy and overlooking - The view from the rear windows would look directly into the rear windows/glazed patio doors of No 25. The visual lines indicated on the plans do not reflect peripheral vision and there would be direct overlooking. Extending No 46 Merton Road would be a better option. Privacy could be improved by removing rear windows. A landscaping scheme would reduce visual impact. Request a copy of the location of the UU sewer. Concern about tree root systems not been addressed. Trees removed should be replaced. The plans do not indicate true picture of foliage on site.
- Avoidance of town cramming - Over development as it would significantly increase housing density.
- Overshadowing/loss of light - To the garden of No 25.
- National Planning Policy Framework - Principles of Section 17 not met - The revised plans do not address or resolve the objections and do not empower the local people to shape their surroundings. The proposed dwelling would not enhance and improve the place we live and would have a negative impact. Does not take account of the needs of the residents.
- Bury Unitary Development Plan - Policy H1/2 - The proposed does not protect and enhance residential amenity.

- Planning application - Design and Access Statement - Would expect to see a short report and accompanying information. Want to know how the applicant justifies such a proposal. Would like confirmation that there would be no lighting in the rear garden - it would visually impact on No 25. Incorrect figures on the form relating to proposed parking. Want assurance there would be no increase in surface water and drainage problems. The site is not currently vacant - there is a garage.
- Nesting birds - is there a condition to be included?
- Planning Control committee - Request a site visit and would like the opportunity to address Committee.

The objectors have been informed of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to condition.

Drainage Section - A drainage scheme would be subject of a condition.

Environmental Health Contaminated Land - No objection subject to conditions and informatives.

Waste Management - No comments received. Advisory attached that the applicant contact the department to arrange bin waste collection.

United Utilities (Water and Waste) - No objection in principle. A public sewer crosses the site and the applicant should be aware works may be required to divert the sewer.

Greater Manchester Ecology Unit - No objection.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/6	Garden and Backland Development
EN1/2	Townscape and Built Design
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
EN8/1	Tree Preservation Orders

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - Following revocation of the North West Regional Strategy on 20th May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable land.

UDP Policy H1/2 states that the Council will have regard to various factors when assessing

a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

The development would be located within an established residential area and would therefore not conflict with the local environment in terms of character and surrounding land uses. There is existing infrastructure in place to facilitate a single dwelling and the scale of the proposal demonstrates there would not be over development of the site.

The principle is therefore considered to be acceptable, and would comply with UDP Policies H1/2, H2/1, H2/2 and H2/6 and the principles of the NPPF.

Layout of new dwelling- The proposed dwelling would be located in the side garden of No 46 Merton Drive and would replace an existing garage. It would be set 1.5m away from the shared boundary with this property and 2.5m from the side elevation of No 46. The building line would follow that of the adjacent property and the row of other properties on this row, and would therefore have a comparable relationship within the street scene.

The ground floor footprint of the dwelling would project 8.17m with a set back of part of the first floor at the rear of 2.4m. The remaining two storey 'outrigger' at the rear would sit flush with the ground floor and have a high level window in the rear elevation.

Access down the side would lead to a garden at the rear 7.5m minimum in depth, which would be bounded by the line of trees and fence at the rear. There would be adequate provision for the storage of bins.

There is an existing dropped kerb to the front of the site (currently used as the access to the garage) and this would be utilised to provide access onto a driveway to provide two parking spaces. The remaining area in front of the house would be grassed over which would soften the frontage of the site and be similar to the frontages of the other dwellings on this row.

It is proposed to retain the existing trees and fence along the rear boundary of the site, which would also provide some screening to the neighbouring house at the back on Malvern Close.

The proposed layout plan demonstrates that a single dwelling with associated access, parking and amenity area can be accommodated within the plot, and the siting and layout of the development would also reflect the character of the surrounding area and relate well to the streetscene within which it would be set.

As such, the layout is considered acceptable and would comply with H2/1 - The Form of Residential Development, H2/2 - The Layout of New Residential Development and H2/6 - Garden and Backland Development.

As a 'garden' development, and to protect the amenity of adjacent occupiers, it is considered to be appropriate and reasonable to remove permitted development rights.

Layout of No 46 Merton Road - There would be a reduction in garden area and loss of a parking space this property. The proposed layout demonstrates that there would still be acceptable levels of amenity area and in terms of parking, one driveway space would be retained. Most of the properties on this cul de sac have either only one in-curtilage space, or none at all. There would be a space on the road directly outside this property which could be used for parking and this is the situation for most of the properties on this row. As two spaces would be provided for the new dwelling, parking provision is considered to be acceptable and there has been no objection to this arrangement by the Highway's Section.

Design and appearance - The elevation plans and visuals demonstrate that the proposed dwelling would be designed to reflect the scale, massing and appearance of the properties in the surrounding and on this cul de sac. In terms of its height and eaves level the dwelling would follow the built form of the adjacent houses.

The front elevation would feature traditional bay windows and canopied entrance whilst at the rear, modest patio windows would open up the rear ground floor living space. At the rear, part of the first floor on the southern side would be set back, with a high level window inserted in the 2 storey element to bedroom 3.

Materials would comprise reclaimed red brick on the front elevation to match the rest of the street, with common red brick to the side and rear elevations and a grey, slate tiled roof. Samples of materials would be required for submission by a condition.

As such, it is considered that the design and appearance of the proposed dwelling would reflect the character of the locality and the streetscene and therefore considered acceptable and in compliance with UDP Policies H2/1 - The Form of New Residential Development and EN1/2 - Townscape and Built Design.

Residential amenity - SPD6 contains supplementary guidance on householder extensions and acceptability of separation distances between new buildings, and is used as guidance to aid assessment of new residential development.

Generally, a separation distance of 20m is required between directly facing habitable room windows and 13m between a principle ground floor window and a 2 storey blank wall. Where there would be a difference in levels or additional storey, an additional 3m is generally sought.

New dwelling to No 91 Merton Road - There would be a distance of 19m between the front elevation of the proposed dwelling and No 91 Merton Road. The new dwelling would be set on the same building line as the adjacent property, No 46, which has a separation of 19m from No 91, and as such a sub-standard already exists. The proposed dwelling would be positioned such that there would only be 1m of new build which would directly face the house opposite, and given the position of both the ground (which is a non habitable small window) and first floor windows of No 91, there would not be direct overlooking into the house.

As such, the relationship of the new dwelling to No 91 is considered to be acceptable.

New dwelling to Nos 20 and 22 Orange Hill Road - There would be a distance of 17m between the side elevation of the new dwelling and the rear elevations of these houses and as such aspect standards would be met.

New dwelling to No 25 Malvern Close -

At the closest point, there would be a separation distance of 16.9m between the 2 storey element of the rear elevation of the new dwelling and No 25 Malvern Close. This part of the proposed rear elevation would have a high level window and as such there would no direct overlooking to this property.

There would be 19.3m between the other proposed 1st floor window on the set back element of the rear elevation and No 25. There would be no direct interface between these

two properties, no direct or peripheral overlooking and given the oblique relationship between the two, privacy would be maintained and aspect standards satisfied.

In terms of distance to the garden of No 25, there would be 10.4m from the habitable 1st floor window of the proposed dwelling to the boundary, which satisfies the guidance advised in SPD6 and considered acceptable. There would be 8m from the projecting 2 storey part of the build to the boundary and views would be restricted from the high level window, and therefore overlooking would not be of concern.

As such, acceptable levels of separation would be achieved and the proposals would comply with Policies H2/1 - The Form of New Residential Development, H2/6 - Garden and Backland Development and SPD6.

Access and parking - It is proposed to utilise the existing dropped kerb which is currently used to access the garage to No 46. This would enable 2 parking spaces to be provided off road. Exiting the site from the driveway would be the same as the existing situation, and given the plot is located at the head of a cul de sac, there would be sufficient room to manoeuvre a car on and off the driveway.

In terms of parking provision for No 46, this has been discussed above. To summarise, whilst there would be a loss of one space for this property, there would be room for one driveway space. There is on street parking directly in front of the house and this is considered to be an acceptable arrangement.

The Highway's Section have raised no objection subject to conditions and as such the proposal is considered to comply with H2/2 - The Layout of New Residential Development H2/6 - Garden and Backland Development and HT2/4 - Car Parking and Servicing.

Bin store - There would be adequate room to accommodate the bins either down the side of the property or in the rear garden. Waste collection would be as the existing arrangements for the houses on this road.

Ecology

Bats - A bat assessment has been submitted with the application, and whilst not carried out by an experienced or qualified surveyor, the building has been assessed by GMEU as low risk. An informative to advise the applicant of their responsibilities under the Habitat Regulations is recommended.

Nesting birds - It appears likely that potential bird nesting habitat would need to be removed to provide space for the dwelling. All British bird nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act, as amended. A condition is therefore recommended to restrict the timing of the removal of vegetation.

Response to objectors

- The issues raised with regards to layout, design, density, overlooking and privacy have been covered in the above report.
- It is proposed to retain the trees along the rear boundary of the site. There is no requirement to retain the trees, for the purposes of this application, and root systems and future stability of the trees would be a private matter for the occupiers and adjacent residents.
- The objector was advised that if there was an issue viewing the revised plans on the website, the plans would be emailed directly - this was not requested by the objector.

Response to objection from No 25 Malvern Close received on 13/2/17

- Detrimental impact on residential properties - The dwelling would be at the end of the row of a cul de sac, and not viewed in the same context as the other properties on the row. It would be of a very similar design and appearance as the houses on Merton Road in terms of ridge and eaves height, window positions, fenestration pattern and materials. The 2.4m single storey rear projection could be added to the property or any

- property under permitted development rights without the need for planning permission.
- Loss of privacy and overlooking - All aspect standards and separation distances would comply with policy guidance. In the first floor rear elevation of the proposed dwelling, one window would be high level and the other in a set back position, with a distance of 19.3m to No 25. With the oblique relationship, it is considered there would be no direct overlooking. An acceptable garden length in compliance with policy advice would be achieved. A landscaping scheme would be a condition of an approval. The objector can obtain a copy of sewer locations from UU. The applicant has been informed of the UU consultation response and is aware of the location of sewers.
- The development would satisfy policy in terms of parking requirements, private amenity area and separation distances and therefore considered not to be overdevelopment.
- Overshadowing - The dwelling would be an acceptable distance from No 25 not to cause undue loss of light.
- NPPF - The applicant has reduced the scale of the development by the submission of revised plans to address issues of residential amenity impact, on which local residents who would be affected were consulted and able to make representations on.
- Bury UDP - The development, as reported above in detail above, would comply with Bury UDP Policies.
- Planning application - Domestic type lighting of gardens or houses can be put in place, to a certain extent, without the need for permission. A drainage scheme would be required by permission.
- Nesting birds - A condition would be included to restrict the removal of vegetation.
- Planning Committee - A site visit is to take place before the Planning Committee meeting.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered P3/715079/01/Rev B; P3/715079/02 Rev A; P3/715079/03; P3/715079/04 Rev A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The proposed vehicular access arrangements indicated on the approved plan reference P3/715079/01/Rev B, incorporating the construction of a footway crossing and double driveway, shall be implemented to an agreed specification and approved by the Local planning Authority prior to the development hereby approved being brought into use and shall thereafter be maintained.

Reason. To ensure good highway design and maintain the integrity of the adopted highway in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential development, HT2/4 - Car Parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.

4. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the LPA.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

5. Where during any works on site, unforeseen contamination is suspected or found, or contamination is caused, works on the site shall cease and the Local Planning Authority shall be notified immediately. The developer shall then produce a risk assessment and submit remediation proposals, if required, for approval to the Local Planning Authority. On approval of the remediation strategy, the development shall then be carried out in accordance with the approved details and process including any required timescales.

Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.

Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

7. No development shall commence unless and until details of surface water drainage proposals have been submitted to and approved by the Local Planning Authority. The proposed scheme must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided where appropriate. The approved scheme only shall be implemented and thereafter maintained.

Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact and to ensure and promote sustainable development pursuant to Bury Unitary Development Plan Policy EN5/1 - New Development and Flood Risk and chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

8. Details/Samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/ shall be used for the construction of the development.

Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Townscape and Built Design, H2/1 - The Form of New

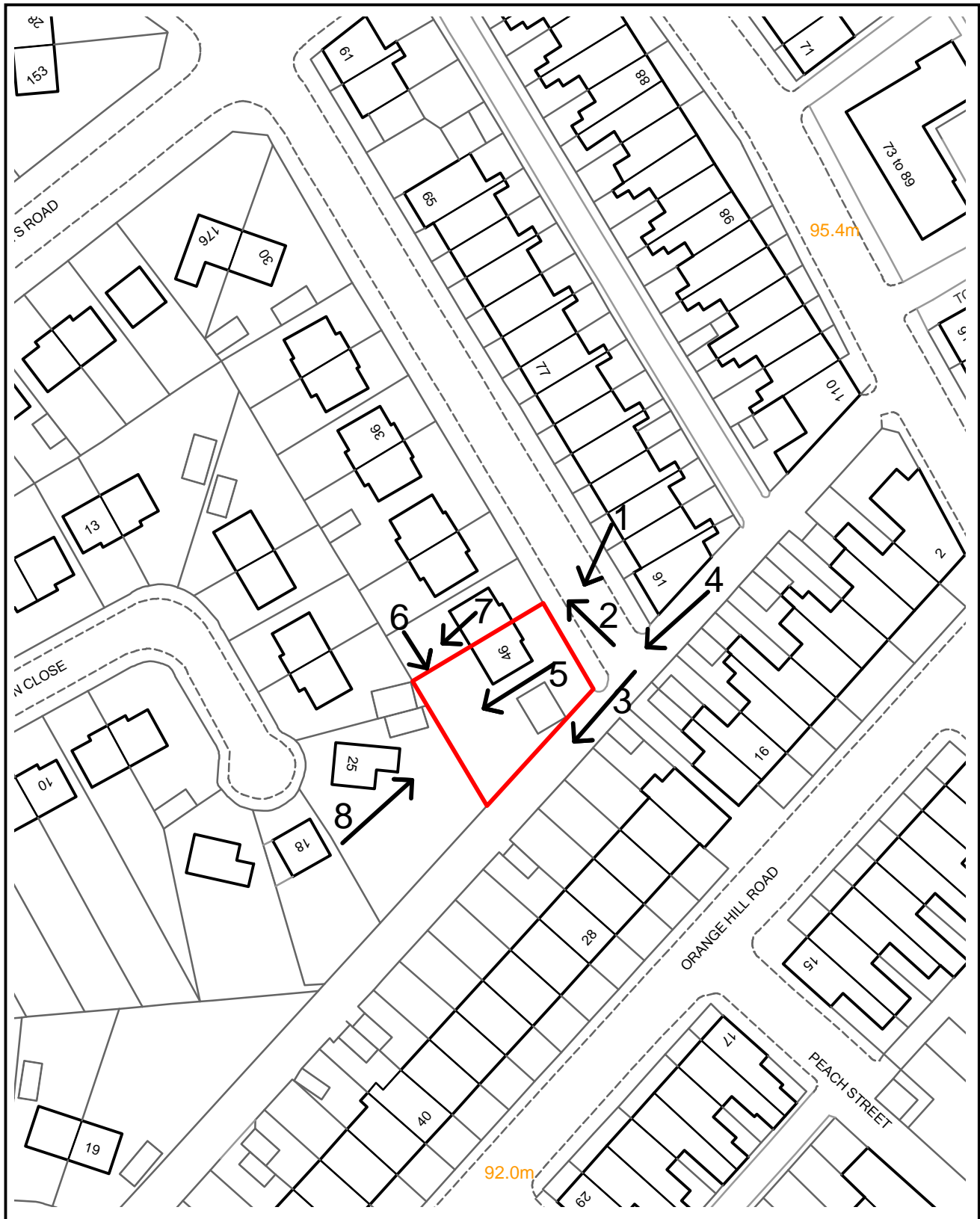
Residential Development.

9. A landscaping scheme shall be submitted to, and approved by the Local Planning Authority prior to the commencement of the development. It shall be implemented not later than 12 months from the date the building(s) is first occupied; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

Reason. Insufficient information submitted at application stage. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60812

ADDRESS: Land at side of 46 Merton Road
Prestwich

Planning, Environmental and Regulatory Services

(C) Crown Copyright and database right (2015). Ordnance Survey 100023063.



Bury
COUNCIL

60812

Photo 1



Photo 2



60812

Photo 3



Photo 4



60812

Photo 5



Photo 6



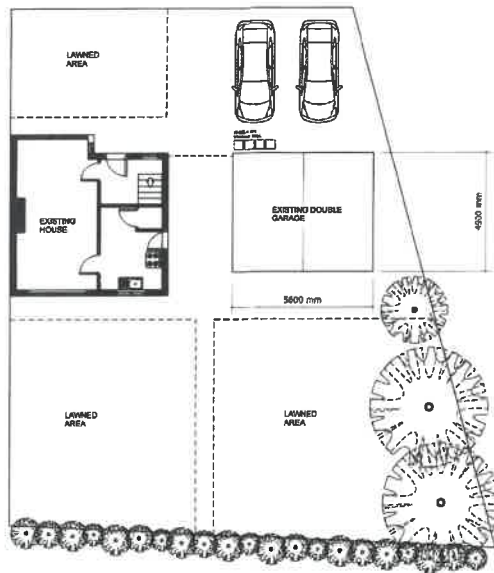
60812

Photo 7



Photo 8





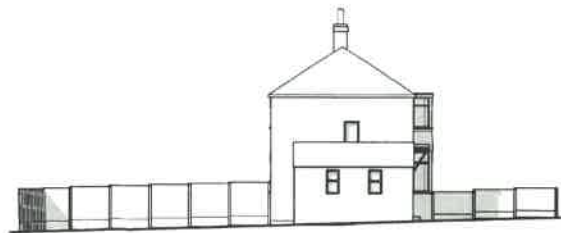
EXISTING SITE / GROUND FLOOR PLAN 1:100



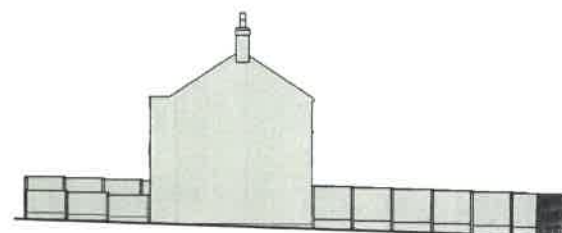
FRONT EXISTING ELEVATION 1:100



REAR EXISTING ELEVATION 1:100



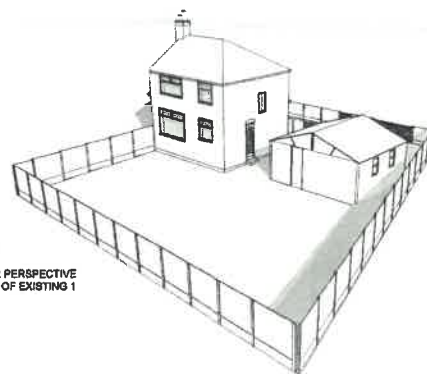
LEFT HAND SIDE EXISTING ELEVATION 1:100



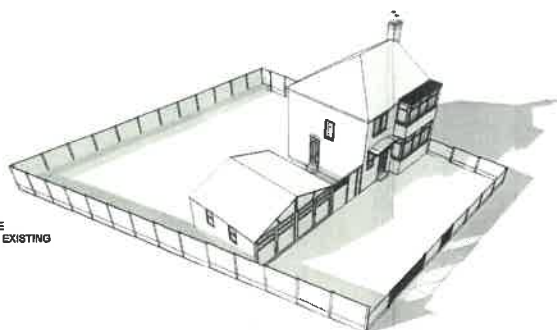
RIGHT HAND SIDE EXISTING ELEVATION 1:100



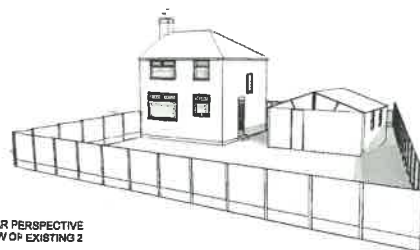
FRONT PERSPECTIVE
VIEW OF EXISTING



REAR PERSPECTIVE
VIEW OF EXISTING 1



BIRD EYE
VIEW OF EXISTING




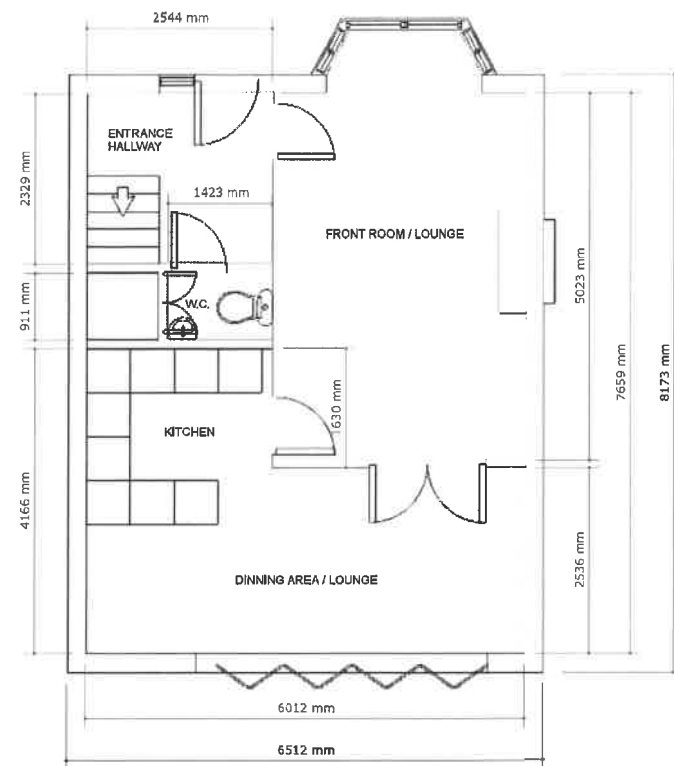
REAR PERSPECTIVE
VIEW OF EXISTING 2



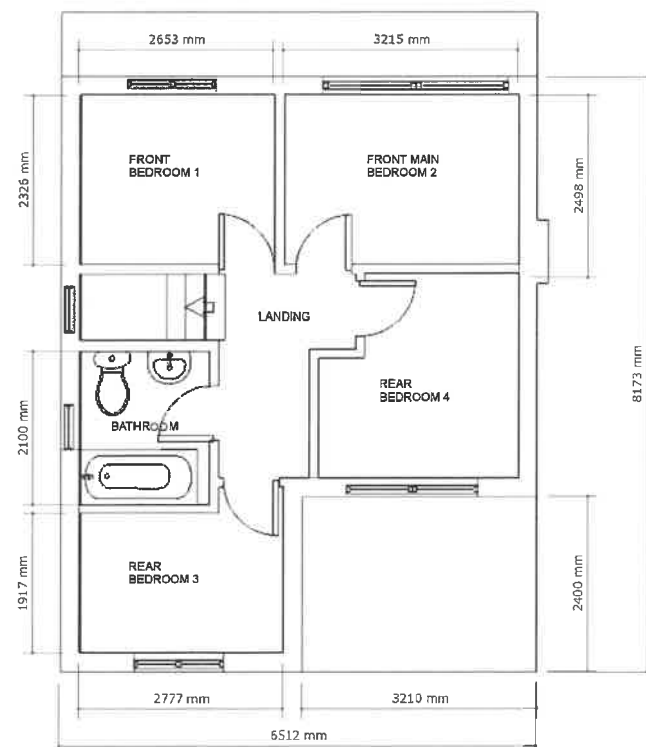
SITE LOCATION PLAN 1:1250

NOTES:
All dimensions must be checked on site and not scaled
from the drawings
All construction must comply with British Standards &
Building Control Regulations as well as being approved by the
responsible Building Control Officer.

Name		Company	
Drawn by		Checked by	
Date		Scale	
 H. M. LAND REGISTRY 1 LONDON ROAD, 2ND FLOOR, MANCHESTER M2 5LH TEL: 0161 275 1271			
Drawn: Mr P. Wilkinson Scale: 1:1250 Planning Application Drawings NEW BUILD & EXTENSION 43 MERTON ROAD PRESTON MANCHESTER M25 1PL			
Drawing Date: EXISTING PLAN & ELEVATIONS SITE LOCATION PLAN			
Scale:		Scale:	
1:100 & 1:1250		1:100 & 1:1250	
Date:		Drawn by:	
21/10/2018		D. White	
Drawing No.: P3 / 110078 / 03 / Rev -			



PROPOSED GROUND FLOOR
PLAN 1:50



PROPOSED FIRST FLOOR
PLAN 1:50



FRONT PERSPECTIVE
VIEW OF PROPOSED

PROPOSAL 1 (Extension)

It is proposed to construct a 2400mm x 4000mm Kitchen extension to the rear of the existing property using new matching materials (white upvc, roof tiles, foundation bricks & mortar) and reclaimed (main brickwork) to match existing build finishes. New front access single car parking space to be provided.

Additional works will be the removal of the existing Kitchen doorway on the side elevation (to be bricked up with reclaimed matching brickwork) as this will be repositioned in the new extension (as shown) and the fitment of a white upvc double glazed patio door in place of a larger single window on the rear elevation.

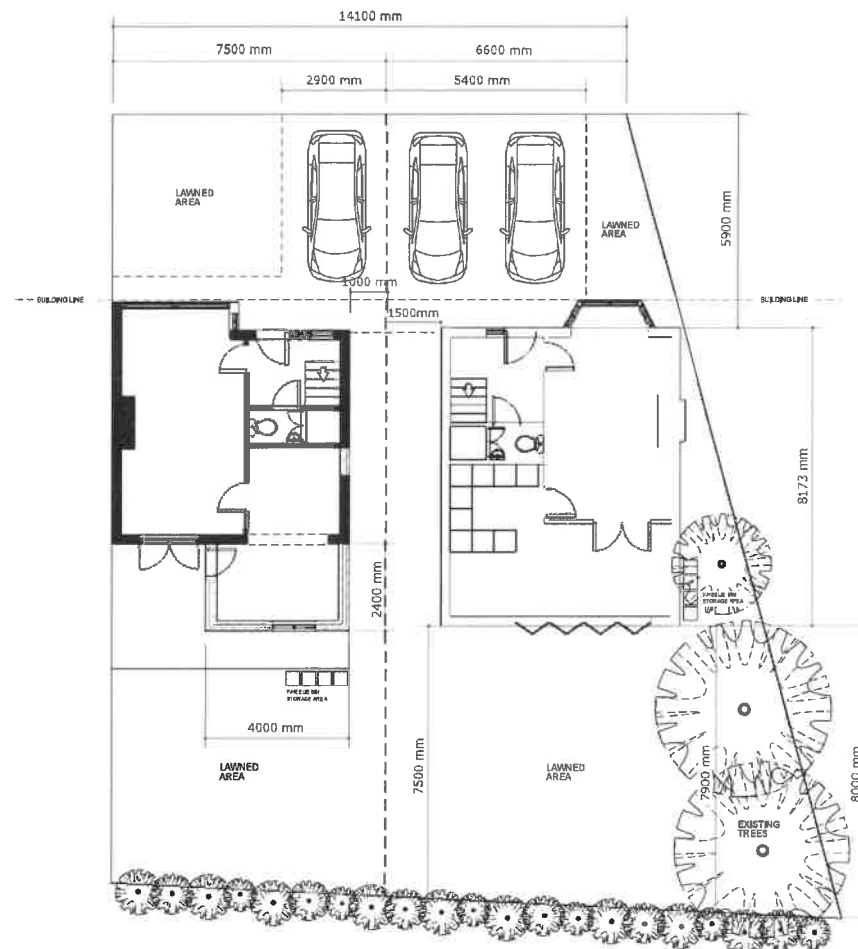
All works are to be carried out a high quality standard ensuring reclaimed brickwork is toothed into the existing main structure brickwork with all mortar finishes to match as closely as possible the existing mortar.

PROPOSAL 2 (New Build)

It is proposed to construct a new detached four bedroomed property using new matching materials (white upvc, roof tiles, foundation bricks & mortar) and reclaimed (main brickwork) to match existing build finishes.

All works will be finished as neighbouring properties.

Construction to be carried out to a high quality standard ensuring reclaimed brickwork is matched to the existing neighbouring structure brickwork with all mortar finishes to match as closely as possible existing properties.



PROPOSED GROUND FLOOR
PLAN 1:100



BIRD EYE
VIEW OF PROPOSED



REAR PERSPECTIVE
VIEW OF PROPOSED 1



REAR PERSPECTIVE
VIEW OF PROPOSED 2

NOTES:
All dimensions must be checked on site and not scaled from this drawing.
All construction work must comply with British Standards & Building Control Regulations as well as being approved by the supervising Building Control Officer.

B	03/02/2017	Drawing amendment, First Floor Plan amendment.
A	03/02/2017	Drawing amendment, Now hold complete link to file
	Date	Revisions



S Langdale Road, Sale, Manchester M33 4EN
Tel: 0161 962 8578

Mr P. Whiteman

Title:

PLANNING APPLICATION DRAWINGS

NEW BUILD & EXTENSION

48 MERTON ROAD
PRESTWICH
MANCHESTER M25 1PL.

Position Titles

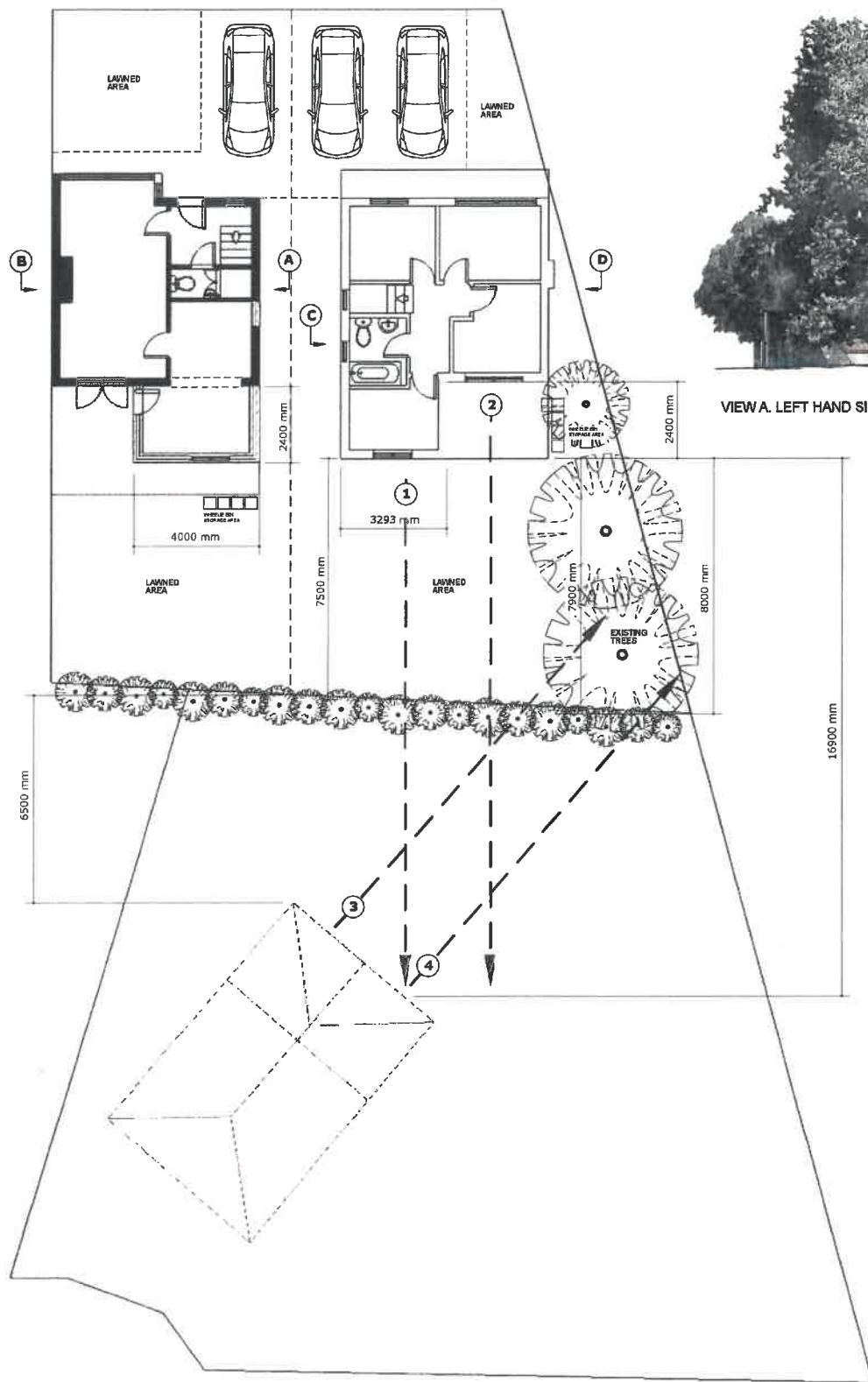
PLANS & VISUALS

1:50 & 1:100

08/02/2017

Drawn by: **D. White**

P3/715079/01/Rev B



PROPOSED GROUND FLOOR
SITE LINES
PLAN 1:100

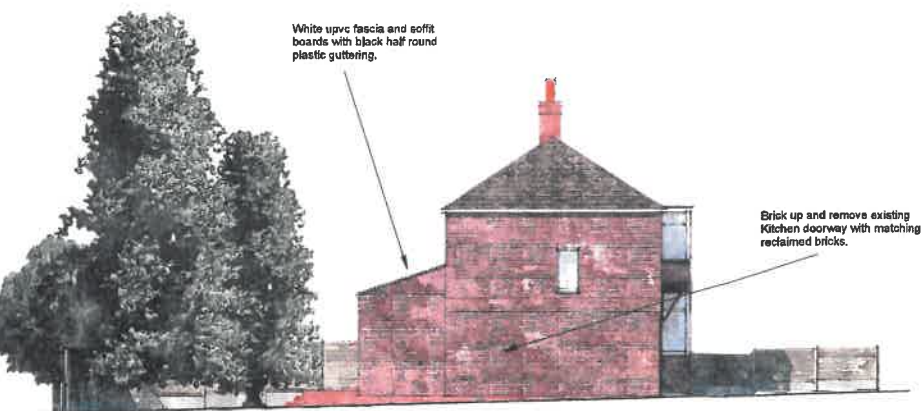
SITE LINES (New Build)

The site lines shown are from centre of existing first floor neighbouring windows (3 & 4) and centres of proposed first floor property windows (1 & 2).

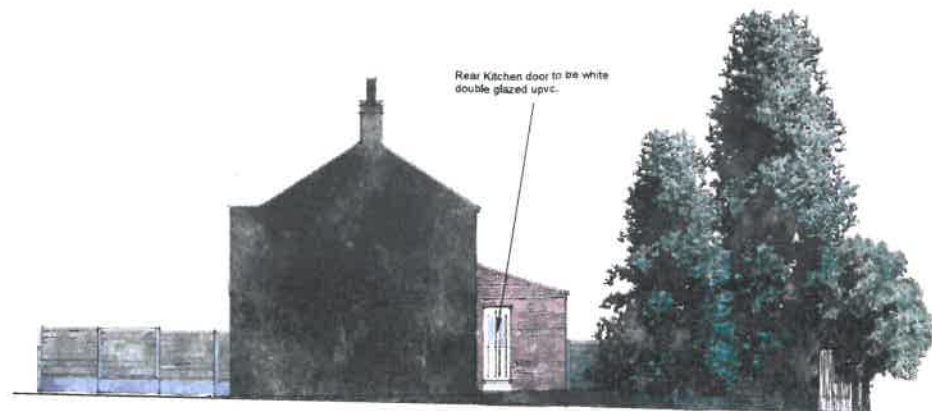
As per 2nd consultation and recommendation by Senior Planning Officer, Ms Jennie Townsend, window 1 has been made high level so as not to overlook neighbouring windows 3 & 4.

Please note the rear property has benefitted from 2No. previous rear extensions culminating in the current neighbouring build massing. It is also noted that these builds do not comply with current or previous planning guidelines relating to distance to boundary and also do not comply to current permitted development rules.

SCALE 1:100
0 5000 10000



VIEW A. LEFT HAND SIDE ELEVATION (extension elevation only) 1:100



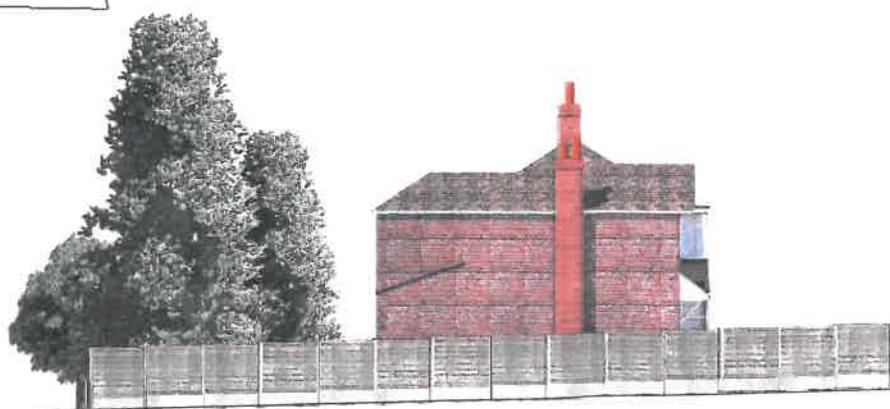
VIEW B. RIGHT HAND SIDE ELEVATION (extension elevation only) 1:100



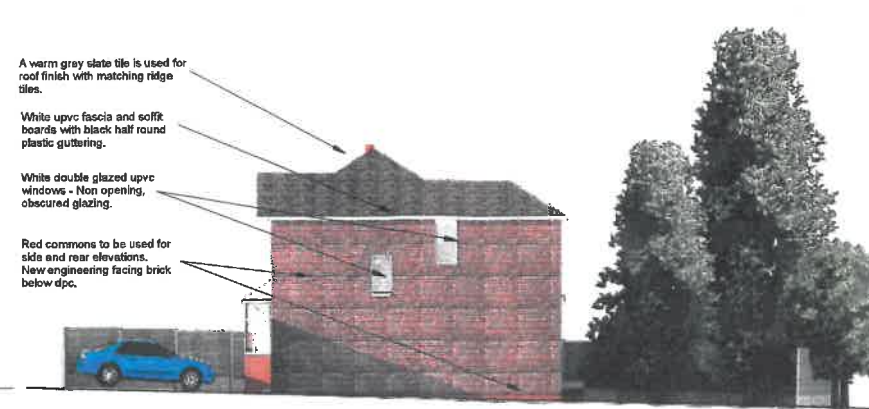
FRONT ELEVATION 1:100



REAR ELEVATION (extension and new build) 1:100



VIEW D. LEFT HAND SIDE ELEVATION (new build only) 1:100



VIEW C. RIGHT HAND SIDE ELEVATION (new build only) 1:100

NOTES:
All dimensions must be checked on site and not scaled from this drawing.
All construction works must comply with British Standards & Building Control Regulations as well as being approved by the supervising Building Control Officer.

Rev	Date	By	Rev
A	03/02/2017	Chris Wardman	1



5 Langdale Road, Sale, Manchester M23 4EN
Tel: 0161 962 5579

Client: Mr P. Whitteman

Job Title:

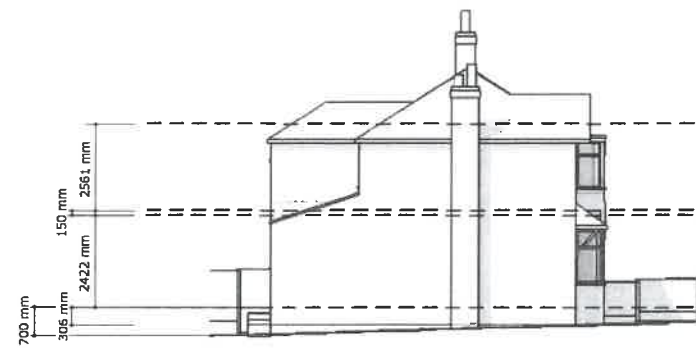
PLANNING APPLICATION DRAWINGS
NEW BUILD & EXTENSION
48 MERTON ROAD
PRESTWICH
MANCHESTER M25 1PL

PLANS & ELEVATIONS

Scale: 1:100

Date: 03/02/2017 Drawn by: D. White

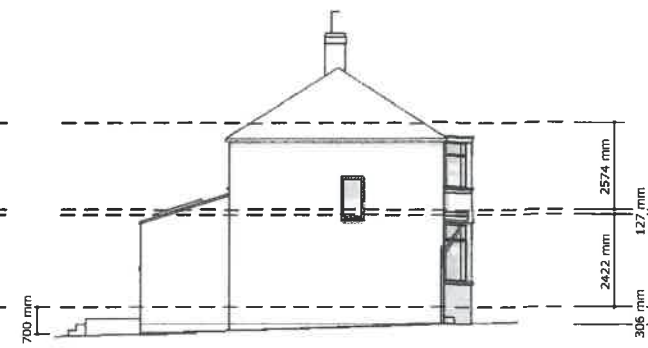
Drawing No: P3 / 715079 / 02 / Rev A



LEFT HAND SIDE ELEVATION (new build) 1:100



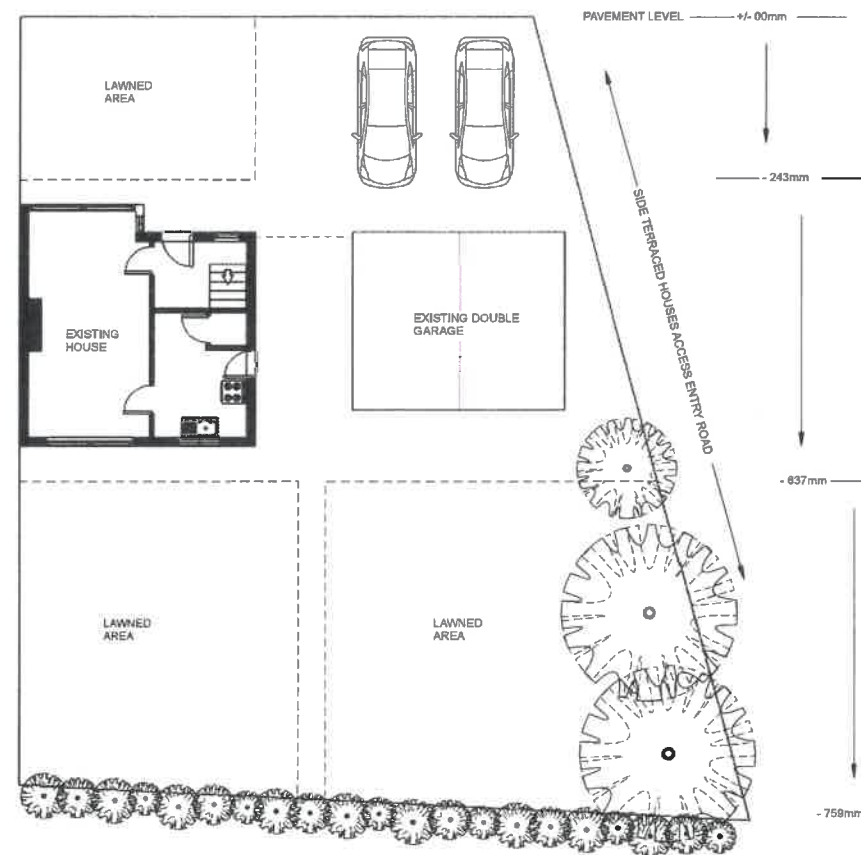
FRONT ELEVATION 1:100



LEFT HAND SIDE ELEVATION (rear extension) 1:100



REAR ELEVATION 1:100



EXISTING LEVELS SITE / GROUND FLOOR PLAN 1:100

NOTE:
AS SHOWN ON ALL SUPPLIED APPLICATION DRAWINGS AND ASSOCIATED PLANNING APPLICATION FORMS - NO TREES ARE TO BE REMOVED AS PART OF THIS APPLICATION.

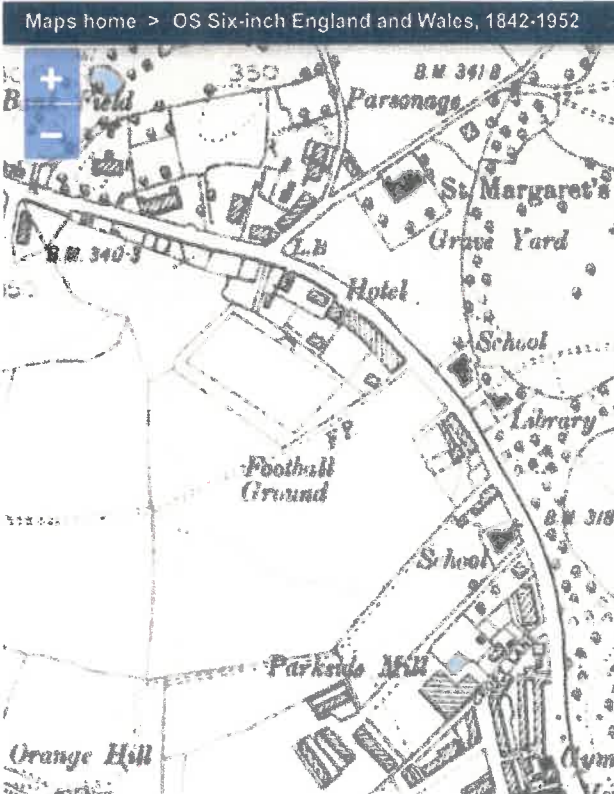
SITE WAS ORIGINALLY AGRICULTURAL GROUND. LATER IN THE 1800'S USED AS A FOOTBALL FIELD CHANGING TO RECREATIONAL LAND IN THE EARLY 1900'S PRIOR TO THE CONSTRUCTION OF DOMESTIC DWELLINGS IN 1934.

MINING SURVEYS HAVE REVEALED THE LAND IS NOT SUBJECT TO SUBSIDENCE OR LAND HEAVE IN THE IMMEDIATE AREA.

LAND AS INFORMED IS NON CONTAMINATED DOMESTIC LAND WITH NO EVIDENCE OF CONTAMINATION.

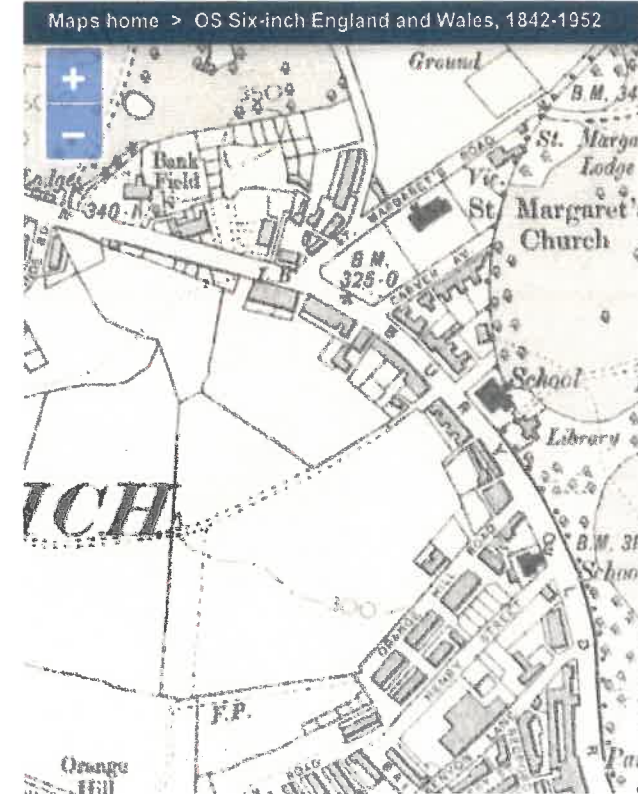
LEVELS
Datum for site levels taken existing finished pavement level and is somewhat linear from front to rear of site. Site levels correspond to the same levels as side terraced houses access entry road.

Lancashire XCVI.SW (includes: Manchester; Prestwich; Salford.)
Surveyed: 1889 to 1891
Published: 1894



SITE PLAN 1 (for reference only) N.T.S.

Lancashire XCVI.SW (includes: Manchester; Prestwich; Salford.)
Revised: 1906 to 1907
Published: 1910



SITE PLAN 2 (for reference only) N.T.S.

NOTES:
All dimensions must be checked on site and not scaled from this drawing.
All construction work must comply with British Standards & Building Control Regulations as well as being approved by the supervising Building Control Officer.

A		03/02/2017	Consent amendment, new build complete redesign.
Date			Revised
Copyright:			
5 Langdale Road, Salford, Manchester M3 4EN		Tel: 0161 962 8578	
Client:		Mr P. Whiteman	
Job Title:		PLANNING APPLICATION DRAWINGS	
		NEW BUILD & EXTENSION	
		48 MERTON ROAD	
		PRESTWICH	
		MANCHESTER M25 1PL	
Drawing Title:		LEVELS PLAN & ELEVATIONS	
Scale:		1:100 & N.T.S.	
Date:		03/02/2017	Drawn by: D. White
Drawing No.:		P3 / 715079 / 04 / Rev A	

SCALE 1:100
0 5000 10000

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 05

Applicant: Excelsior Properties

Location: Bast House Farm Barns, Manchester Road, Ramsbottom, Bury, BL9 5LZ

Proposal: Conversion of two redundant agricultural outbuildings to create 6 no. dwellings

Application Ref: 60870/Full

Target Date: 02/02/2017

Recommendation: Approve with Conditions

Description

The application relates to a redundant farm complex immediately to the east of Bast House and Bast House Farm Cottage and to the west of Bast House Farm. The site is to the south east of Gollinrod, next to the junction 1 of the M66 between Bury and Ramsbottom. The site is surrounded by open countryside and there is a small lodge to the south-east.

The buildings to be converted have two distinct built elements separated by a cobbled courtyard. The larger building on the north side of the site has evolved with a mix of different stone and brick forms and is characterised by larger relatively open barns with three distinct gables on the north-east elevation. The building on the south side of the farmyard is a smaller, more enclosed stone built barn. A structural survey was submitted and confirmed that the buildings are substantial and capable of conversion but they are in a relatively poor state of repair.

The site is accessed from a private track off Manchester Road which loops around the site and runs back onto Manchester Road at the same point.

The site lies within the Green Belt and Area of Special Landscape. Bast House Farm Cottage, to the west, is a Grade 2 listed building and a Public Right of Way (PRoW) (229) runs through the site west to east. The location plan shows the routes of the PRoW in and around the site.

The proposal involves converting the existing buildings into six dwellings, five within the larger barn complex on the north side, and a single dwelling in the barn on the south side. The vast majority of the built form would be retained although there would be a small amount of demolition and rebuilding, most notably the single storey outrigger on the east elevation.

The existing farm track would serve the proposed dwellings. A new farm track would extend off the existing road and loop around the south side of the development for the dual purpose of allowing vehicular access to the detached dwelling in the barn and in order to facilitate the redirected public right of way running through the site. The detached house would have a private garden area on its south side.

A survey and method statement with recommendations relating to bats, owls and other protected species has been submitted with the application. The survey confirmed the presence of both bats and owls, the consequence of which has been addressed in the Ecology section below.

Drainage would be dealt with by a private water treatment plant and a soakaway within the applicants land.

Relevant Planning History

None relevant.

Publicity

Site notice posed 09/12/16. Press notice in Bury Times 15/12/16. The following neighbouring properties were notified by letter dated 09/12/16. Bast House Farm Cottage, Bast House, Bast House Cottage, Higher Gollinrod, Westgate, 323,325, 327,329, 339, 341 and 390 Manchester Road.

Representations from residents at 327 and 341 Manchester Road.

- There are no rights of access along the existing private access road for the new dwellings.
- The construction traffic and traffic generated by the new dwellings would make surroundings roads less safe.

Those making representations have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No comment to date

Drainage Section - No objection

Environmental Health - No objection.

Public Rights of Way Officer - No objection

United Utilities - No objection.

Greater Manchester Ecology Unit - No objection.

Fire Service - No objection.

Unitary Development Plan and Policies

OL1/4	Conversion and Re-use of Buildings in the Green Belt
EN9/1	Special Landscape Areas
EN1/1	Visual Amenity
EN2/3	Listed Buildings
EN7	Pollution Control
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7/5	Waste Water Management
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/4	Conversions
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
RT3/4	Recreational Routes
NPPF	National Planning Policy Framework
SPD 9	Conversion and Re-use of Buildings in the Green Belt
HT2	Highway Network
SPD11	Parking Standards in Bury

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - OL1/4 - Conversion and Re-use of Buildings in the Green Belt states that the conversion and re-use of buildings in the Green Belt is not inappropriate development

and will be permitted providing that:

- a) it does not have a materially greater impact than the present use on the openness of the Green Belt and the purposes of including land in it;
- b) any extension of re-used buildings, and any associated uses of land surrounding the building do not conflict with the openness of the Green Belt and the purposes of including land in it.
- c) the buildings are of permanent and substantial construction, and are capable of conversion without major or complete reconstruction;
- d) the form, bulk and general design of the buildings are in keeping with their surroundings;
- e) suitable access and likely traffic generation can be accommodated without creating a traffic hazard or the need for major road improvements or lengthy new routes;
- f) all the necessary services can be provided without extensive works;
- g) it can be established by the applicant, to the satisfaction of the local planning authority, whether or not the building contains any protected species, such as bats or barn owls. Where it is shown such species are present, measures to prevent damage to habitats will be required.

SPD9 Conversion and Re-use of Buildings in the Green Belt sets out relevant guidance and assessment criteria.

Impact on the Openness of the Green Belt - The existing buildings would not be significantly extended beyond their existing form and the areas taken for rear domestic garden provision would not be excessive or disproportionate to the development. The new single storey outrigger on the east elevation would replace a larger outrigger and there is a small infill section between buildings on the north side. The areas proposed for external car parking would be within forecourt areas immediately adjacent to the buildings and the detached timber car port on the north east side of the development would be small in scale and related to the existing building complex. Whilst the complex of buildings sits within the open countryside, given the limited external alterations beyond new window and door openings, it is considered that there would be no material impact on the openness of the Green Belt. The proposal therefore complies with UDP Policy OL1/4 Conversion and Re-use of Buildings in the Green Belt.

Structural Integrity - A structural report from Booth King Partnership Limited and dated May 2015 was submitted with the application. The barn complex is substantial and relatively robust and therefore capable of conversion without significant reconstruction. As the buildings do have some historical and architectural merit the retention of the majority of the existing structures through the conversion would be welcomed.

Layout, Design and Appearance - The external treatment proposed, with limited extensions and new openings and the use of traditional materials, would ensure that the rural character of the buildings would be maintained. Infilling of larger openings for the large cart doors would be sensitively handled with appropriate glazing or timber cladding. Any consent, however, would include conditions to ensure that the approved details would be adhered to and that the precise detailing of important external elements would be properly controlled. In order to retain control of any future alterations and extensions in what is a sensitive location, a condition removing permitted development rights is considered appropriate.

Residential Amenity - Given the position of neighbours, there are no serious residential amenity concerns arising from the development.

Access and Parking - The existing access road from Manchester Road would be utilised. The access would run around the north and south of the development and there would be a turning head on the east side. There would be parking spaces for 12 cars, 6 within the courtyard area and 6 under two car ports. The proposed parking provision is acceptable and complies with UDP policy and guidance. The traffic generated by the six dwellings, whilst different to that generated by the farm complex, would not have a seriously detrimental impact on highway safety.

The new unmade access track running around the garden on the south side would accommodate the rerouted Public Right of Way that runs through the development site and also allows access to the car port on the east side of the detached dwellinghouse. An appropriate condition would ensure that this track is constructed with an appropriate permeable stone surface and fencing.

The proposed access and parking arrangements are acceptable and complies with UDP Policies H2/2, HT2/4 and associated guidance documents on conversion of buildings and parking.

Ecology - The ecological survey submitted with the application confirmed that the buildings have been used by bats and Barn Owls. The bat roosts were day roosts, as opposed to hibernation roosts, in the buildings these where for common and widespread species (Common Pipistrelle).

Under the terms of the Habitats Directive and the Conservation of Habitats and Species Regulations 2010, a licence may be required from Natural England. Before a licence can be granted three tests must be satisfied. These are:

i) That the development is “in the interest of public health and public safety, or for other imperative reasons of overriding public interest, including those of a social or economic nature and beneficial consequence of primary importance for the environment”;

ii) That there is “no satisfactory alternative”;

iii) That the derogation is “not detrimental to the maintenance of the populations of the species concerned at a favourable conservation status in their natural range”.

All three tests must be satisfied before planning permission is granted on a site. The first two tests are essentially land-use planning tests.

With regard to the first test, the Local Planning Authority is satisfied that the proposed conversion and redevelopment of the disused and derelict barn complex would benefit the local economy, add to the local housing stock and help preserve the buildings within their historic landscape setting.

With regard to the second test, given the nature of the proposal and location, there is no satisfactory alternative available.

As regards the third test, it is noted that the roost found is small, temporary and of a relatively common bat species. Compensation for any possible disturbance to bats will be straight forward and a Construction Environmental Management Plan (Biodiversity) would be attached as a condition of approval.

Greater Manchester Ecology Unit have no objections to the application on nature conservation grounds.

Drainage - Foul drainage would be dealt with by a private water treatment plant and surface water run off would be dealt with by a soakaway. An appropriate condition requiring details to be submitted prior to occupation would be attached to any approval notice

Objections - The concern of the resident, who owns part of the access road to the site, with regard to access rights, is a private matter and not a material planning issue. The correct ownership certificate and notices were served on all the owners of the site. The concerns about construction traffic would be dealt with by a suitable condition requiring a Construction Management Plan to be submitted. The impact of the traffic generated by future residents has been addressed in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to Location Plan and drawings numbered 001/D, 002, 004/Q, 005/E, 006/E, 007/C, 008/E, 010, 012A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Sample panels of brickwork, stonework and mortar and paving, demonstrating the colour, texture, face bond and pointing, not less than 1 sq.m in size, shall be erected on site for inspection, and approval in writing, by the Local Planning Authority prior to the commencement of the development. Samples of the roofing materials shall also be made available for inspection on site. Thereafter the development shall be constructed in the approved materials and manner of construction.
Reason. Samples have not been submitted and In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/1 Visual Amenity of Bury Unitary Development Plan.
4. Prior to the commencement of development, details relating to the proposed boundary treatment for the site shall be submitted to and approved in writing by the Local Planning Authority. The approved details only shall be implemented as part of the approved development.
Reason - Details have not been submitted and to secure the satisfactory development of the site and in the interests of the visual amenities of the area pursuant to Policy EN1/1 Visual Amenity of Bury Unitary Development Plan.
5. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.
6. No development shall take place (including demolition, ground works, vegetation clearance) until a construction environmental management plan (CEMP: biodiversity) has been submitted to and approved in writing by the local planning authority. The CEMP (Biodiversity) shall include the following.
 - a) Risk assessment of potentially damaging construction activities.
 - b) Identification of "biodiversity protection zones".
 - c) Practical measures (both physical measures and sensitive working practices) to avoid or reduce impacts during construction (may be provided as a set of method

statements).

- d) The location and timing of sensitive works to avoid harm to biodiversity features.
- e) The times during construction when specialist ecologists need to be present on site to oversee works.
- f) Responsible persons and lines of communication.
- g) The role and responsibilities on site of an ecological clerk of works (ECoW) or similarly competent person.
- h) Use of protective fences, exclusion barriers and warning signs.

The approved CEMP shall be adhered to and implemented throughout the construction period strictly in accordance with the approved details.

Reason. No details have been submitted and in the interests of ecology and protected species pursuant to the NPPF and UDP Policy EN6 Conservation of the Natural Environment.

7. Prior to occupation, a "lighting design strategy for biodiversity" for areas and buildings to be externally lit shall be submitted to and approved in writing by the Local Planning Authority. The strategy shall:
 - a) identify those areas/features on site that are particularly sensitive for bats and barn owls and that are likely to cause disturbance in or around their breeding sites and resting places or along important routes used to access key areas of their territory, for example, for foraging; and
 - b) show how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb or prevent the above species using their territory or having access to their breeding sites and resting places.All external lighting shall be installed in accordance with the specifications and locations set out in the strategy, and these shall be maintained thereafter in accordance with the strategy. Under no circumstances should any other external lighting be installed without prior consent from the Local Planning Authority.

Reason.
8. Prior to the commencement of development an historic/architectural survey of the buildings, including a photographic record, shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. To allow for the proper recording of archaeological evidence before its disturbance by the works hereby approved pursuant to the NPPF and UDP Policy EN3 Archaeology.
9. Detailed drawings of how the new/ replacement windows/rooflights and doors would be installed within the development, at no less than 1:20 scale, shall be submitted to and approved in writing by the Local Planning Authority before any development is commenced.

Reason. Details sections of windows/doors has not been submitted and to ensure a satisfactory form of development in the interests of visual amenity pursuant to UDP Policies EN1/1 Visual Amenity and OL1/4 Conversion and Re-use of Buildings in the Green Belt and SPD9 Conversion and Re-use of Buildings in the Green Belt.
10. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed

Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

11. Following the provisions of Condition 10 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

12. No development shall commence unless and until a Preliminary Risk Assessment report to assess the actual/potential ground gas / landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority.

- Where actual/potential ground gas/landfill gas risks have been identified, a detailed site investigation(s), ground gas monitoring and suitable risk assessment(s) shall be submitted to, and approved in writing by the Local Planning Authority;
- Where remediation / protection measures are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. To alleviate any possible risk associated with the production of landfill gas and ground gas in accordance with the recommendations of the Environment Agency and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

13. No development approved by this permission shall be occupied until a scheme for the disposal of foul and surface waters has been submitted to and approved by the Local Planning Authority. Such a scheme shall be constructed and completed in accordance with the approved plans.

Reason. Drainage details have not been submitted and to ensure a satisfactory means of drainage pursuant to the NPPF and UDP Policy EN7/5 Waste Water Management.

14. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the extension hereby approved being brought into use.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 Car Parking and New Development of the Bury Unitary Development Plan.

15. Prior to the commencement of development details of roadworks and footpaths, including the rerouted Public Right of Way indicated on the layout plan 012A shall be submitted to, and approved by, the Local Planning Authority. The approved works shall be implemented before the development is brought into use.

Reason. Details have not been submitted and to ensure good highway design in the interests of road safety pursuant to UDP Policy HT6/2 pedestrian/Vehicular Conflict.

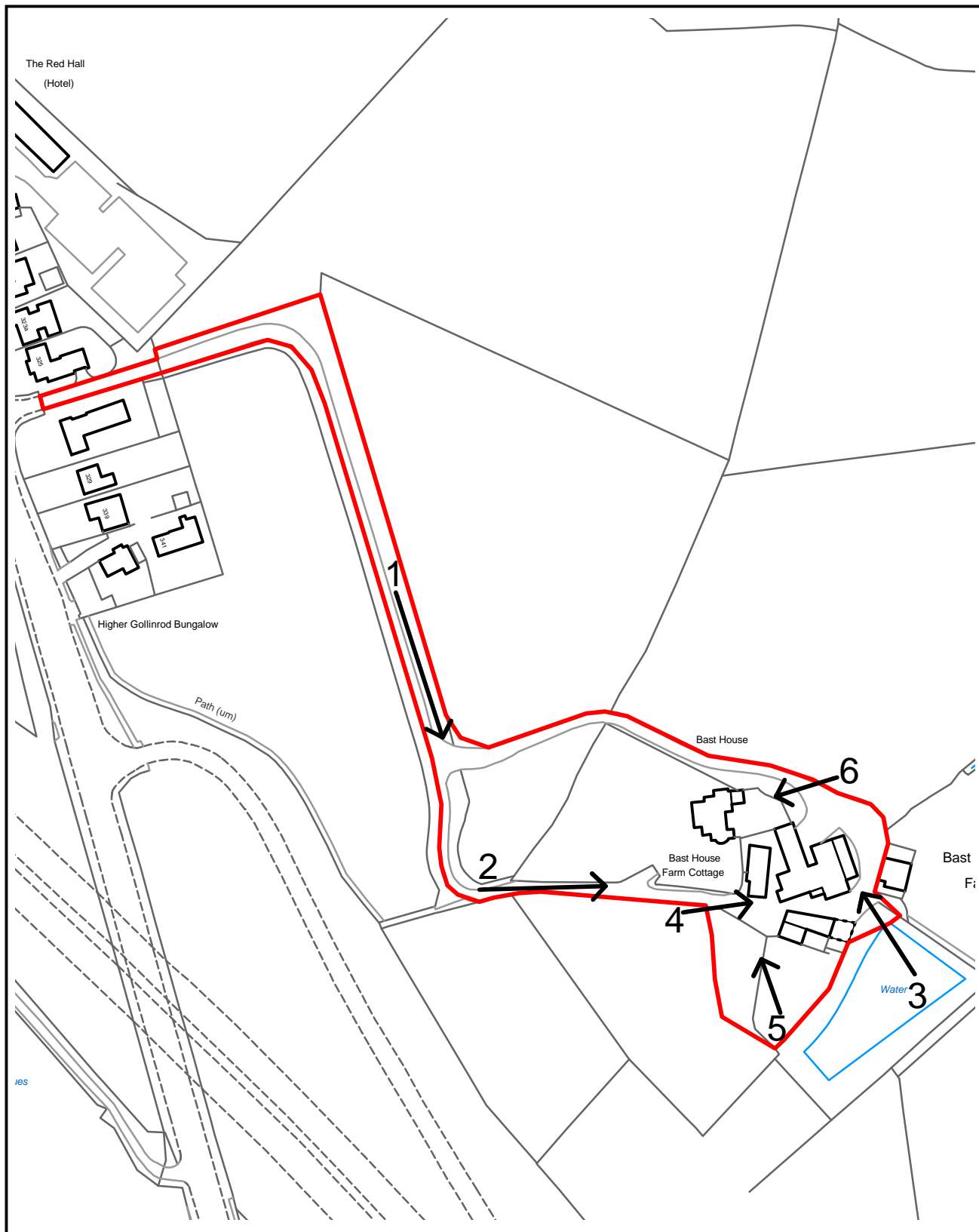
16. No development shall commence unless and until a 'Construction Management Plan' (CMP), has been submitted to and agreed in writing with the Local Planning Authority and shall provide the following:
- Hours of operation and number of vehicle movements;
 - Parking on site or on land within the applicant's control of operatives' and construction vehicles together with storage on site of construction materials.
 - Details of measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to UDP Policy H2 Highway Network.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60870

**ADDRESS: Bast House Farm barns
Manchester Road, Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60870

Photo 1



Photo 2

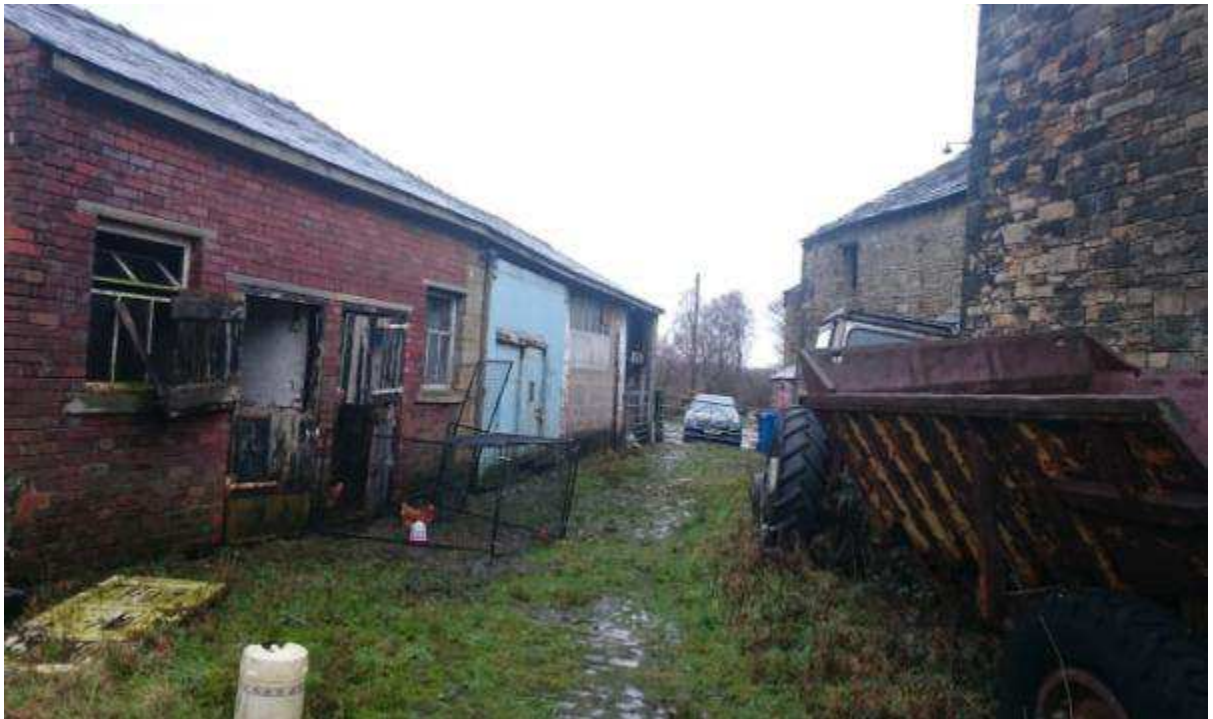


60870

Photo 3



Photo 4



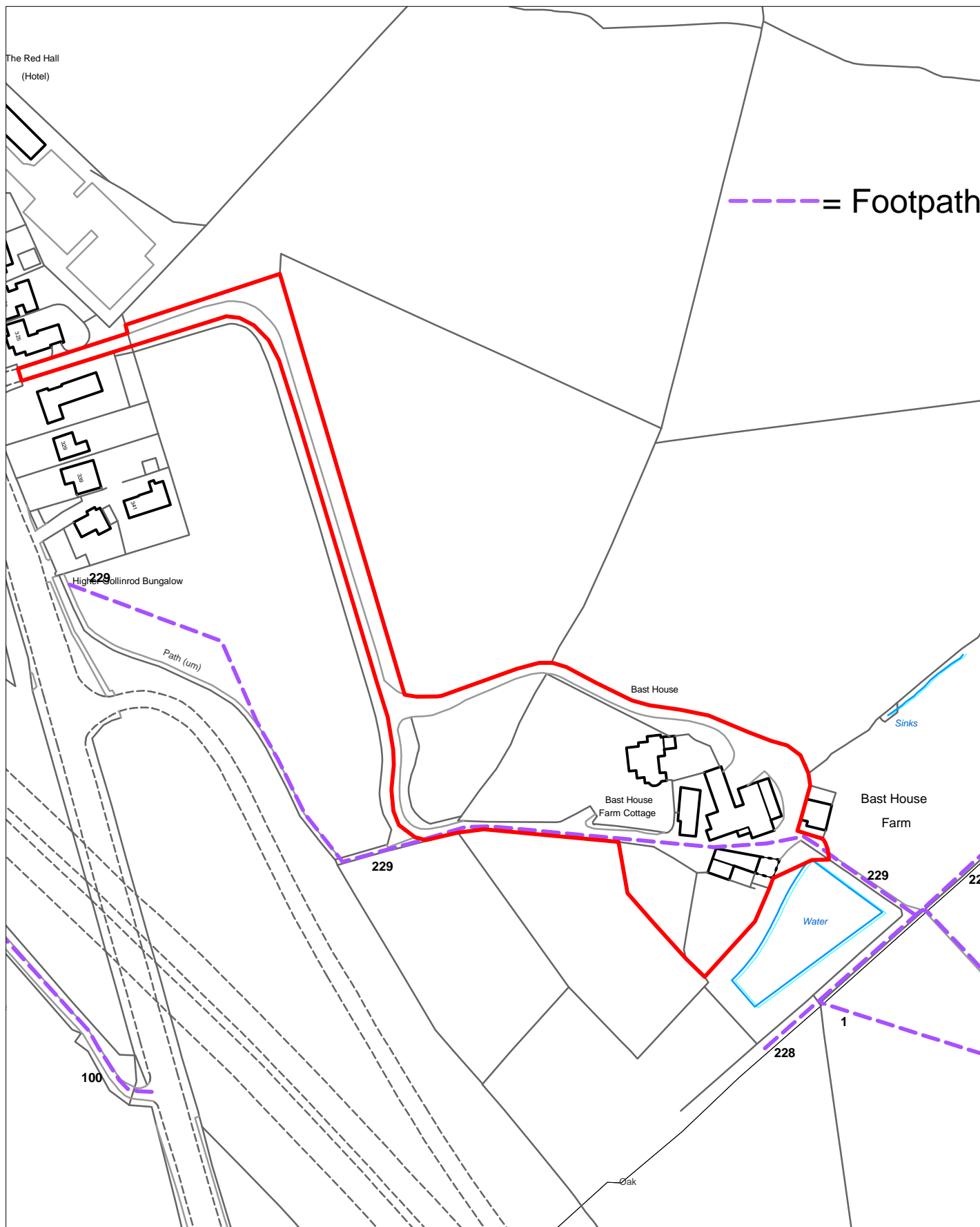
60870

Photo 5



Photo 6





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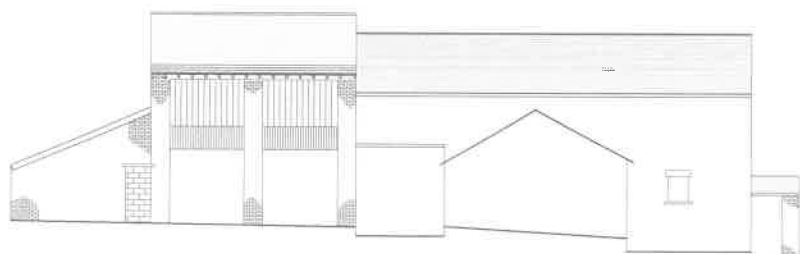
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Bury
 COUNCIL

Notes

1. All drawings are to be read in conjunction with the specification and the contract documents.
2. The client is responsible for providing accurate information and for obtaining all necessary permissions.
3. The architect is not responsible for the construction of the building or for the safety of the building.
4. The architect is not responsible for the design of the building or for the safety of the building.
5. The architect is not responsible for the design of the building or for the safety of the building.



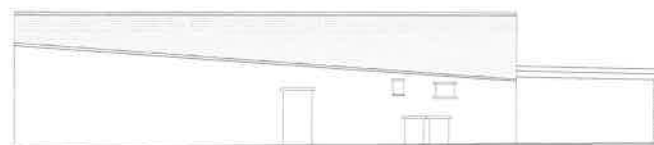
Main Building North Elevation @ 1:100



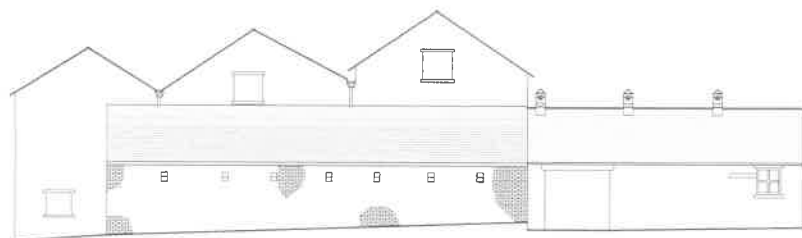
Detached Building East Elevation @ 1:100 Detached Building West Elevation @ 1:100



Main Building South Elevation @ 1:100



Detached Building South Elevation @ 1:100



Main Building East Elevation @ 1:100



Detached Building North Elevation @ 1:100



Main Building West Elevation @ 1:100



Jennings Design Associates

Architect	Project Manager
Designer	Client
Consultant	Contract
Author	Drawn
Checked	Scale

Architects Designers Project Managers
The Warehouse, Saxon Street, Denton, Manchester M34 3DS

Excelsior Properties

Ball House Farm, Bury

798 A 02 Existing Elevations

Notes

Jennings Design Associates take no responsibility for any alterations, omissions or inaccuracies from this document. The drawings are intended for use as a guide only and are not to be used for construction purposes. The drawings are intended for use as a guide only and are not to be used for construction purposes. The drawings are intended for use as a guide only and are not to be used for construction purposes.



Roof Plan @ 1:500

Rev	Description	Date
1	Initial Design	21.04.17
2	Revised Design	21.04.17
3	Final Design	21.04.17

Jennings Design Associates

C	Concept	C	Client
LC	Planning	C	Construction
E	Building Regs	E	As Built
P	Production	P	Section

Architects Designers Project Managers
The Warehouse, Saxon Street, Denham,
Manchester M34 3DS

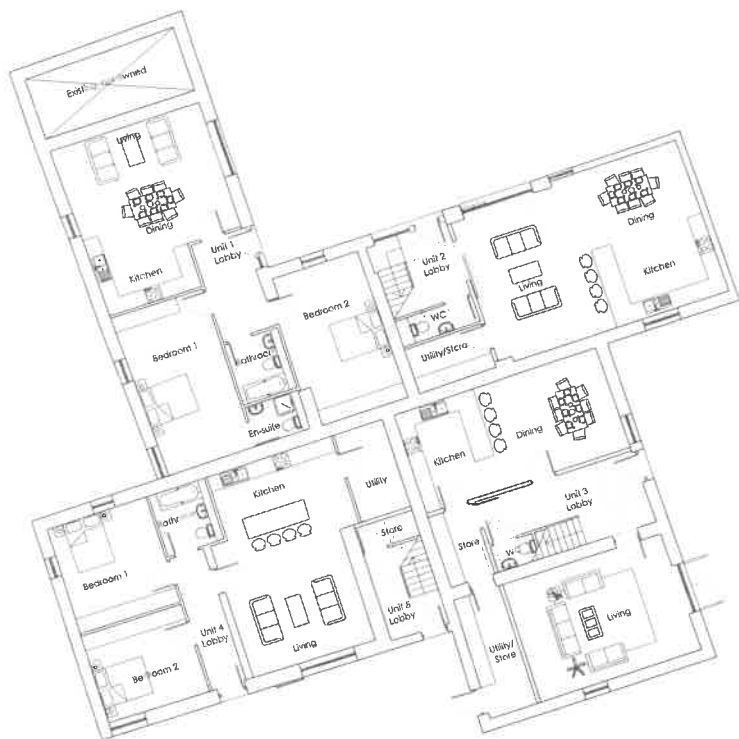
Excelsior Properties

Ball House Farm, Bury

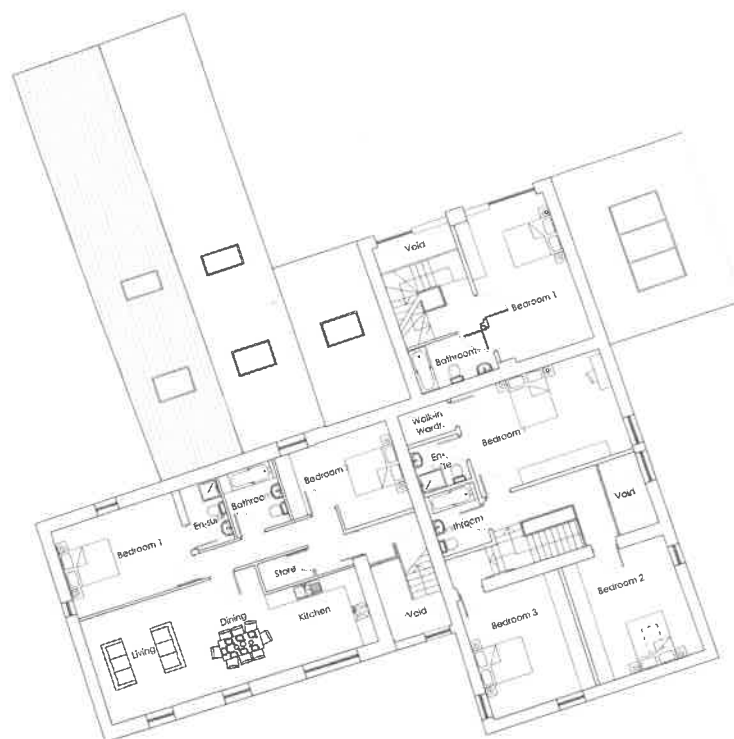
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798 A 1:5 Proposed Plans Ground and 1st

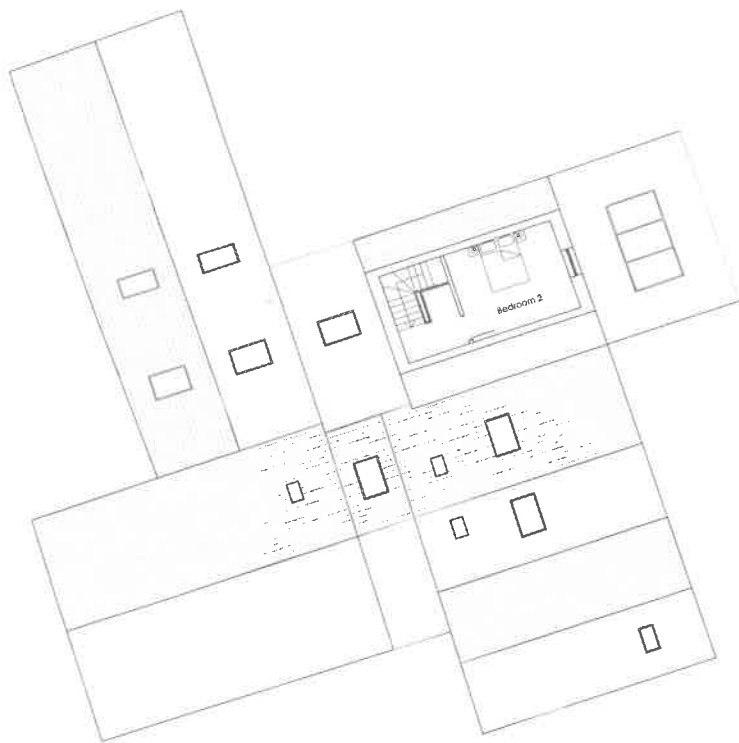
2018-2019 House Farm Design & Construction Ltd



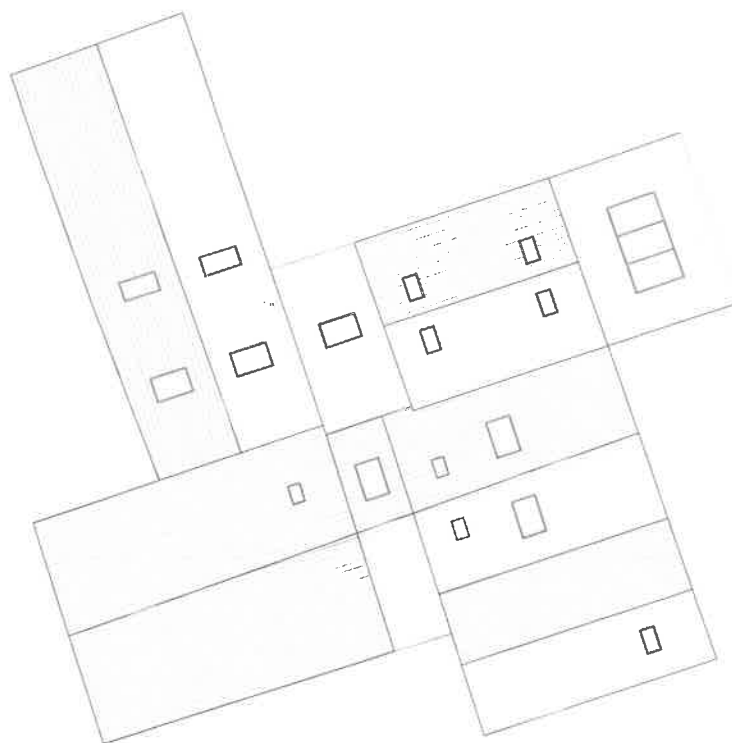
Ground Floor Plans @ 1:100



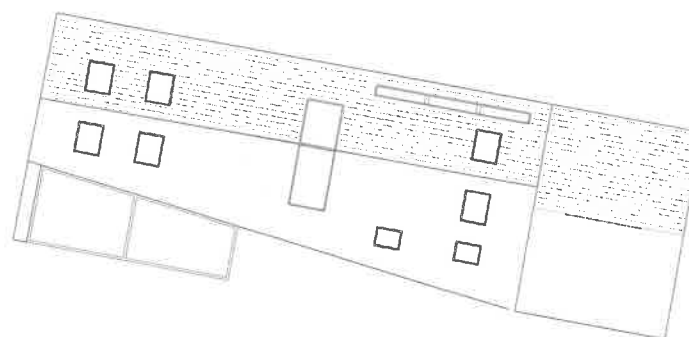
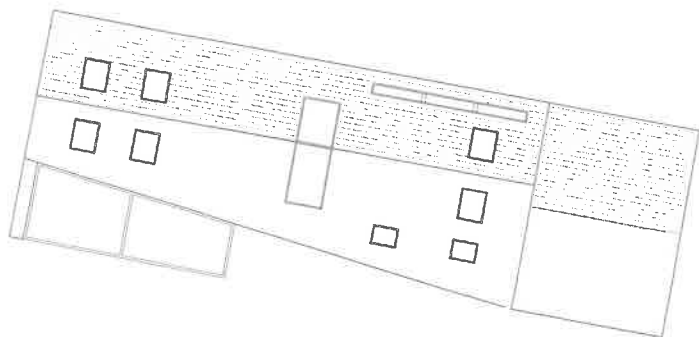
First Floor Plans @ 1:100



Second Floor Plans @ 1:100



Roof Plans @ 1:100



Notes

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Roof Plan @ 1:500

Quantity	Area (m2)	Unit
Roof Area	105.0	m2
Roof Area	146.9	m2
Roof Area	210.9	m2
Roof Area	97.6	m2
Roof Area	122.9	m2
Roof Area	208.9	m2

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C	Concept	G	General
F	Planning	C	Construction
B	Building	L	As Built
P	Production	S	Section

Architects Designers Project Managers
The Warehouse, Saxon Street, Denton,
Manchester M34 3DS

Excelsior Properties

Bail House Farm, Bury

VAR	AL	05.10.16	JH	RH
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798	A	6	Proposed Plans and roof	E
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Notes

1. All dimensions are in millimetres unless otherwise stated.

2. The architect's name and address are given in the title block.

3. The architect's telephone and fax numbers are given in the title block.

4. The architect's e-mail address is given in the title block.

5. The architect's website is given in the title block.

6. The architect's professional registration number is given in the title block.

7. The architect's professional registration body is given in the title block.

8. The architect's professional registration number is given in the title block.

9. The architect's professional registration body is given in the title block.

10. The architect's professional registration number is given in the title block.

11. The architect's professional registration body is given in the title block.

12. The architect's professional registration number is given in the title block.

13. The architect's professional registration body is given in the title block.

14. The architect's professional registration number is given in the title block.

15. The architect's professional registration body is given in the title block.

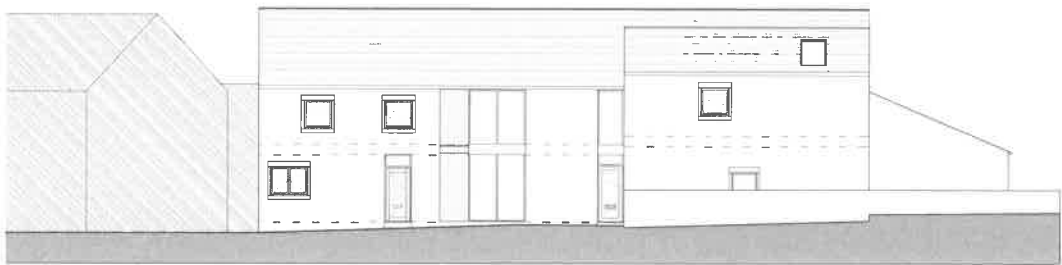
16. The architect's professional registration number is given in the title block.

17. The architect's professional registration body is given in the title block.

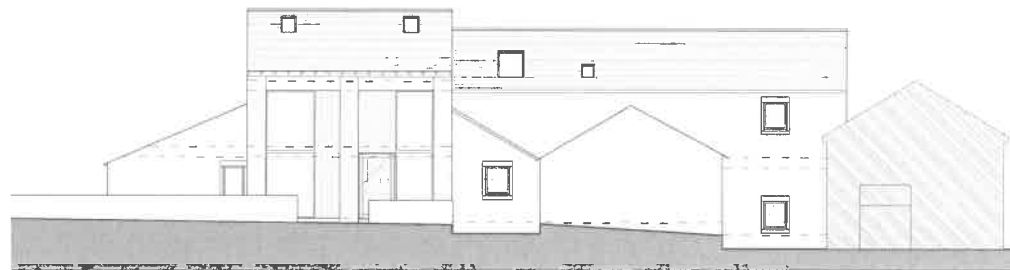
18. The architect's professional registration number is given in the title block.

19. The architect's professional registration body is given in the title block.

20. The architect's professional registration number is given in the title block.



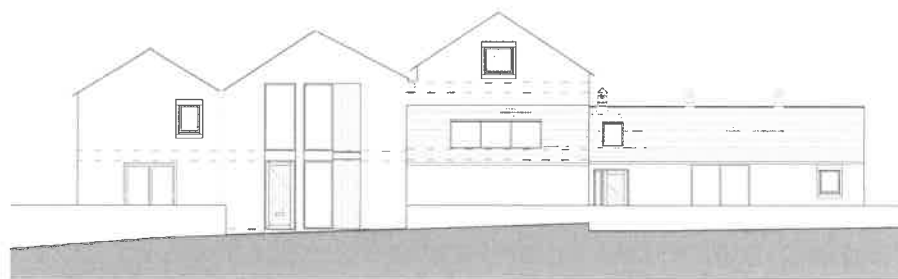
South Elevation @ 1:100



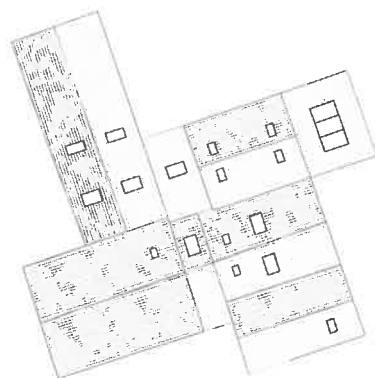
North Elevation @ 1:100



West Elevation @ 1:100



East Elevation @ 1:100



Roof Plan @ 1:200



Jennings Design Associates
ARCHITECTS

1. Client	2. Project	3. Date
4. Location	5. Scale	6. Drawing
7. Author	8. Check	9. Date

Architects Designers Project Managers
The Warehouse, Saxon Street, Denton
Manchester M34 3DS

Excavator Properties

Boat House Farm, Bury

VAR	A1	05.10.16	Gravel	JH	RH
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798	A1	Prop. Apartment Blobs
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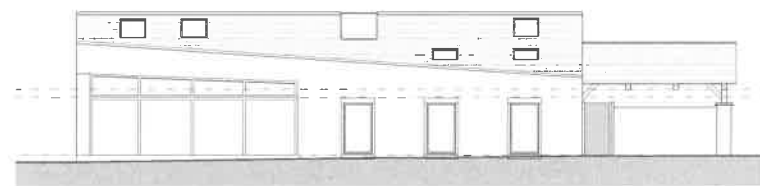
1:100 East Elevation, 05.10.16, 1:100 North Elevation, 05.10.16, 1:100 South Elevation, 05.10.16, 1:100 West Elevation, 05.10.16, 1:200 Roof Plan, 05.10.16

Notes

1. All dimensions are in millimetres unless otherwise stated.
2. The architect shall be responsible for obtaining all necessary planning and building regulations approvals.
3. The architect shall be responsible for obtaining all necessary planning and building regulations approvals.
4. The architect shall be responsible for obtaining all necessary planning and building regulations approvals.
5. The architect shall be responsible for obtaining all necessary planning and building regulations approvals.



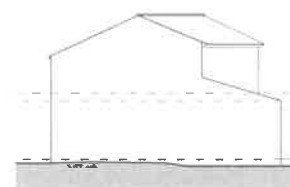
South Elevation @ 1:100



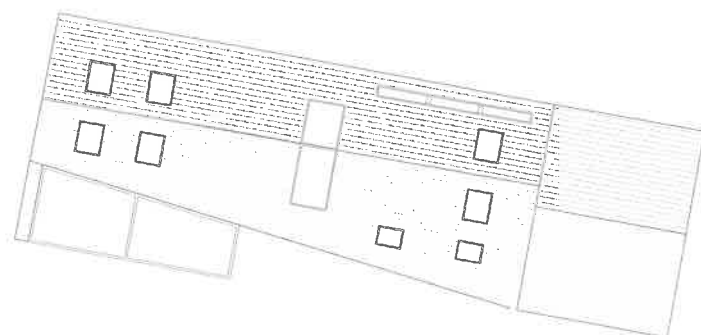
North Elevation @ 1:100



West Elevation @ 1:100



East Elevation @ 1:100



Roof Plan @ 1:200

Jennings Design Associates

Architects Designers Project Managers

The Warehouse, Saxon Street, Denton, Manchester M34 3DS

Excelsior Properties

Bail House Farm, Bury

1:100 A1 1:5 1:10 1:50 1:100

798 A 008 Prop.Detached Elev E

23.798 - Bail House Farm, Bury, Prop.Detached Elev, A08, Prop.Detached Elev, 23.798

Ward: Whitefield + Unsworth - Pilkington Park

Item 06

Applicant: MERJE Limited

Location: Land at Bank Street, Whitefield, Manchester, M45 7JF

Proposal: Demolition of existing buildings and erection of two storey office (Class B1) building and associated car parking, hard and soft landscaping works and associated works

Application Ref: 60901/Full

Target Date: 03/02/2017

Recommendation: Approve with Conditions

Description

The site relates to a piece of land, located in Whitefield Town Centre, which was previously in a commercial and business use but has been unoccupied and vacant for a number of years. It accommodates two 1960's pre fabricated buildings which have fallen into a state of disrepair and are in poor condition.

Access to the site is via a mesh gate located to the south of the site and accessed from Bank Street which is an unadopted road. There is an area to service and park within the site although this is not currently formally demarcated.

The site is bounded by the Metrolink to the west and a commercial property and a residential dwelling to the north on Devon Avenue. Bank Street is to the east and runs directly adjacent to the boundary of the site, beyond which are flats in Silver Court, a 3 storey building which is set approximately 2m lower than the site. There is a row of parking between Bank Street and Silver Court which are specifically allocated for use by occupiers of the flats.

The application seeks the demolition of the two existing buildings and redevelopment of the site for a two storey office building. The building would be positioned in the north eastern area of the site and extend southwards adjacent to the boundary with Bank Street. It would be 28.6m in length, 11.6m wide and to a height of 7.8m. Over two floors, the building would provide 613 sqm of office space.

The building would have a contemporary and modern external appearance, with grey brickwork, intermittent coloured panelled and recessed symmetrical windows set back within the fenestrations. The main entrance would be located on the southern elevation leading to a lobby and glazed stairwell. The main office windows would be on the western elevation overlooking the carpark area which is proposed to serve employees and visitors. A total of 17 parking space would be provided which would be laid out and formally demarcated together with a parking stand for 8 cycles. A pedestrian path would be provided around the building from the car park and a secondary entrance to the building located on the north western elevation.

Pedestrians would access the site by the existing set of steps from Bank Street and improvements would be made to the footpath and highway to facilitate a safer route into the site. Vehicular access would be from the existing gated entrance from Bank Street.

The application states that up to 50 employees would be employed.

Relevant Planning History

01737/E - Proposed demolition of existing buildings and erection of a two storey office building (5,500 sq ft), car parking and associated works.

Publicity

54 letters sent on 12/12/2016 to properties at Nos 1-18 Silver Court, 231-271 (odds) Bury New Road; 12, 14, 16, 18, 18A, 20A, 20, Church Lane, 1, 2, The Bungalow, Sunnybank House, Devon Avenue, Units 1,2,3, Dixons Upholstery, 1, Flat 1 and 2, 1 Bank Street.

Site notice posted 22/12/16

Press advert in the Bury times 15/12/2016.

5 letters of objection received from Nos 12, 18A Church Lane, 1 Devon Avenue, 11 Bank Street, Flat 17 Silver Court with the following issues:

- Pressure on the lane with additional traffic will cause more damage to the existing surface which is damaged already and difficult and unsafe to negotiate with a pram, wheelchair or pedestrians;
- Bank Street is unadopted and very stressed due to increase in traffic and parking for Porada and The Garrick - the state of the road and access to it will worsen;
- Increase in parking problems - access is often blocked by illegal parking; access difficult for emergency vehicles;
- Bank Street is unsuitable for this type of development - parking restrictions should be put in place along Bank Street and Back Church Street to protect existing residents;
- Devon Avenue is often blocked by illegal parking, and made busier by Swift cars taxis using the road - it is unsafe particularly for pedestrians;
- The proposed parking layout is not workable - cars would have to reverse out of spaces onto Bank Street;
- A sub station is proposed adjacent to my boundary (1 Devon Avenue), which may block my view;
- The bin store would be a magnet for vermin;
- Consideration should be given to loss of light from the building;
- Construction traffic could not access the site unless there was significant widening and improvements to Bank Street - but if improvements were made, it would increase traffic making further deterioration of the area;

Letter of support received from Whitefield Garrick Theatre:

- This is the type of development needed to bring a scruffy, neglected corner of central Whitefield back into use and to create employment and footfall in the area;
- Applaud the Council's comments concerning the required upgrade to Bank Street/site access to Bank Street and the problem of parking on that road.
- Would comment though, that the photographs and area plans used in the application do not show our 2015 development adjacent to the site in question and in some cases also omit to show our 2001 development.

Revised plans received to include amendments to the red edge boundary, set back of the footprint of the building from Bank Street, reconfiguration of the car park to provide 17 spaces, re-siting of the bin store, sub-station elevations.

Neighbours re-consulted on the amendments by letter on 3/2/2017.

Further objection received from No 1 Devon Avenue with the following issues raised:

- Devon Avenue is a nightmare for both pedestrian and driver. There is parking on double yellow lines and double parking;
- According to the plans, there will be between 70/80 people working from the unit with only 17 parking spaces - where will the other drivers/vehicles park?;
- The amended plans show the building moved approx 1m to the west and the palisade fencing being erected along the railway embankment - how is it intended to plant trees between the existing fence and new palisade, or intended to plant within their boundary?;
- Three or four of the proposed spaces are impossible to drive out of the site unless reversing - would the council consider this safe?;

- Thought there would be a footpath located around the offices for pedestrian safety - one of the parking spaces would require the driver to reverse out of his space onto the pedestrian walkway;
- The application is a non starter - not enough space to achieve what they need;
- Do not want the car park lighting located in such proximity to my house.

Comment received from No Flat 3 Silver Court -

- Not objecting to the application, but the site plan shows the red edge boundary which looks to stop residents parking in designated bays and could restrict access to the garages behind Silver Court. Hope the contractors would not park in this area and that alternative provision would be made.

Comment received from No 9 Silver Court -

- Whilst development in the area is welcomed, Bank Street is in a state of disrepair due to seeing increased traffic without increased maintenance. Parking is an issue often blocking access due to inconsiderate parking, in particular at the restaurant end and taxi drivers by Silver Court. Surely any further development would exacerbate these issues further, causing increased traffic, increased parking (abuse of residents parking on Silver Court) both in long term and during construction;
- Any assurances about traffic control measures, prevent overflow parking and construction parking in the private space and the maintenance of the road would be greatly appreciated.

The objectors have been informed of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to conditions.

Drainage Section - A drainage condition incorporating the requirements of TfGM would be included.

United Utilities (Water and waste) - No objection subject to a condition to submit a drainage scheme for approval

Transport for Greater Manchester - No objection subject to conditions.

Environmental Health Contaminated Land - No objection subject to conditions.

Greater Manchester Ecology Unit - No objection subject to conditions and informatives.

Waste Management - The applicant is advised to contact the waste management section regarding refuse collection arrangements.

Greater Manchester Police - designforsecurity - Recommend lighting to car park areas and security measures to secure the site and building.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EC3/1	Measures to Improve Industrial Areas
EC4/1	Small Businesses
EC6/1	New Business, Industrial and Commercial
EN1/2	Townscape and Built Design
EN6/4	Wildlife Links and Corridors
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
S1/3	Shopping in District Centres
EN2	Conservation and Listed Buildings
EN6/3	Features of Ecological Value
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
EN1/5	Crime Prevention
EC3/1	Measures to Improve Industrial Areas
EN5/1	New Development and Flood Risk
HT4	New Development

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - At the heart of the NPPF is the presumption of sustainable development with economic, social and environmental gains to be jointly and simultaneously sought through the planning system.

UDP Policy EC3/1 - Measures to Improve Industrial Areas is concerned with improving older industrial areas and premises and encourages measures including the promotion of new development, good standards of design, reuse of derelict and vacant land and improve the visual appearance and environment of the area.

UDP Policy 6/1 - Assessing New Business, Industrial and Commercial Development expects development to be of a high standard of design and appearance and take account of the surrounding environment, amenity and the safety of employees, visitors and occupiers.

The site has been vacant for a number of years, has become neglected, in bad condition and occupied by two pre fabricated buildings which are also in a poor state of repair. The proposed development would utilise an existing employment site and proposes a high quality office scheme which would bring it back into a productive and positive use for the area. Improvements would be made to the appearance of the site and also bring about improvements to vehicular and pedestrian access.

The site already benefits from a business and commercial use and the proposed development would also occupy the site as a business use. The area is characterised by a mix of commercial, retail and residential uses and being in a town centre location, it would be ideally sited to contribute to the vitality and viability of the area.

As such, it is considered that the proposed development would be acceptable in principle and would comply with UDP Policies EC3/1, EC6/1 and EC1/2.

Layout - The two existing buildings would be demolished to facilitate the new building and associated parking and servicing requirements on the site. The proposed building would be positioned on part of the footprint of one of the existing buildings which is adjacent to Bank Street, in the north eastern part of the site. It would extend 28.6m along this part of the boundary of the site and be 11.6m in width.

The main entrance would be located on the southern elevation with a secondary entrance on the north west elevation of the building. A footpath would be provided around the building to enable safe pedestrian access around the site which would segregate areas from the car park and vehicular movement.

It is proposed to provide 17 parking spaces, 5 of which would be located adjacent to the Bank Street boundary in the southern part of the site with the remaining 12 spaces and an area for cycle parking located to the rear of the site. A sub station is also proposed at the rear of the site in the furthestmost western area, and a bin store would be located adjacent to the entrance` set back behind a set of gates.

The existing access from Bank Street would be utilised for vehicular traffic and the stepped access which is currently provided for pedestrians off Bank Street would be retained and this area of the site improved to provide a safer and alternative access for pedestrians.

The proposed layout demonstrates that the scale of the development could be accommodated without compromise to the surrounding area or impact on highway safety. The development would also allow for improvements to be made to the current pedestrian and vehicular access arrangements.

As such, it is considered that the proposed development and layout would be acceptable and would comply with UDP Policies EC3/1 - Measures to Improve Industrial Areas, EC6/1 - Assessing new Business, Industrial and Commercial Development and EN1/2 - Townscape and Built Design.

Design and appearance - The two storey building would be positioned adjacent to the boundary with Bank Street and would address the street scene to provide an important frontage. It would be two storey in height and similar to the scale and massing of other buildings in the locality including Silver Court directly opposite.

The development proposes a contemporary and modern design approach in terms of the appearance of the building, which would comprise grey brickwork and recessed aluminium window frames with intermittent coloured panels to form clean lines and fenestration patterns. Glazing would predominantly characterise the southern elevation which would be more visible to public views and which would also denote the main entrance to the building and add a different dimension to the appearance.

Surfacing materials of the car park and pedestrian areas around the building and site would distinguish the separate areas and landscaping is proposed to the front of the site along Bank Street and to the north and west boundaries to soften the development. Details would be required by a condition.

As such, it is considered that the proposed development would improve on the current state of the site which at the moment accommodates run down and fairly untidy looking buildings and land, which would be appropriate within the setting of a town centre and the nearby conservation area. The development would therefore comply with UDP Policies EC3/1 - Measures to Improve Industrial Areas, EC6/1 - Assessing New Business, Industrial and Commercial Development and EN1/2 - Townscape and Built Design.

Impact on the surrounding properties - Whilst there are no set standards in terms of separation distances between commercial developments and residential properties, SPD6 is used as a guide to assess proximity of new buildings and potential impacts or otherwise on amenity issues.

SPD6 states that a minimal distance of 20m should be maintained between habitable room windows, plus 3m for every additional storey or difference in levels and 13m between windows and a 2 storey blank wall.

To Silver Court - The proposed building would be 2 storeys in height and would be in an elevated position compared to the flats on Silver Court which are approximately one storey lower. However, as the flats are 3 storey in height, comparatively there would be little difference in heights between the two buildings. As such, a distance of 20m would be required between the properties to satisfy guidance. This would be achieved for the majority of the built form, although there would be a pinch point of 19.25m. The windows which would overlook Bank Street and the flats, would either be to service areas or the stairwell, and not from any offices or areas of the building which could potentially overlook the flats. The building would be orientated such that the fenestration with the main office windows would be on the western elevation overlooking the site car park. As such, aspect standards would be met and the proposal would comply with SPD6.

To Devon Avenue - The building directly behind the site is a single storey commercial build, separated by boundary fencing and vegetation. The elevation which would face this property would be blank and there would be no overlooking issues.

There is a residential property to the north west on Devon Avenue. Again, there would be no direct overlooking from the new building and no direct interface between elevations. It is proposed to locate the sub-station adjacent to the shared boundary with this house. The sub station would be 2.5m in height. Class E of the General Permitted Development Order, 2015, allows for buildings incidental to a dwellinghouse to be erected up to a height of 2.5m within 2m of a boundary with a neighbouring property. The proposed development would be no different to what could be carried out without the need for planning permission in a residential setting. Whilst the proposed plans indicatively show the design with materials to be confirmed, the principle of a building of this scale and height and in this location is considered acceptable, and these details can be sought by condition.

It is also proposed to erect paladin mesh type fencing along this boundary, and again details will be sought by the use of a condition.

As such, it is considered that the proposed siting of the buildings would comply with aspect standards and the guidance of SPD6 and therefore considered to be acceptable.

Highways issues

Parking - SPD11 states that maximum parking provision required for a B1 office use would be 1 per 35 sqm of floor area. This would equate to a total of 17 spaces required for a development of 613 sqm of floor area. It is proposed to provide 17 parking spaces and bike stands for 8 cycles and as such, parking on site for a development of this scale would be acceptable and comply with the maximum standards.

In addition, the site is located in a highly sustainable town centre location, and ideally located for access to the Metrolink and bus services which are close by.

The objectors have raised concerns about the existing parking problems from other businesses and commercial operations in the area. Whilst illegal and inconsiderate parking does appear to happen, and which has been noted from photographs submitted by one of the residents as well as experienced from site visits, it should not be presumed that the development would add to the parking pressure in the area or presume that people would break the law by parking illegally. In addition, the site as it is now, could operate as a business or commercial use without the need for planning permission which in itself could generate a certain amount of parking requirements and traffic to the area.

The objectors have also referred to the scale of the development and that the floor plans submitted for the development show that up to up to 60/70 employees could be accommodated in the offices. Regardless of 'open plan' type office accommodation which is shown on the plans, the arrangement of the offices is indicative only and would not necessarily generate such volumes of numbers who would be employed. Furthermore, this could be said for any type of B1 office business development which are in similar town centre locations, or even less sustainable and accessible areas where parking provision and access to public transport is not as readily available or accessible.

The site is in sustainable town centre location and with the nearby Metrolink and bus stops within walking distance, its highly sustainable location and compliance with the numbers of parking spaces required, the development is considered to be acceptable and in accordance with UDP Policies HT2/4, EC 6/1, SPD11 and the principles of the NPPF.

Access - It is proposed to use the existing access to the site which is situated at the southern end of the site and accessed via Bank Street. Bank Street is an unadopted road, parts of which are in a state of disrepair with uneven surfacing. There would be a certain amount of traffic which would use Bank Street to access the site, but this would be no different to the site operating as the current business use, and which could be used by far heavier vehicles on a more regular basis. Notwithstanding this, and whilst the applicant does not own have ownership rights over the whole street, the applicant is agreeable to a carrying out some repair works to the road and also to improve pedestrian facilities on either

side of the stepped pedestrian access to the site from Bank Street. This would be secured by way of a suitably worded condition where the applicant would set out a programme of works to be agreed and approved by the Highway's Section.

This would bring a benefit to the locality in terms of access and pedestrian safety, both of which have been raised as issues by some of the objectors.

As such, it is considered that the access arrangement would be beneficial and would better the situation which currently exists, and therefore would be compliant with UDP Policies EC3/1, EC6/1, EN1/2 and HT6/2.

TfGM - Given the proximity of the proposed development to the Metrolink, TfGM have been consulted on the application. Whilst there is no objection in principle, a number of issues were raised by TfGM, to which the applicant has either addressed or can address by way of condition.

Part of the development would include the top of the Metrolink cutting. TfGM have recommended that an Armco type barrier fencing be erected along the western boundary of the site to mitigate against the risk of vehicle incursion onto the tracks, and this has been incorporated within the scheme on a proposed boundary treatment plan. A condition requiring implementation and maintenance would be included as part of an approval. The proposed building itself would be sufficiently remote from the top of the cutting slope and the submission of a Construction Management Plan, again required by condition, would address any potential impacts during the construction phase which can be controlled and regulated by appropriate mitigation measures.

TfGM would also require details of the drainage proposals to ensure that the scheme would mitigate for any potential discharge of water onto the Metrolink and this would be incorporated into the standard drainage condition.

TfGM also wish to draw the developers attention to the compliance with working safely adjacent to the Metrolink requirements. This would be included as an informative.

As such, subject to the inclusion of the conditions highlighted above, the proposed scheme is considered to be acceptable and would comply with TfGM's requirements.

Crime Prevention - The design for security team recommend that appropriate forms of lighting and security measures are incorporated into the design and layout of the development. A plan has been submitted to indicatively show the location of lighting within the site area. Details of the type and exact positioning would be required by condition to ensure it would be appropriately located so as not to cause a nuisance to neighbouring properties and demonstrate the site would be adequately lit for security purposes.

Ecology

Summary - A bat assessment was carried out on 8th October and 2nd November 2015 which assessed the buildings as low risk but recommended that one emergence survey at an appropriate time of year be carried out. A further building inspection for bats was therefore conducted on 16th December 2016. No signs of bats were found during the survey.

Whilst no other ecological information has been provided, GM Ecology Unit (GMEU) are satisfied that potential risks such as nesting birds, invasive species and badger can be resolved via informative and or condition.

GMEU have been consulted and have raised no objections in principle, requiring conditions or informatives be included, as follows:

Bats - The buildings were assessed for their bat roosting potential. No evidence of bats were found and the buildings assessed as low risk. One emergence survey has been

recommended. Whilst accepting the buildings are low risk in a high risk location, the buildings are currently unoccupied and there are numerous more suitable roosting opportunities nearby and along the Metrolink.

GMEU are therefore satisfied that the emergence survey can be conditioned as precautionary prior to demolition.

An informative to advise the applicant of their responsibility under the Habitat Regulations would also be included.

Badgers - Badgers are known to utilise the Metrolink corridor between Radcliffe and Whitefield. An informative is advised.

Nesting birds - It would appear from the proposed layout that the existing overgrown hedge along the frontage of the site will be lost, and potential bird nesting habitat. Some of the buildings may also provide nesting opportunities. All British birds nests and eggs (with certain limited exceptions) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended. A condition to restrict the removal of trees or vegetation would therefore be included.

Invasive species - The site is adjacent to the Metrolink corridor which is high risk for species such as Japanese knotweed. It is recommended that an informative to advise the applicant of action which would be required if such species were to be disturbed as a result of the development.

Ecological mitigation - The overgrown hedgerow is a feature of ecological value and appears to be proposed for removal. Without mitigation, this would be contrary to UDP Policy EN6/3 and guidance within the NPPF that LPA's should aim to conserve and enhance biodiversity. It would appear that the hedgerow would be primarily removed for landscape and convenience, but which at the same time could be brought back in to management or replaced once cleared, like for like where not obstructing access etc. Alternatively, mitigation elsewhere on site could be proposed as it is noted that some of the land at the southern end would not appear to be developed as part of the scheme. This detail can be conditioned by way of the submission of a landscaping plan.

Response to objectors -

- The issues raised in terms of access, parking, highway safety, scale, position and impact of the building on surrounding properties and treatment along the boundary with the Metrolink have been covered in the report above.
- A Construction management plan would be required for submission by condition to ensure safe working practices are adhered to throughout the construction period, and in consideration of local occupiers.
- The bin store would be enclosed within a timber structure and collected on a regular basis as is the case with other commercial premises in the area.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered Site survey 04116CP-01; PL01_Rev D; PL02_Rev E; PL03_Rev D; PL04_Rev F; PL05_Rev A; PL06_Rev A; PL07_Rev C; PL08_Rev C; PL10_Rev D; PL11_Rev B; PL12_Rev B; PL13_Rev B; Bat Survey by Hilton Ecology November 2015 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Any soil or soil forming materials brought to site for use in garden areas, soft landscaping, filling and level raising shall be tested for contamination and suitability for use on site. Proposals for contamination testing including testing schedules, sampling frequencies and allowable contaminant concentrations (as determined by appropriate risk assessment) and source material information shall be submitted to and approved in writing by the Local Planning Authority prior to any soil or soil forming materials being brought onto site, and;
The approved contamination testing shall then be carried out and validity evidence (soil descriptions, laboratory certificates, photographs etc) submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. All instances of contamination encountered during the development works which do not form part of an approved Remediation Strategy shall be reported to the Local Planning Authority (LPA) immediately and the following shall be carried out where appropriate:
 - Any further investigation, risk assessment, remedial and / or protective works shall be carried out to agreed timescales and be approved by the LPA in writing;
 - A Site Verification Report detailing the conclusions and actions taken at each stage of the works including validation works shall be submitted to, and approved in writing by, the LPA prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. Should demolition not commence before 30th April 2017, emergence surveys shall be carried out and the findings submitted for approval by the Local Planning Authority. The development shall thereafter be carried out in accordance with the approved findings, and any recommendations and mitigation measures shall be implemented as approved.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
8. No works to trees or shrubs shall occur, and no building demolition works shall commence between 1st March and 31st August inclusive in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present, that has been approved in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
9. No development shall commence unless and until a landscaping scheme has been submitted to and approved by the Local Planning Authority. The content of the plan shall include elements to mitigate for loss of trees and shrubs and bird nesting habitat, and shall include a timetable for implementation and management plan. The approved landscaping scheme only shall be implemented, and in accordance with the approved timetable.

Any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

Reason. Insufficient information submitted at application stage. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan and chapter 11 - Conserving and enhancing the natural environment of the NPPF.
10. No development shall commence unless and until details of a lighting scheme and timetable for implementation has been submitted to and approved by the Local Planning Authority. The approved scheme only shall be implemented and thereafter maintained.

Reason. Insufficient information has been submitted at application stage. To ensure there would be no detrimental impact to local residents and to ensure adequate security measures to the site pursuant to Bury Unitary Development Plan Policies EC6/1 - Assessing New Business, Industrial and Commercial Development and EN1/2 - Townscape and Built design.

11. No development shall commence unless and until details of surface water drainage proposals, including details of mitigation of discharge of water onto the Metrolink, have been submitted to and approved in writing by the Local Planning Authority. The proposed scheme must be based on the hierarchy of drainage options in the National Planning Practice Guidance. It must be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Proposed maintenance arrangements for any SuDS features should also be provided. The development shall be carried out in accordance with the approved scheme and thereafter maintained.

Reason. To promote sustainable development and reduce flood risk. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact, pursuant to Unitary Development Plan Policy EN5/1 - New Development and Flood Risk and chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

12. The Armco vehicle barrier fencing along the western boundary, as shown on approved plan PL13_Rev B, shall be implemented prior to the development hereby approved being brought into use. The barrier fencing shall thereafter be maintained as approved.

Reason. To secure the boundary to the Metrolink and ensure there would be no adverse impact from vehicles accessing or using the site pursuant to Bury Unitary Development Plan Policies EC6/1 - Assessing New Business, Industrial and Commercial Development and EN1/2 - Townscape and Built Design.

13. Notwithstanding the details shown on the approved plans, no development shall commence unless and until full details of the following have been submitted to and approved by the Local Planning Authority:

- A programme of works, setting out the details and schedule of repair works to Bank Street and the un-named access to the rear of No. 12 Church Lane at the junction with the site access with; the access road to the rear yard have been submitted to and approved in writing by the Local Planning Authority.
- Improved pedestrian facilities on either side of the stepped pedestrian access to the development with Bank Street;
- Improvements to visibility at the junction of the site access with Bank Street/the un-named access to the rear of No. 12 Church Lane.

The approved programme of works/details subsequently approved shall be implemented in full before the development hereby approved is brought into use.

Reason. Insufficient information submitted at application stage. To ensure good highway design, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H3/1 - Measures to Improve Industrial Areas, EC6/1 - Assessing New business, Industrial and Commercial development, EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

14. No development shall commence, including any works of demolition, unless and until a Construction Traffic Management Plan (CTMP) with detailed method statements of demolition, construction and risk assessments, has been submitted to, and approved by the Local Planning Authority (approval to be in consultation with Transport for Greater Manchester). The approved CTMP shall include agreed safe methods of working adjacent to the Metrolink Hazard Zone and shall be adhered to throughout the construction period. The CTMP shall provide for:

- Access route for demolition and construction traffic from the highway network;
- Hours of operation and number of vehicle movements;
- Loading and unloading of plant and materials including arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site of operatives', visitors and demolition/construction vehicles together with storage of plant and demolition/construction materials;
- Measures to protect the Metrolink tunnel from potential impact from construction traffic loading;
- Measures to ensure that the development will not adversely affect the stability of the Metrolink cutting slope. Developer to fund slope monitoring during works, if deemed necessary, subject to the outcome of the loading, to ensure that construction works do not adversely affect the Metrolink cutting slope stability.
- Construction and demolition methods to be used; including the use of cranes;
- The erection and maintenance of security hoarding;
- Measures to control the emission of dust and dirt during construction;
- A scheme for recycling/disposing of waste resulting from demolition and construction works.

The approved plan shall be adhered to throughout the demolition/construction periods and the measures shall be retained and facilities used for the intended purpose for the duration of both periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials

Reason. Insufficient information submitted at application stage. In the interests of highway safety, to mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets, ensure adequate off street car parking provision and materials storage arrangements for the duration of the demolition/construction period, to safeguard the amenities of the locality, to safeguard Metrolink infrastructure and to ensure that the developer complies with all the necessary system clearances and agrees safe methods of working to meet the safety requirements of working above and adjacent to the Metrolink system. pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design and HT4 - New Development.

15. No development shall commence unless and until details have been submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the periods of demolition and construction.

Reason. Insufficient information submitted at application stage. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

16. The turning facilities on the approved plans shall be provided before the development is brought into use and shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies H3/1 - Measures to Improve Industrial Areas, EC6/1 - Assessing New business, Industrial and Commercial Development, EN1/2 - Townscape and Built Design and HT6/2 - Pedestrian/Vehicular Conflict.

17. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the development hereby approved being brought into use and thereafter maintained.

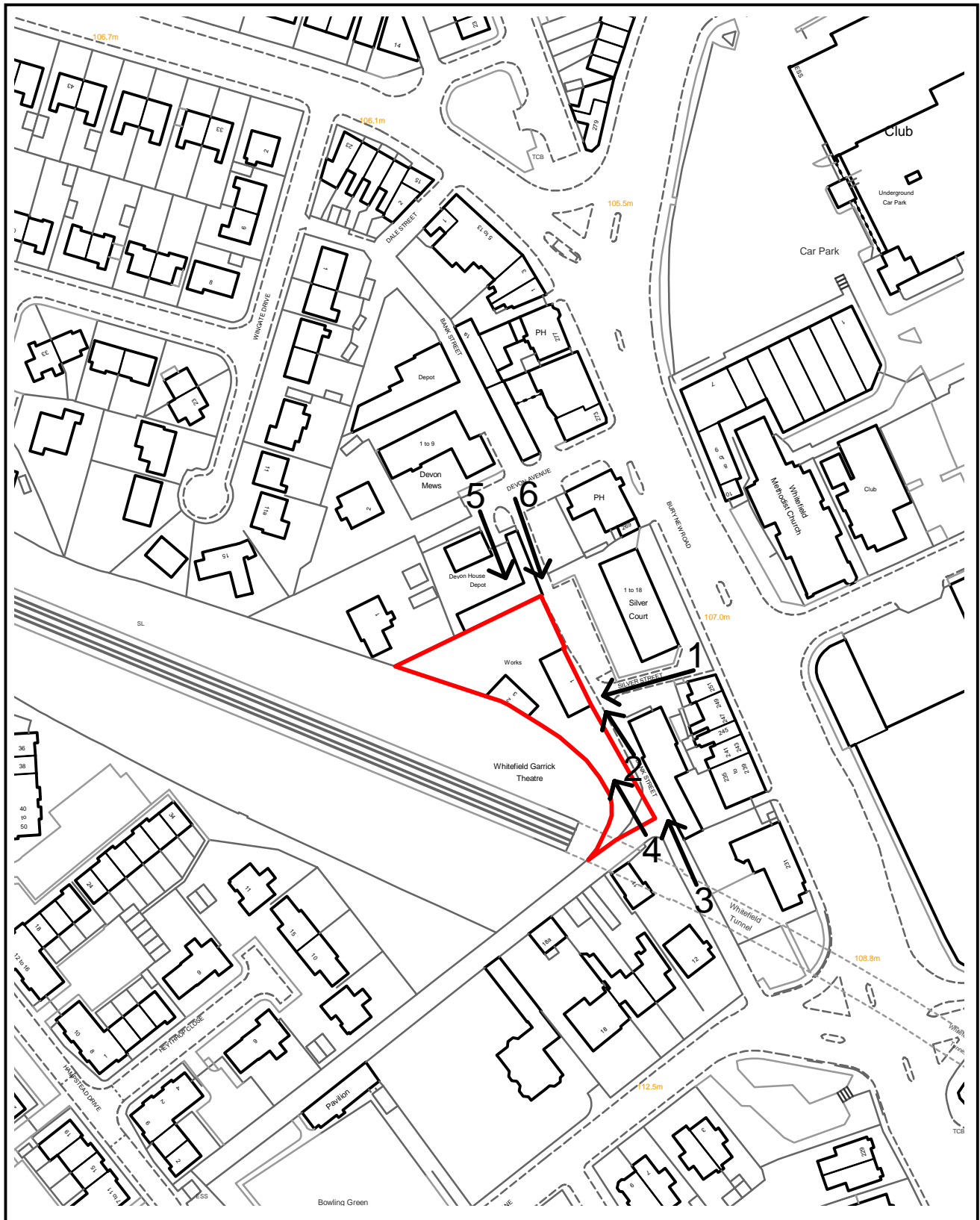
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policies H3/1 - Measures to Improve Industrial Areas, EC6/1 - Assessing New business, Industrial and Commercial Development, EN1/2 - Townscape and Built Design and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

18. Prior to the commencement of development, details relating to the proposed boundary treatment for the site shall be submitted to and approved by the Local Planning Authority. The approved details only shall be implemented as part of the approved development.

Reason - Insufficient information submitted at application stage. To secure the satisfactory development of the site and in the interests of the visual amenities of the area pursuant to Policies EN1/2 - Townscape and Built Design, EC3/1 - Measures to Improve Industrial Areas and EC6/1 - Assessing new Business and Commercial Development of Bury Unitary Development Plan.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60901

**ADDRESS: Land at Bank Street
Whitefield**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60901

Photo 1



Photo 2



60901

Photo 3



Photo 4



60901

Photo 5



Photo 6





01 Existing Building B - View from Bank Street



02 Existing Building B - View of the Existing from within the site



03 Site Entrance



04 View of the Existing from Within the site



05 Existing Building A



06 Site View from across the metro link

2.0 Existing Buildings to be Demolished

- Application Boundary
- Area of Works
- Existing Building to be demolished

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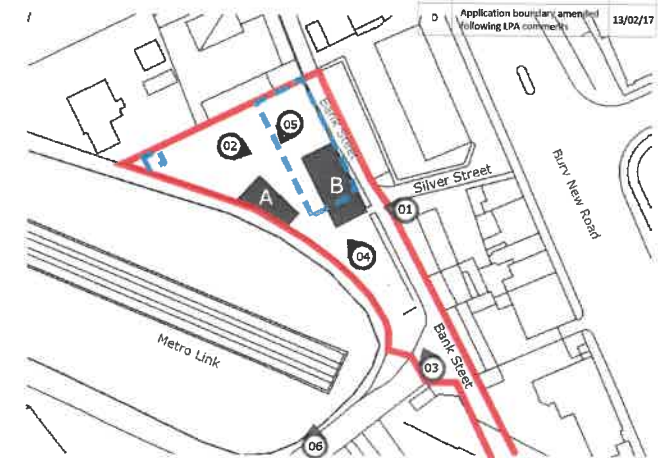
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Planning Drawing Series

Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16
C	Amended following LPA comments	23/01/17
D	Application boundary amended following LPA comments	13/02/17

Existing Site Plan NTS



Planning Series
Proposed Office Development

0291_Bank Street, Whitefield

PL 03_Rev D - Existing Site



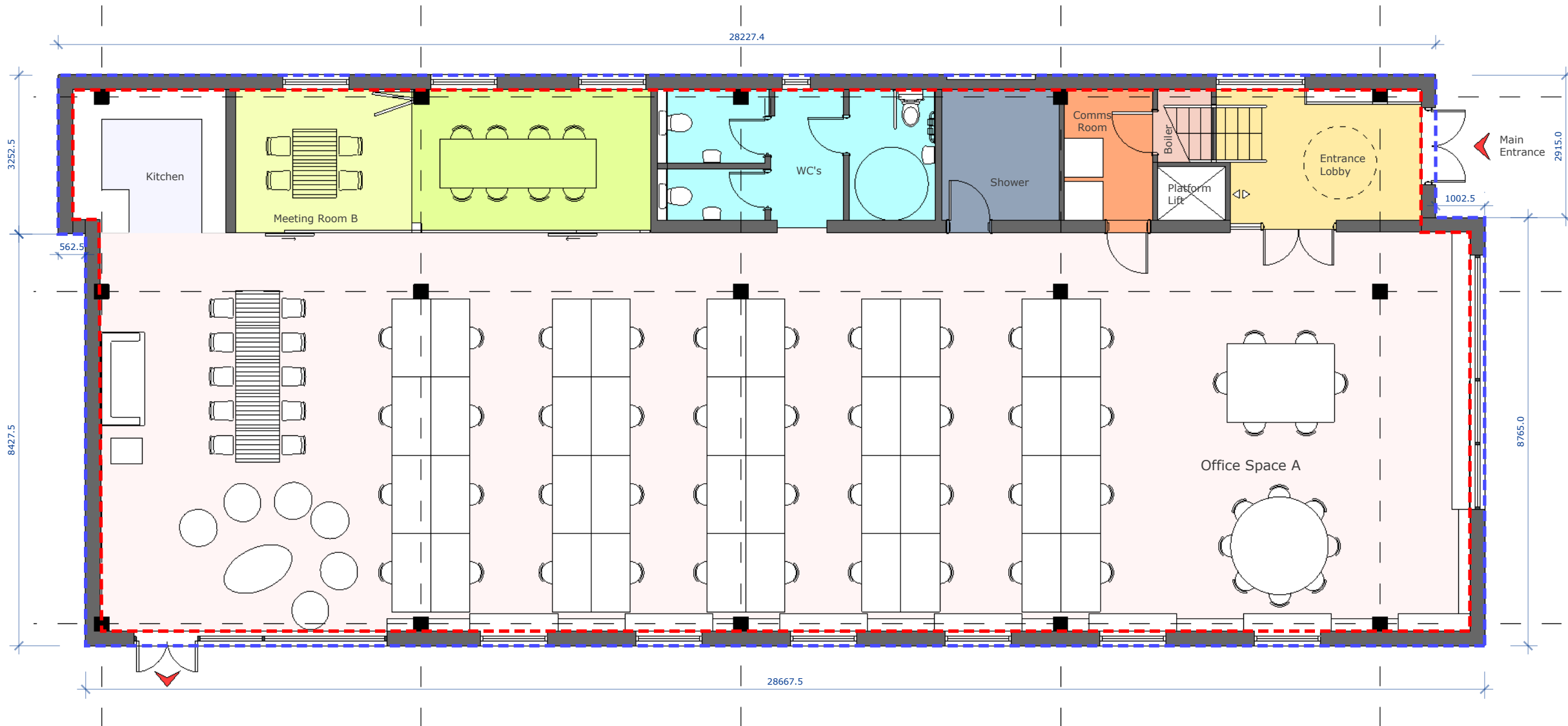


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Planning Drawing Series		
Rev	Description	Date
A	For Client Approval	12/12/16
B	Planning Application	25/11/16
C	Drawing Title Updated	06/12/16
D	Revised Following LPA Comments	04/01/17
E	Revised Following LPA Comments	23/01/17
F	Application boundary amended following LPA comments	13/02/17





Annotated Ground Floor Plan
Scale 1:100 @ A3

---	Total Gross Internal Ground Floor Area 309.5 SqM / 3331.4 SqF
---	Total Gross External Ground Floor Area 333.6 SqM / 3590.8 SqF

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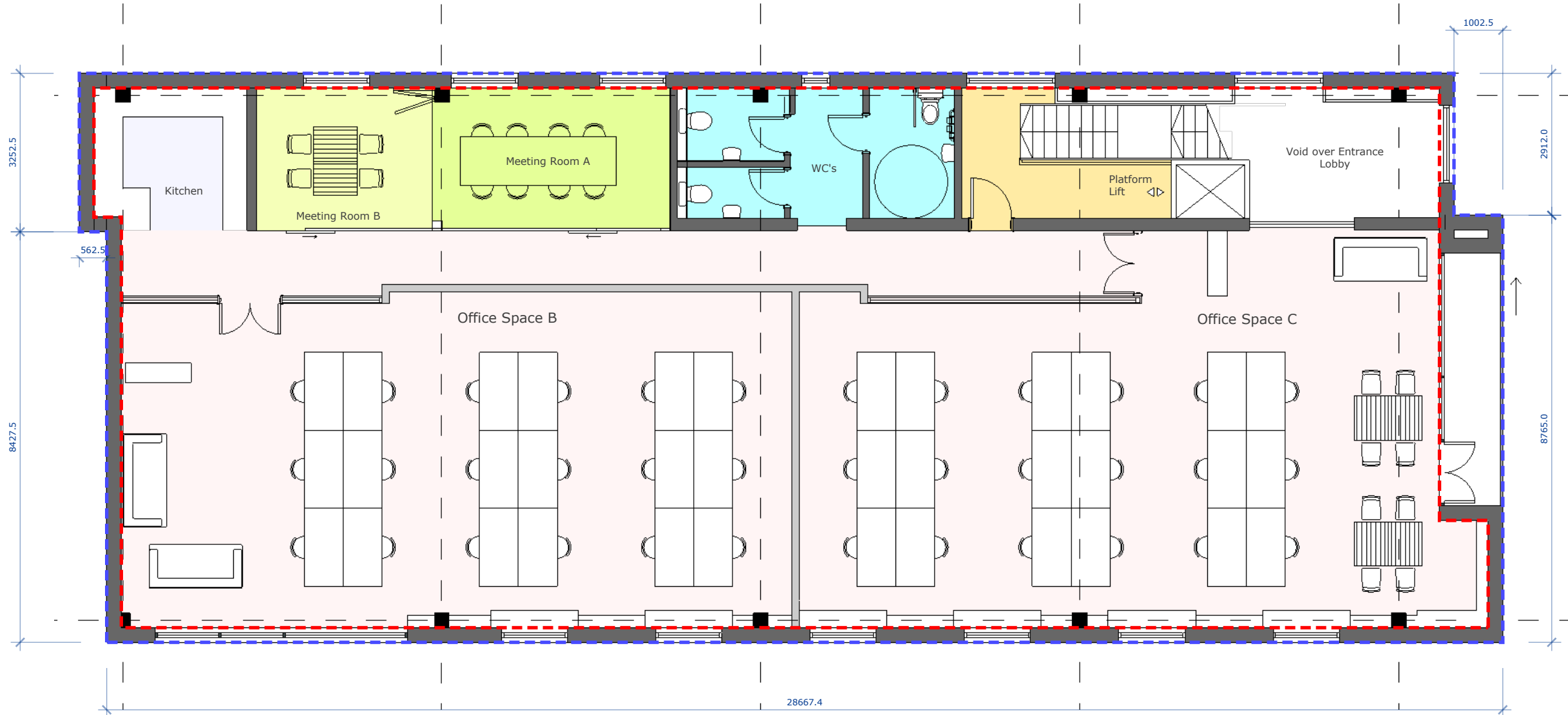
Planning Drawing Series

Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16

Concept Development
Proposed Office Development

0291_Bank Street, Whitefield
PL 05_Rev A - Proposed Ground Floor Plan





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Planning Drawing Series

Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16

Annotated First Floor Plan

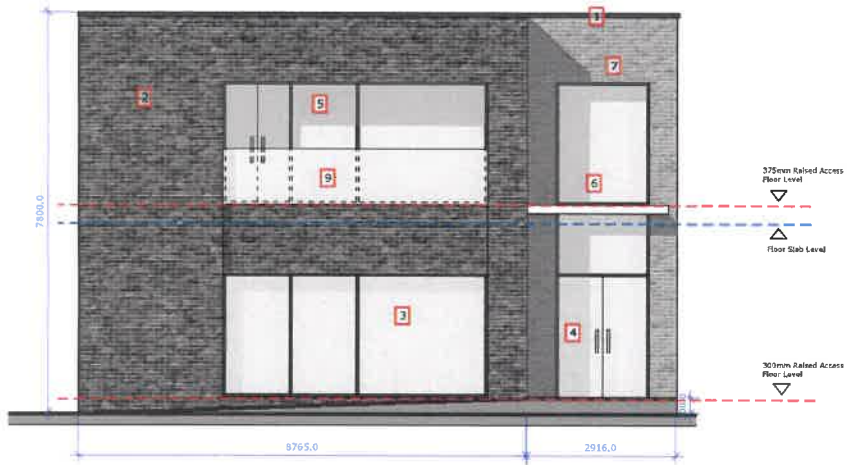
Scale 1:100 @ A3

- - - Total Gross Internal First Floor Area 303.5 SqM / 3266.8 SqF
- - - Total Gross External First Floor Area 333.6 SqM / 3590.8 SqF

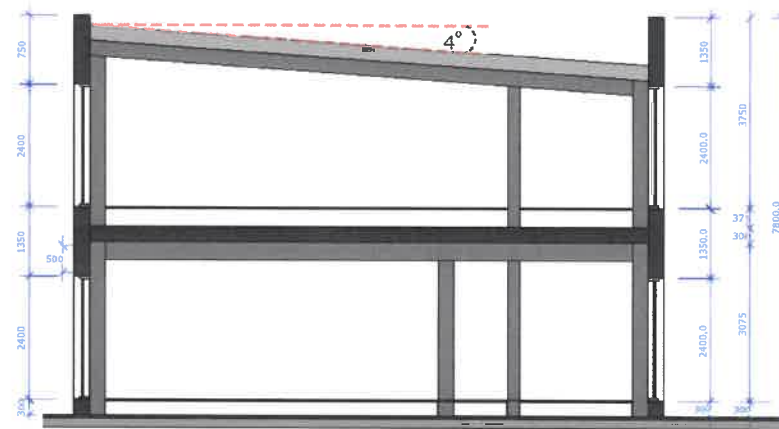
Planning Series
Proposed Office Development

0291_Bank Street, Whitefield
PL 06_Rev A - Proposed First Floorplan

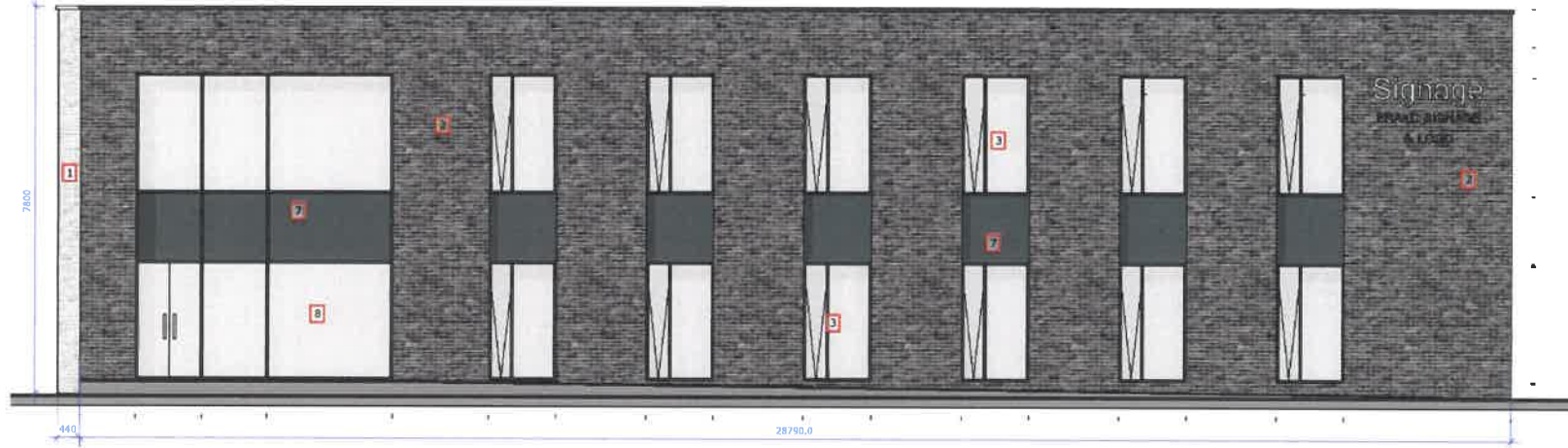




Elevation A
Front Elevation viewed from the Approach road
Scale 1:100 @ A3



Section ZZ



Elevation B
Front Elevation Viewed from the Carpark
Scale 1:100 @ A3

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Planning Drawing Series

Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16
C	Amended following LPA comments	23/01/17

Elevation Key

- 1 Natural multi tonal speckle pale grey brick, with light grey mortar bucket; handle stock brick
- 2 Natural multi tonal speckle pale charcoal grey brick, with light grey mortar bucket; handle stock brick
- 3 Double glazed windows with powder coated aluminium frames with thin 50mm profile - Colour TBC.
- 4 Glazed full height entrance doors within double height curtain wall, with powder coated aluminium frames 50mm profile - Colour TBC.
- 5 Full Height glazing with aluminium powder coated aluminium frames with thin 50mm profile, Colour TBC. Double doors to allow level threshold access onto balcony area.
- 6 200mm thick entrance canopy finished with GRC cladding Panels (Glass reinforced concrete panels), with external downlights
- 7 Spandrel panels to match window frames, located to mask service/Structural area behind, colour TBC by client
- 8 Full Height glazing with powder coated aluminium frames and a thin 50mm profile - Double doors at ground floor to allow level threshold opening onto external BBQ area.
- 9 Glass balustrade with 1100mm high aluminium handrail to match window frames

Planning Series
Proposed Office Development
0291_Bank Street, Whitefield
PL 07_Rev C - Proposed Elevations

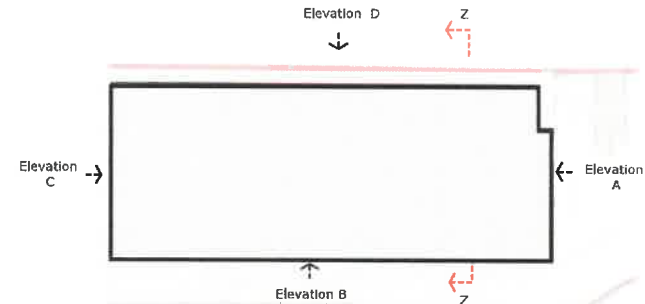




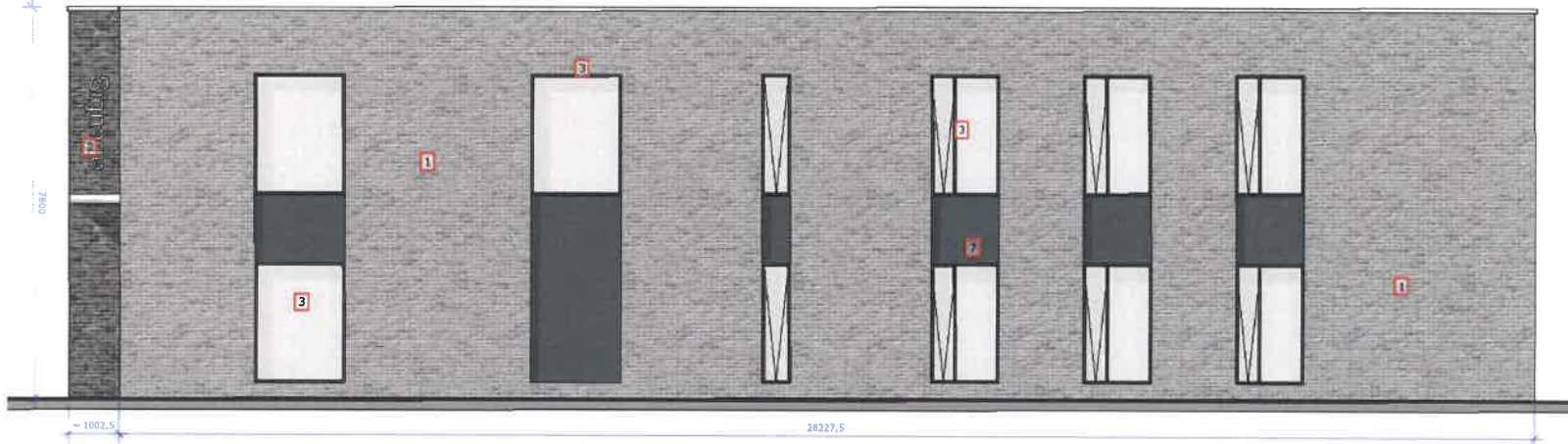
Elevation C

Blank gable to avoid overlooking neighbouring residential property

Scale 1:100 @ A3



Elevation and Section Location Plan



Bank Street Elevation

Elevation D

Scale 1:100 @ A3

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Planning Drawing Series

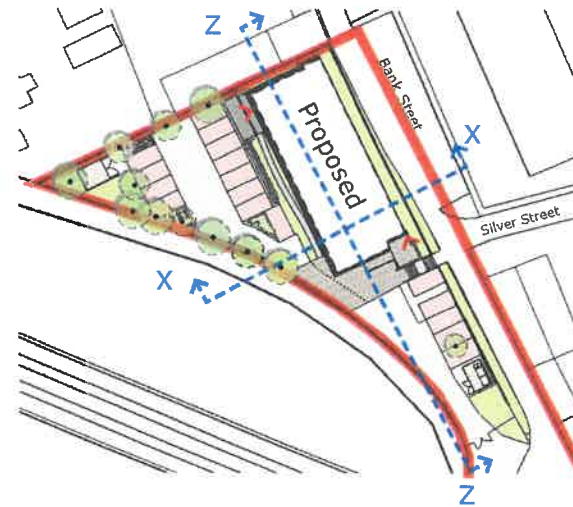
Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16
C	Amended following LPA comments	23/01/17

Elevation Key

- 1 Natural multi tonal speckle pale grey brick, with light grey mortar bucket handle stock brick
- 2 Natural multi tonal speckle pale charcoal grey brick, with light grey mortar bucket handle stock brick
- 3 Double glazed windows with powder coated aluminium frames with thin 50mm profile - Colour TBC.
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Planning Series
Proposed Office Development
0291_Bank Street, Whitefield
PL 08_Rev C - Proposed Elevations





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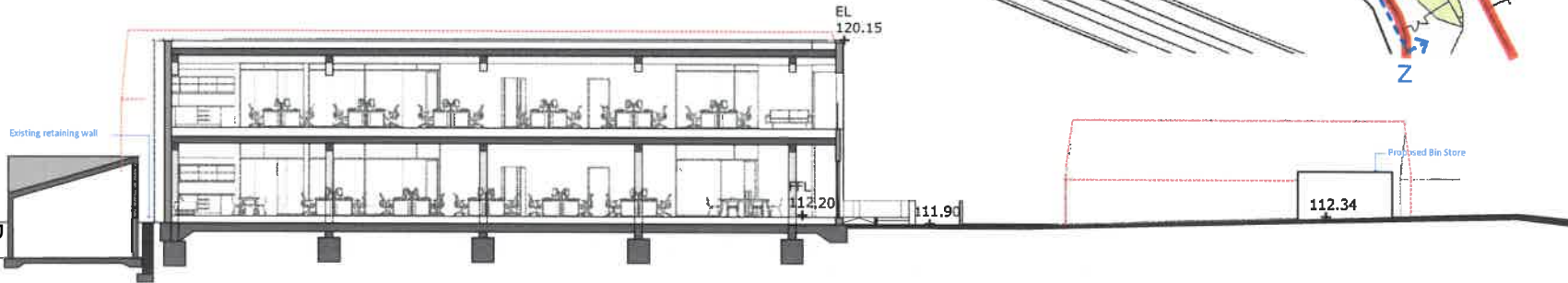
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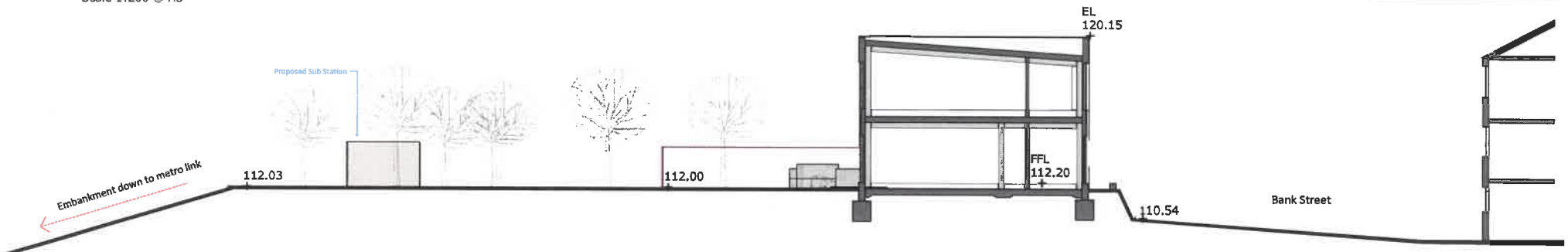
Planning Drawing Series

Rev	Description	Date
A	For Client Approval	11/11/16
B	Planning Application	25/11/16
C	Revised Following LPA Comments	04/01/17
D	Amended following LPA comments	23/01/17

Page 120



Section ZZ
North - South Site Section
Scale 1:200 @ A3



Section XX
East - West Site Section
Scale 1:200 @ A3

Planning Series
Proposed Office Development
0291_Bank Street, Whitefield
PL 10_Rev D - Detail First Floor Plan

TIM
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Ward: Ramsbottom and Tottington -
Ramsbottom

Item 07

Applicant: Mr Andrew Longstaff

Location: Side of 10 Scotland Place, Ramsbottom, Bury, BL0 9BD

Proposal: Erection of 1 no. attached dwelling

Application Ref: 60974/Full

Target Date: 13/02/2017

Recommendation: Approve with Conditions

Description

The application relates to the side garden area of a two storey, stone built house at the end of a terraced row of similar styled properties. The site lies within a residential area, to the south of Ramsbottom Town Centre and within Ramsbottom Conservation Area. The area has a mix of architectural styles. To the south is a complex of three storey apartments. To the north, across Cross Street are two storey terraced properties. To the east and situated at a higher level, are terraced houses fronting Bolton Street. The site comprises a side garden and parking area, accessed from Cross Street, that serves the applicants's house at 10 Scotland Place.

The proposal involves the construction of a two storey 4-bed dwelling with accommodation in the roofspace, at the western end of the row. It would be built in stone with a lime mortar, coursed to Scotland Place and random to Cross Street, with dressed stone features on the elevations. The windows would be timber sash and case style and designed to match the existing windows. Doors would be timber. The proposed orangery on the Scotland Place elevation would have a stone base with a painted timber frame above. The garden of the new dwelling would measure approximately 110sqm, leaving the existing house with a garden of 62sqm.

The new dwelling would have off-street parking for two cars, accessed from Cross Street. The existing dwelling at No.10 Scotland Place would as a consequence lose its off street parking. A parking survey by JMP Consultants Ltd, was submitted with the planning application. The assessment concludes that, given its central location, the size of the applicant's dwelling and availability of on street parking in the vicinity, there would be sufficient spare capacity within the immediate area to accommodate demand generated by the proposal.

Relevant Planning History

60316 - New Dwelling - Withdrawn.

Publicity

Press notice posted 12/01/17. Site notice posted 22/12/16. The following neighbours were notified by letter dated 22/12/16 - Flats at 55 Bolton Street, 99-141 Kay Brow, 8-16 Scotland Place, 1-5(odd) Cross Street, 2 St Johns Court, 56 Square Street, 57-67(odd) Bolton Street, 45 Cross Lane.

Objection from the resident at 59 Bolton Street on the following grounds:

- The proposal will increase existing parking problems in the area. Lodge St has been suggested as appropriate parking area however is in fact an unadopted street clearly displaying a "No Motor Vehicles, Access only" sign. My garage is regularly blocked & used as overflow parking for the existing properties and visitors to Ramsbottom due to

- proximity to shops etc.
- The new building is irregular sized & much larger than of the other properties in terrace and will impact our picturesque views.
- Building works will cause disruptions. Given Cross Street's steep gradient, I can assume that Lodge St will be used to unload & park while work takes place, further blocking our garages.

The objector has been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Drainage Section - No objection.

Environmental Health - No objection.

Conservation - No objection.

United Utilities - No objection.

Ramsbottom Heritage Society - Some concerns about proportions of windows in initial plans but no objection in principle. Plans have been revised to address these issues.

Unitary Development Plan and Policies

EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN1/2	Townscape and Built Design
HT2/4	Car Parking and New Development
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/3	Extensions and Alterations
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policies - UDP Policy EN1/1 Visual Amenity states that development will not be permitted where proposals would have a detrimental effect on:

- public views of prominent or important buildings, especially those in areas of architectural or historic interest;
- the visual amenity both within, or viewed from, areas of environmental interest such as the Green Belt, Special Landscape Areas or the river valleys.

Policy H1/2 - Further Housing Development. The Council will have regard to the following factors when assessing proposals for housing development on sites not identified on the Proposals Map:

- the need to direct development towards the urban area;
- the availability of infrastructure;
- the need to avoid the release of peripheral open land, unless this can be shown to be consistent with urban regeneration;
- the suitability of the site in land use terms with regard to amenity, the nature of the local environment and surrounding land uses;

e) other policies and proposals of the Plan.

H2/1 - The Form of New Residential Development

All new residential development will be expected to make a positive contribution to the form and quality of the surrounding area. Factors to be considered when assessing proposals will include:

- a) the height and roof type of adjacent buildings;
- b) the impact of developments on residential amenity;
- c) the density and character of the surrounding area;
- d) the position and proximity of neighbouring properties;
- e) the materials to be used in proposed developments, especially their colour and texture.

H2/2 - The Layout of New Residential Development

New residential development will be required to demonstrate acceptable standards of layout. Factors to be considered when assessing proposals will include:

- a) car parking provision;
- b) access for both vehicles and pedestrians, and provision for public transport;
- c) the possible need for traffic calming measures;
- d) density;
- e) space about and between dwellings;
- f) landscaping and screening;
- g) protection/provision of trees and hedgerows;
- h) open space/children's play areas;
- i) design for safety and security;
- j) access/facilities for the disabled;
- k) the existence of any public rights of way.

H2/6 - Garden and Backland Development

The Council will not permit proposals which will result in the loss of private gardens and backland for infill development unless such proposals can be shown not to adversely affect the character and amenity of the area.

When assessing proposals, special regard will be given to:

- a) the concentration of such development in the surrounding area;
- b) the relative density of the proposal to that of the surrounding area;
- c) the impact on neighbouring properties and the local environment;
- d) access arrangements.

EN2/1 - Character of Conservation Areas. The Council will take action as appropriate to preserve or enhance the character or appearance of the Borough's Conservation Areas. The Council will be especially concerned with encouraging and, where appropriate, implementing measures to:

- a) retain, replace and restore features of historical and architectural interest;
- b) retain and enhance existing landscape features including trees, parks and gardens;
- c) initiate and promote environmental improvement/enhancement schemes such as landscaping, refurbishment of street furniture, traffic management and pedestrian schemes;
- d) remove dereliction and bring unused land or buildings back into beneficial use;
- e) prepare and promote design guidelines to ensure sympathetic development.

EN2/2 - Conservation Area Control. Development within a Conservation Area will only be acceptable if it preserves or enhances the special character or appearance of the area. In considering proposals for development in Conservation Areas, regard will be had to the following criteria:

- a) the nature of the development in terms of its bulk, height, materials, colour, design and detailing;
- b) the relationship between the proposed development and the architectural and visual qualities of the surrounding area;
- c) where demolition is proposed, the contribution of any proposed new building to the character or appearance of the area as compared to the building to be demolished;
- d) in the case of the re-use of buildings or the introduction of new uses, the impact of the

proposal on the character or appearance of the area and the fabric of the existing building.

Principle - Policy H1/2 relates to the principle of new residential development outside sites identified in the UDP and states that proposals within the urban area, where there is available infrastructure and land is suitable in terms of land use and amenity will be acceptable. As the proposed site is within the urban area with available infrastructure, it is considered to be acceptable in principle.

Visual Amenity and Character of the Conservation Area - Policies H2/1, H2/2 relate to the form and layout of residential development and new housing. The proposed site is considered to be of sufficient size to accommodate the proposed house without appearing to constitute overdevelopment or be out of character within the locality.

Following slight concerns over the proportion of the windows, the elevations have been revised to indicate a more traditional size. The size, height and massing of the new build with the stone finish and traditional sash and case windows with stone heads and sills is now considered to be appropriate to the streetscape and the wider conservation area. Whilst the new building is slightly wider than the other terraced units on the row, this is not a significant design problem as one views the terrace more as a single entity rather than individual units. The proposal is considered to be acceptable preserves the character of the conservation area and complies with UDP Policies H2/1, H2/2 EN2/1 and EN2/2 with regard to its siting and design and character of the conservation area in which it is located.

Residential amenity - Across Cross Street there is a separation distance of just over 19m between the windows on the front elevation of the new house and those on 1 and 3 Cross Street. Given that the new house would follow the existing residential street pattern along Scotland Place/Cross Street and the new building levels would be set down, there are no serious privacy issues arising. The separation distances on the south side are in excess of 20m and are also acceptable. To the west, the new gable end would, because of levels differences, not significantly impinge on privacy or outlook from properties fronting Bolton Street. The proposal is considered to be acceptable in terms of residential amenity and complies with UDP Policies H2/1 The Form of Residential Development in this respect.

Parking and Access - Adopted guidance suggests a maximum of 2 spaces per house in new build development. Whilst the proposal includes sufficient off-street parking for the new house, the off-street parking for the existing dwelling would be lost. The parking survey submitted with the application makes a reasonable case for accepting the scheme with on-street parking for the applicant's existing house. It is noted that the applicant's house is only two bedrooms, is in a sustainable location close to the town centre and has unrestricted parking immediately to the front, on Cross Street. On balance, the proposal is acceptable and complies with UDP Policies relating to parking and access.

Objection - The objections relating to parking and size of development have been addressed in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

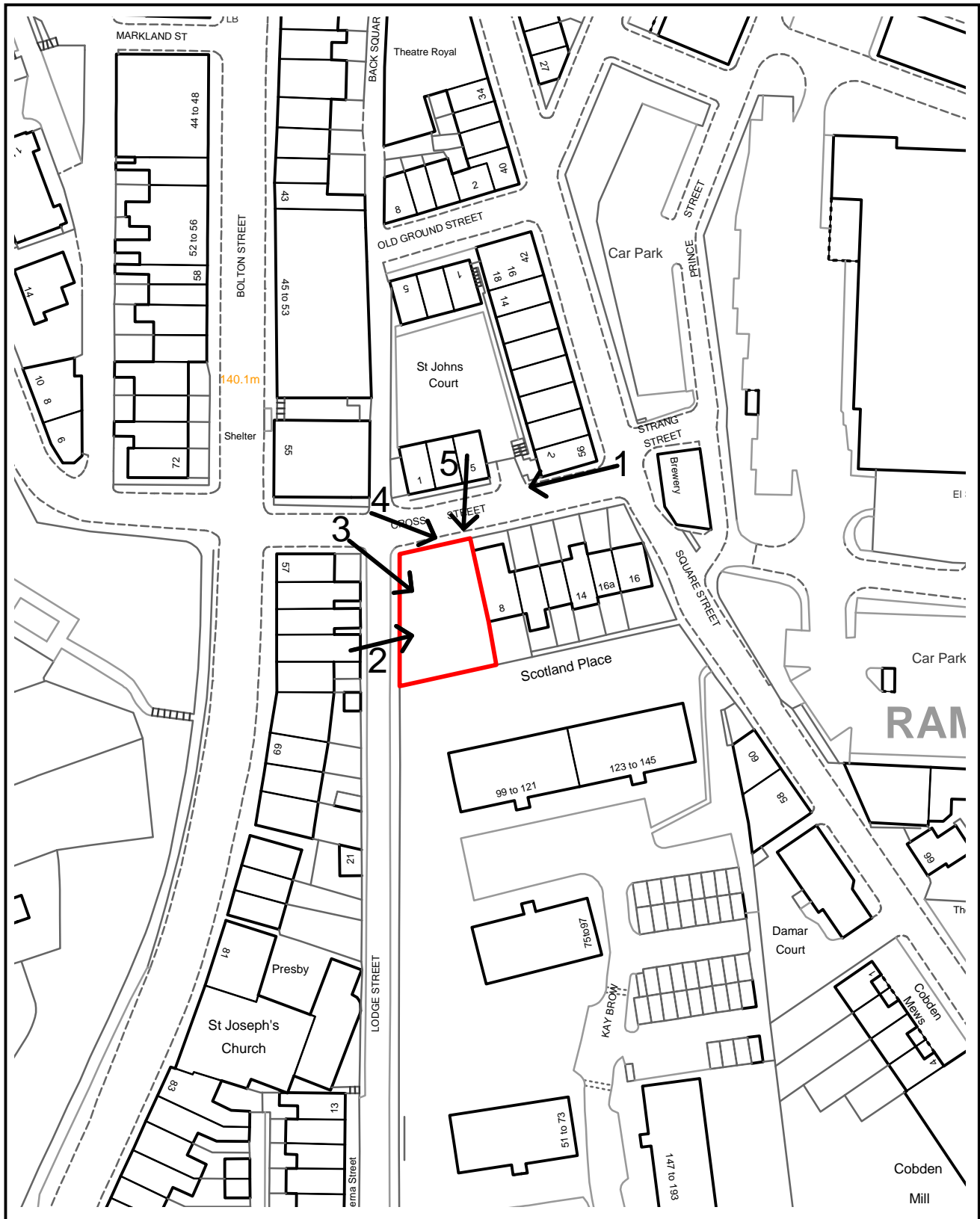
Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 6166(P)100, 102/A, 103/F, 103-1/G, 200, 202/B, 204, 203/E and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the stonework, including sills, heads and door surrounds, roofing materials, rainwater goods and windows shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. Detailed drawings/sections of how the proposed windows/rooflights would be installed within the development, at no less than 1:20 scale, shall be submitted to and approved in writing by the Local Planning Authority before any development is commenced.
Reason. Details have not been submitted and in order to protect the character of the Ramsbottom Conservation Area the interests of visual amenity pursuant to UDP Policies EN1/2 Townscape and Built Design, EN2/1 Character of Conservation Areas and EN2/2 Conservation Area Control.
5. No development shall commence unless and until a 'Construction Management Plan' (CMP), has been submitted to and agreed in writing with the Local Planning Authority and shall provide the following:
 - Hours of operation and number of vehicle movements;
 - Parking on site or on land within the applicant's control of operatives' and construction vehicles together with storage on site of construction materials.
 - Details of measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.
Reason. No details have been submitted and to mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to UDP Policy H2 Highway Network.
6. The car parking indicated on the approved plans shall be made available for use prior to the dwelling hereby approved being brought into use. It shall be thereafter maintained at all times.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
7. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of

the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60974

**ADDRESS: Side of 10 Scotland Place
Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60974

Photo 1



Photo 2



60974

Photo 3



Photo 4



60974

Photo 5



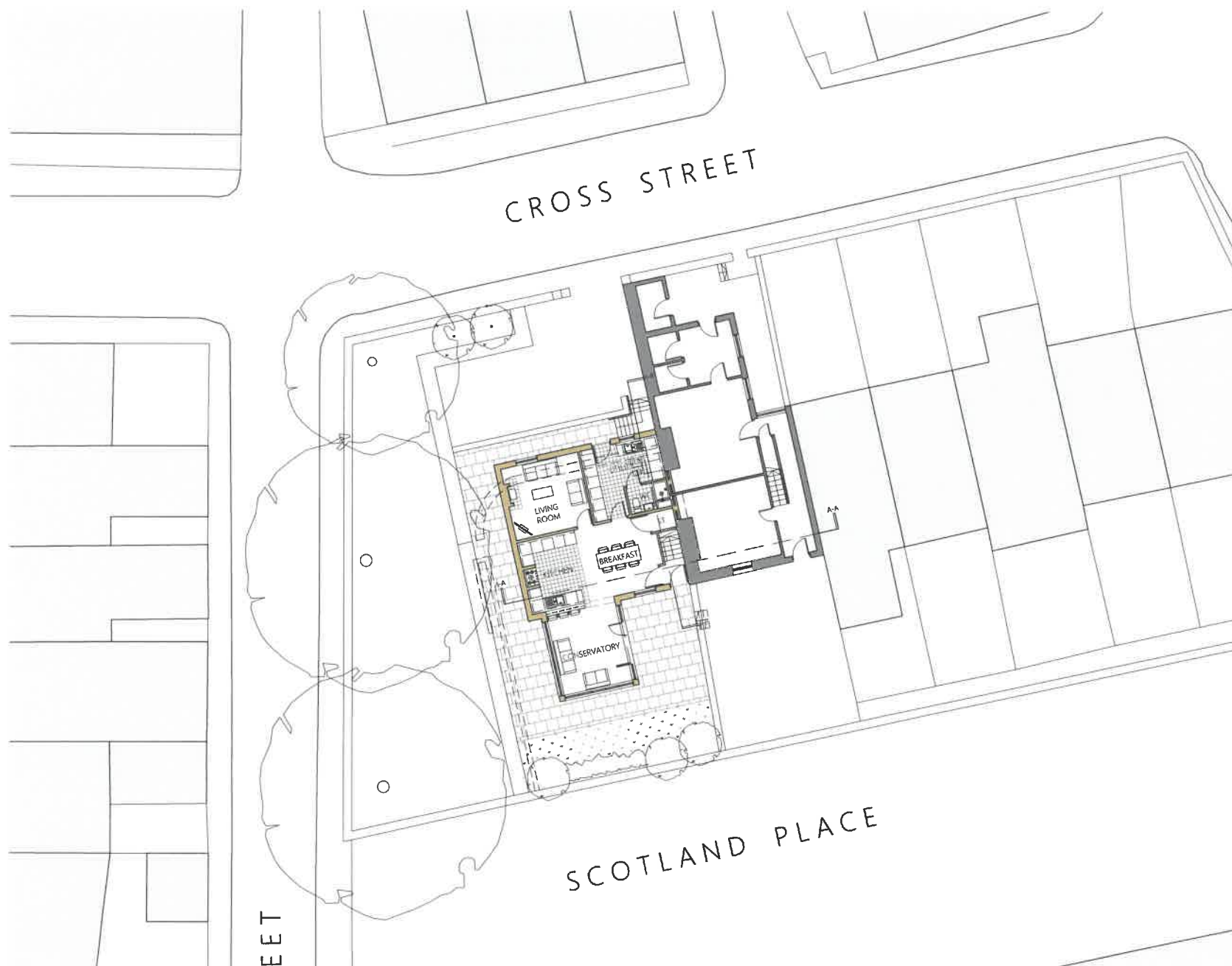


Client	Drawn by	Scale	Drawn by	Checked by
MR & MRS LONGSTAFF	EXISTING SITE PLAN & GROUND FLOOR PLAN	1:100 @ A1	BP	PJH
Project	Date	Discipline	Discipline	Discipline
LAND AT 10 SLOLAND PLACE, RAMSBOTTOM, BLO 98D	11/10/2016	architecture		
	Drawn by	Discipline	Discipline	Discipline
	6166(P)102			



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E-111116-PLANNING USED
E-261134-AMENDED FOLLOWING CLIENT'S COMMENTS
C-241134-AMENDED FOLLOWING CLIENT'S COMMENTS
E-161135-AMENDED FOLLOWING CLIENT'S COMMENTS
A-111131-AMENDED FOLLOWING CLIENT'S COMMENTS

Office
MR & MRS LONGSTAFF
Page 1
**LAND AT 10 SCOTLAND PLACE,
RAMSBOTTOM, BL0 9BD**

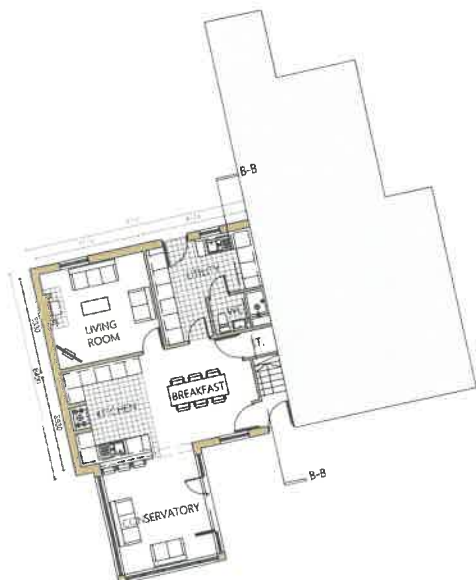
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GROUND FLOOR PLAN**

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Date
11/10/2016
Drawing number
6166(P)103

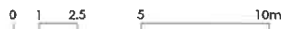
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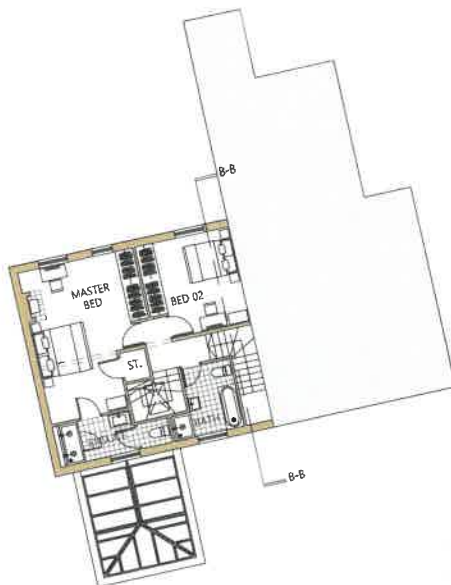


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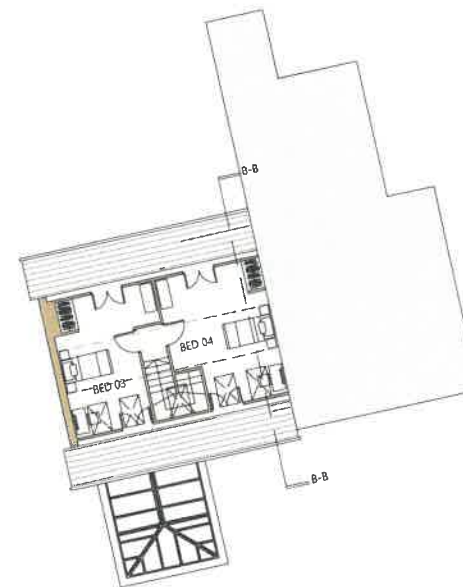


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E = 27.09.98 = 11/2000 FOLLOWING CLIENT COMMENTS
D = 26.11.16 = AMENDED FOLLOWING CLIENT COMMENTS
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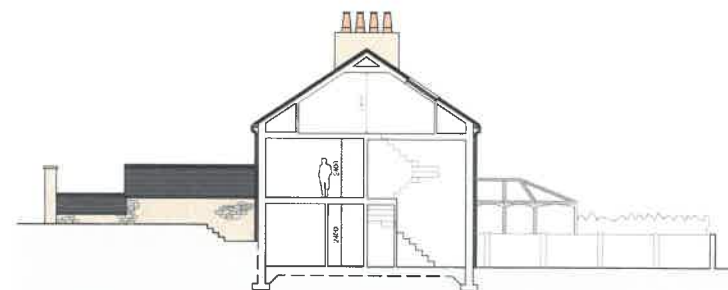
Reviewed:



PROPOSED FIRST FLOOR PLAN

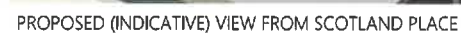


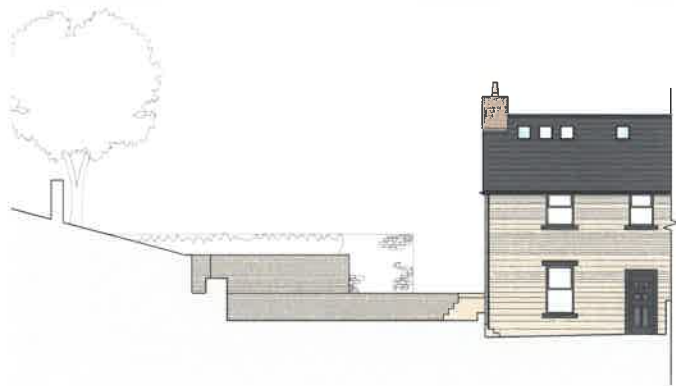
PROPOSED SECOND FLOOR PLAN



SECTION B-B

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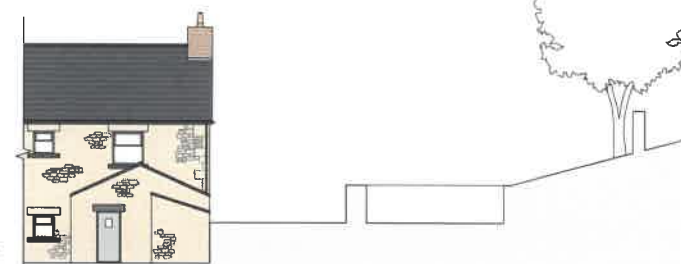




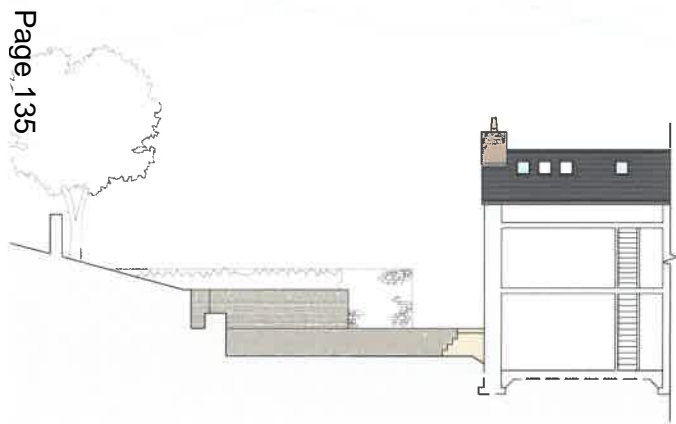
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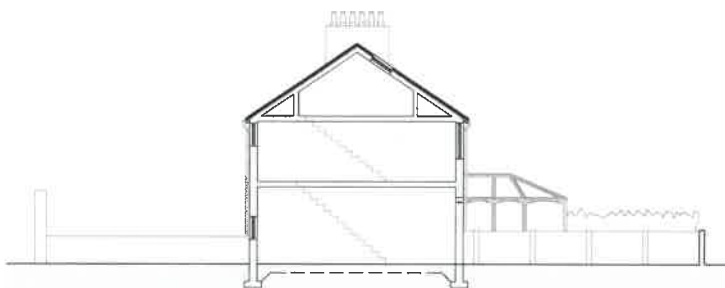
EXISTING ELEVATION B FROM LODGE STREET @ 1:100



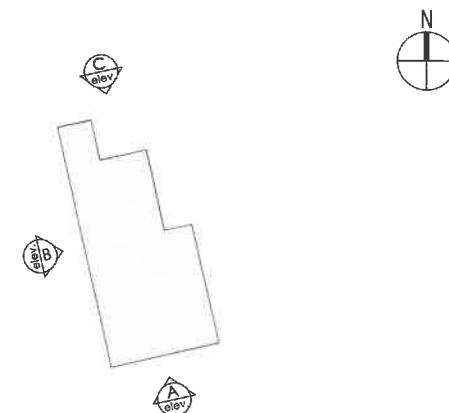
EXISTING ELEVATION C FROM CROSS STREET @ 1:100



EXISTING SECTION A-A FROM SCOTLAND PLACE @ 1:100



EXISTING SECTION B-B FROM LODGE STREET @ 1:100



ELEVATION KEY - NTS

0 1 2.5 5 10m

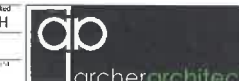


Architect: MR & MRS LONGSTAFF
 Drawn: BP
 Date: 11/10/2016
 Project: LAND AT 10 SCOTLAND PLACE, RAMSBOTTOM, BL0 9BD

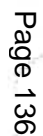
Client: MR & MRS LONGSTAFF
 Project: LAND AT 10 SCOTLAND PLACE, RAMSBOTTOM, BL0 9BD
 Date: 11/10/2016
 Project: 6166(P)202

EXISTING BUILDING ELEVATIONS & SECTIONS

Scale: 1:100@A1
 Drawn: BP
 Date: 11/10/2016
 Project: 6166(P)202



archer architects | Suite 11, 100-101, 102-103, 104-105, 106-107, 108-109, 110-111, 112-113, 114-115, 116-117, 118-119, 120-121, 122-123, 124-125, 126-127, 128-129, 130-131, 132-133, 134-135, 136-137, 138-139, 140-141, 142-143, 144-145, 146-147, 148-149, 150-151, 152-153, 154-155, 156-157, 158-159, 160-161, 162-163, 164-165, 166-167, 168-169, 170-171, 172-173, 174-175, 176-177, 178-179, 180-181, 182-183, 184-185, 186-187, 188-189, 190-191, 192-193, 194-195, 196-197, 198-199, 200-201, 202-203, 204-205, 206-207, 208-209, 210-211, 212-213, 214-215, 216-217, 218-219, 220-221, 222-223, 224-225, 226-227, 228-229, 230-231, 232-233, 234-235, 236-237, 238-239, 240-241, 242-243, 244-245, 246-247, 248-249, 250-251, 252-253, 254-255, 256-257, 258-259, 260-261, 262-263, 264-265, 266-267, 268-269, 270-271, 272-273, 274-275, 276-277, 278-279, 280-281, 282-283, 284-285, 286-287, 288-289, 290-291, 292-293, 294-295, 296-297, 298-299, 300-301, 302-303, 304-305, 306-307, 308-309, 310-311, 312-313, 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2256-2257, 2258-2259, 2260-2261



E = 01.02.97 - WINDC HAS REVISED FOLLOWING COMMENTS FROM HERITAGE SOCIETY
C = 30.03.95 - PLANNING BOARD
C = 28.11.95 - AMENDED FOLLOWING CLIENT COMMENTS
B = 21.11.95 - AMENDED FOLLOWING CLIENT COMMENTS
A = 12.01.96 - AMENDED FOLLOWING CLIENT COMMENTS

MR & MRS LONGSTAFF

PROPOSED BUILDING ELEVATIONS & SECTIONS

File	Draw	Classed
& 1:100@A1	BP	PJH
Print	Printing Unit	
11/10/2016	architecture	
Plotting number		Site
6166(P)203		E



architect: steven nigel | suite 42 | first floor | mindenhall court |
high street | stoke on trent | st 10 3UN | t: 01426 366 968
architect: mark chisholm | 60b-a | 5th floor | 4th prince street |
manchester | M1 6DE | t: 0161 226 4020
e: aro@archerarchitect.co.uk
www.archerarchitect.co.uk

Ward: Prestwich - St Mary's

Item 08

Applicant: Mr Steve Costello

Location: Land to the rear of Sandy Lane, Prestwich, Manchester, M25 9PS

Proposal: Demolition of existing garages and erection of 2 no. semi detached houses

Application Ref: 60997/Full

Target Date: 07/03/2017

Recommendation: Approve with Conditions

Description

The application site comprises land to the rear of the existing row of shops on Sandy Lane which form the Neighbourhood Shopping Centre. The land is made up of a tarmacadam hardstanding and accommodates a garage colony of 17 units, most of which are vacant with a run-down appearance. The shopping centre lies to the east and there are houses to the south, west and north.

It is proposed to demolish the existing garages and construct a pair of two storey semi-detached houses. There would be private garden areas at the rear and tandem parking on either side of the houses. The open tarmacadam area in front of the houses would be maintained to allow access to the rear of the shops and servicing.

Each house would have three bedrooms and a footprint 7.4m by 4.7m. Each house would be constructed in red brick with a hipped roof to a maximum ridge height of 7.5m.

It should be noted that an application for a three storey block of 6 apartments within the site was refused in May 2012. The subsequent appeal was dismissed.

Relevant Planning History

54837 - Demolition of existing garages and construction of 6 flats with associated external works - 07/02/2013

54837 - Demolition of existing garages and construction of 6 flats with associated external works - Refused 16/05/2012. Appeal dismissed 07/02/13.

Publicity

The following neighbouring properties were notified by letter dated 11/01/17. Flats at Lowther Court, 63-73(odd), 78-84 Sandy Lane, 10-16(even), 2-16(even) Lynmouth Grove, 1 and 2 Lynmouth Court, 138-152(even) Butterstile Lane, 2-8(even) Carr Avenue, Carrigart Bury New Road.

Objections received from residents at 4 and 6 Carr Avenue, 6 Lynmouth Road and 79 Sandy Lane. Objections are summarised:

- disturbance the building work will cause, in terms of heavy vehicles & noise, as the entry point & passage to the proposed houses is adjacent.
- this pair of semis appears to have been pushed right into a tight corner without any thought for existing properties.
- overlooking from upstairs windows, therefore reducing privacy.
- loss of light as well as being invasive and an eyesore.
- drainage issues need to be addressed so they do not cause problems with immediate neighbours.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.
Drainage Section - No objection.
Environmental Health - No objection.
Waste Management - No objection.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/6	Garden and Backland Development
EN1/2	Townscape and Built Design
EN1/5	Crime Prevention
EN5/1	New Development and Flood Risk
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework
SPD6	Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - As the site is within the urban area with available infrastructure, the main issues with regard to the proposal are the siting of the building within what is a backland area and the size of the development in relation to the plot and the surroundings. The proposal is acceptable in principle and would be in accordance with UDP Policy H1/2 Further Housing Development.

Layout, design and appearance - The proposed siting of the houses within what is a backland area, behind a local shopping centre is unusual. However given that the site is a brownfield site within the urban area, the layout works in terms of circulation.

The previous scheme, proposing a three storey block of four apartments was refused by the Local Planning Authority in 2012 and the subsequent appeal was dismissed. The building would have appeared incongruous in the locality and resulted in undue overlooking of the neighbours. This revised scheme is more appropriately scaled and would not result in serious overlooking. As such the proposal would be appropriate in terms of its position, height, bulk, scale and massing and would not be a prominent feature within the immediate backland area.

In a wider context, policy suggests that new development should reflect local characteristics and the existing urban grain, including the layout and pattern of streets, spaces and buildings. Whilst the siting of the pair of semi-detached houses within a former garage colony, behind a Local Shopping Centre is rather unusual, it is not unheard of. The land in question is disused and a pair of semi-detached properties is considered to be an acceptable use, particularly in view of the current housing shortage. The proposed development would comply with the NPPF and UDP Policies H2/1, H2/2 and EN1/2 in relation to visual amenity and townscape.

Residential amenity - SPD6 provides guidance on aspect standards between residential properties and is relevant in this instance. SPD6 states that there should be a minimum of

13 metres between a habitable room window and a two storey blank wall/gable.

The new side gable is set back 4.5m from the shared boundary with houses on Lynmouth Grove and there would be approximately 26m between this gable and the rear elevations of houses on Lynmouth Grove. The distance on the eastern side, between the new building and the flats above the shops on Sandy Lane would be approximately 18m. Both these separation distances are considered acceptable in relation to the rooms within neighbouring properties and there are no habitable room windows in either gable.

The windows on the front and rear elevations of the proposed houses face directly over the existing tarmac service area and the proposed garden areas respectively. There would be no direct overlooking of neighbouring houses. As such there would be no serious reduction in privacy.

The proposed amenity space to the rear of the new houses would reflect those of other properties in the immediate locality and therefore would be acceptable.

The proposed development would not have a seriously adverse impact upon the residential amenity of the neighbouring residents and would be comply with Policies H1/2, H2/2 and EN1/2 of the adopted Unitary Development Plan.

Traffic and Parking - In terms of parking, two spaces per unit would be considered appropriate and complies with UDP Policies H2/2 and HT2/4 and associated guidance within SPD11 Parking Standards in Bury in this respect.

The proposed layout allows sufficient space for delivery and service vehicles to access both the rear of the shops and the proposed houses, turn and leave the site in a forward gear. This is considered to be acceptable in terms of highway safety and maintaining the free flow of traffic.

In terms of traffic volume generated by the proposal, it is not considered that this would increase given the potential traffic that could be generated by the existing garage colony use. The proposal complies with UDP Policies H2/2 and HT2/4 with regard to parking and access.

Trees - The trees within the site are not particularly good specimens and would not warrant protection. The removal of these trees would not be resisted on planning grounds.

Objections - Issues raised by objectors have been addressed in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered 9297/001/A, 011/B, 021/B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.
5. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
- Access route for demolition/construction traffic from the highway network;
 - Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations.
 - Hours of operation and number of vehicle movements;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
 - Parking on site of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials clear of existing turning/servicing facilities for the adjacent flats and commercial units.
- The approved plan shall be adhered to throughout the demolition and construction periods and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition and construction periods. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.
Reason. Details have not been submitted and to mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety.
6. The proposed pedestrian access improvements indicated on the approved plans, incorporating the removal of the existing service yard gates and provision of a surfaced pedestrian route, shall be implemented to the satisfaction of the Local Planning Authority prior to the dwellings hereby approved being occupied and thereafter maintained at all times.
Reason. To ensure adequate pedestrian access arrangements from the adopted highway, in the interests of pedestrian safety.
7. The turning facilities indicated on approved layout plan shall be provided before

the development is first occupied and the areas used for the manoeuvring of vehicles in each parking area shall subsequently be maintained free of obstruction at all times.

Reason. To ensure adequate pedestrian access arrangements from the adopted highway, in the interests of pedestrian safety.

8. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use to the written satisfaction of the Local Planning Authority prior to the development hereby approved being occupied.

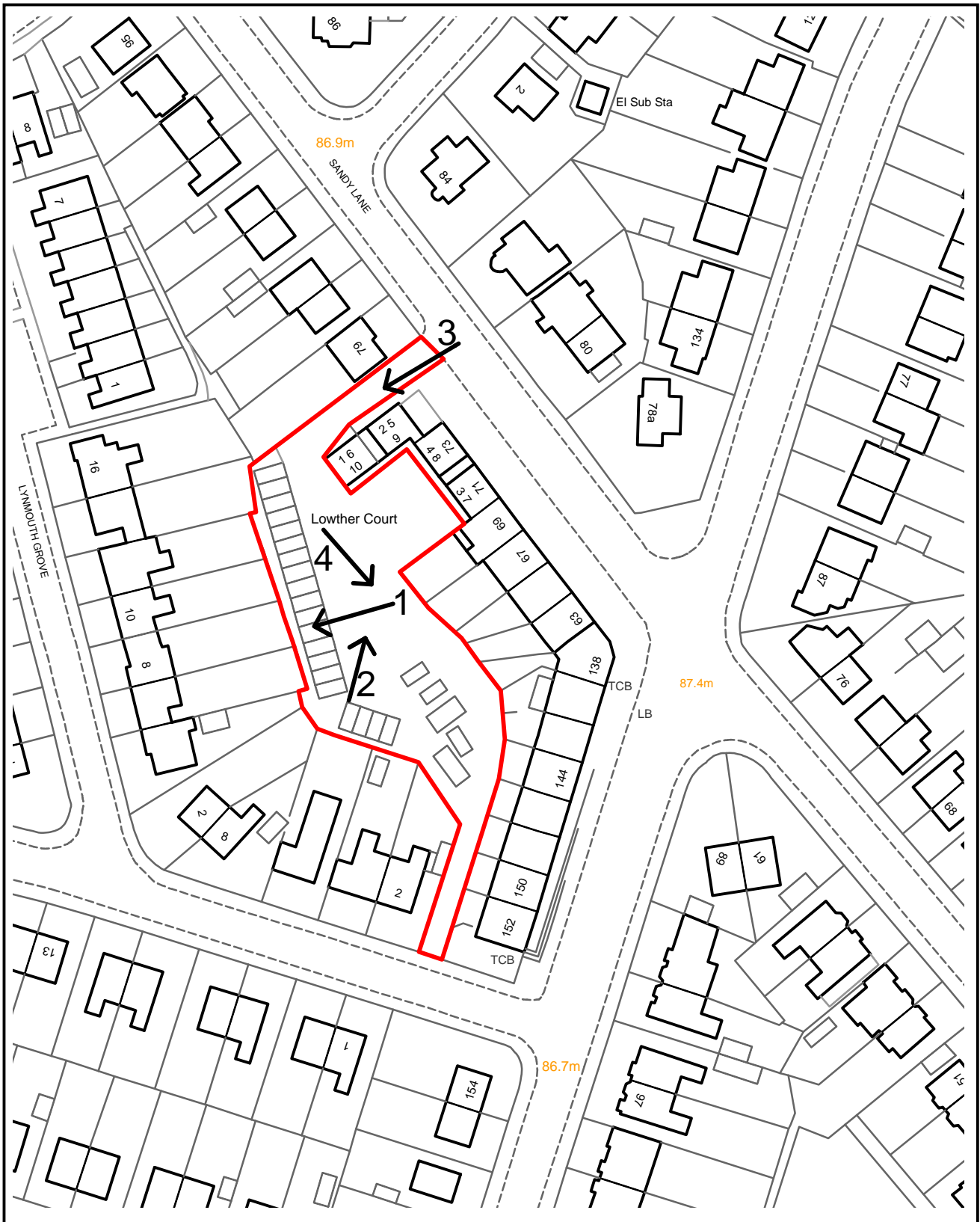
Reason. To ensure adequate off street car parking provision in the interests of road safety.

9. Prior to occupation of the proposed houses, the entrance gates onto Sandy Lane shall be removed to the satisfaction of the Local Planning Authority.

Reason. In the interest of highway safety pursuant to UDP Policy and guidance.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60997

**ADDRESS: Land to The Rear Of Sandy Lane
Prestwich**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60997

Photo 1



Photo 2



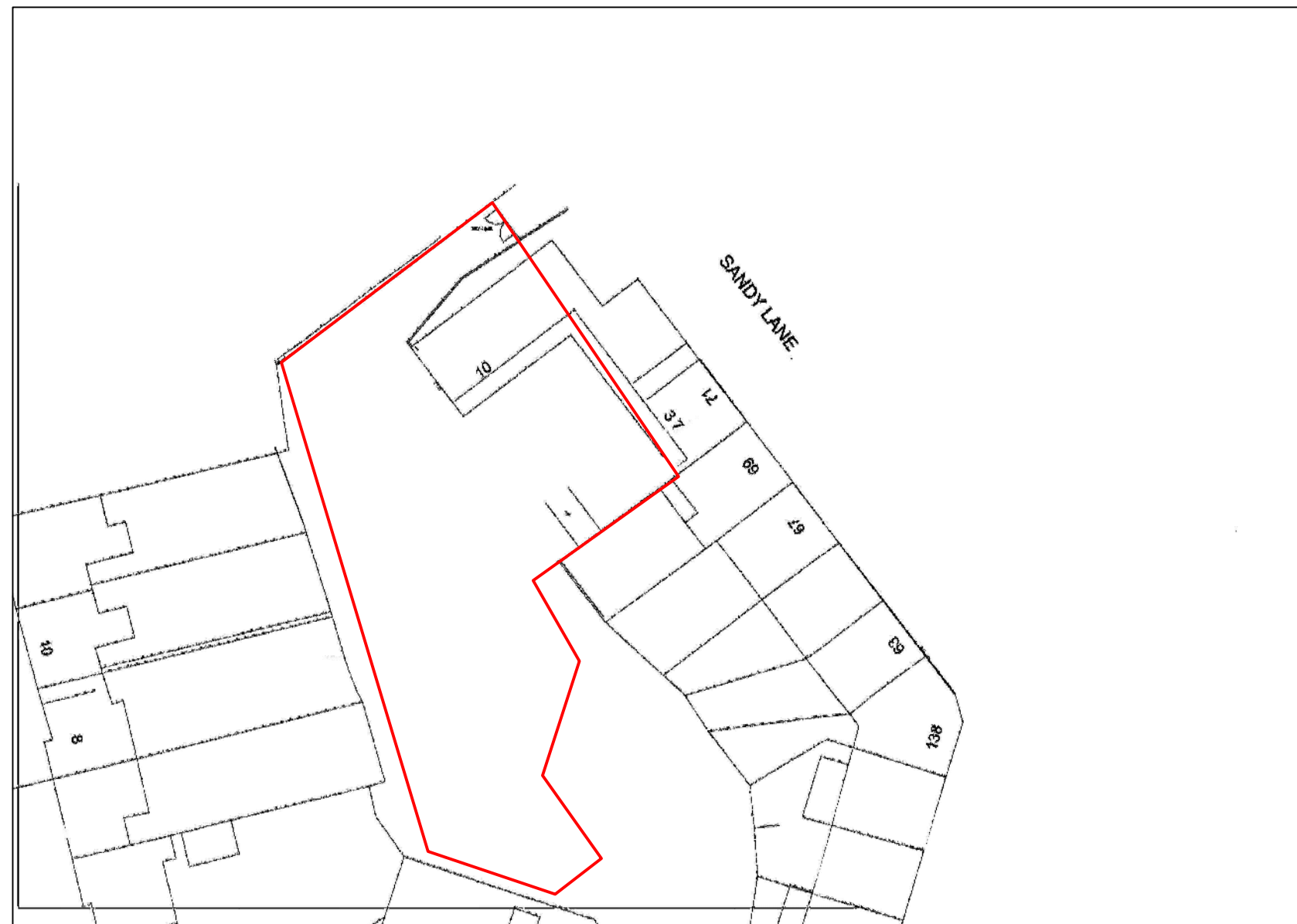
60997

Photo 3



Photo 4





Site Plan

General Notes

No.	Revision/Issue	Date

Firm Name and Address
 Steve Costello
 76 Riverside Drive
 Stoneclough Prestolee
 Manchester 26
 M26 1HY
 Tel: 07806777251

Project Name and Address
 Property Capital Investments Ltd
 15 Mayfair
 Mayfield Road
 Salford
 M7 3WZ

Project 1 pair Semi Detached Houses	Sheet
Date 29.11.2016	
Scale 1 : 500	



900mm Front Waynelayp Fence

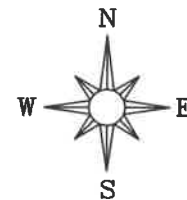
1.2m Boundary Fence

Existing Masonry Wall

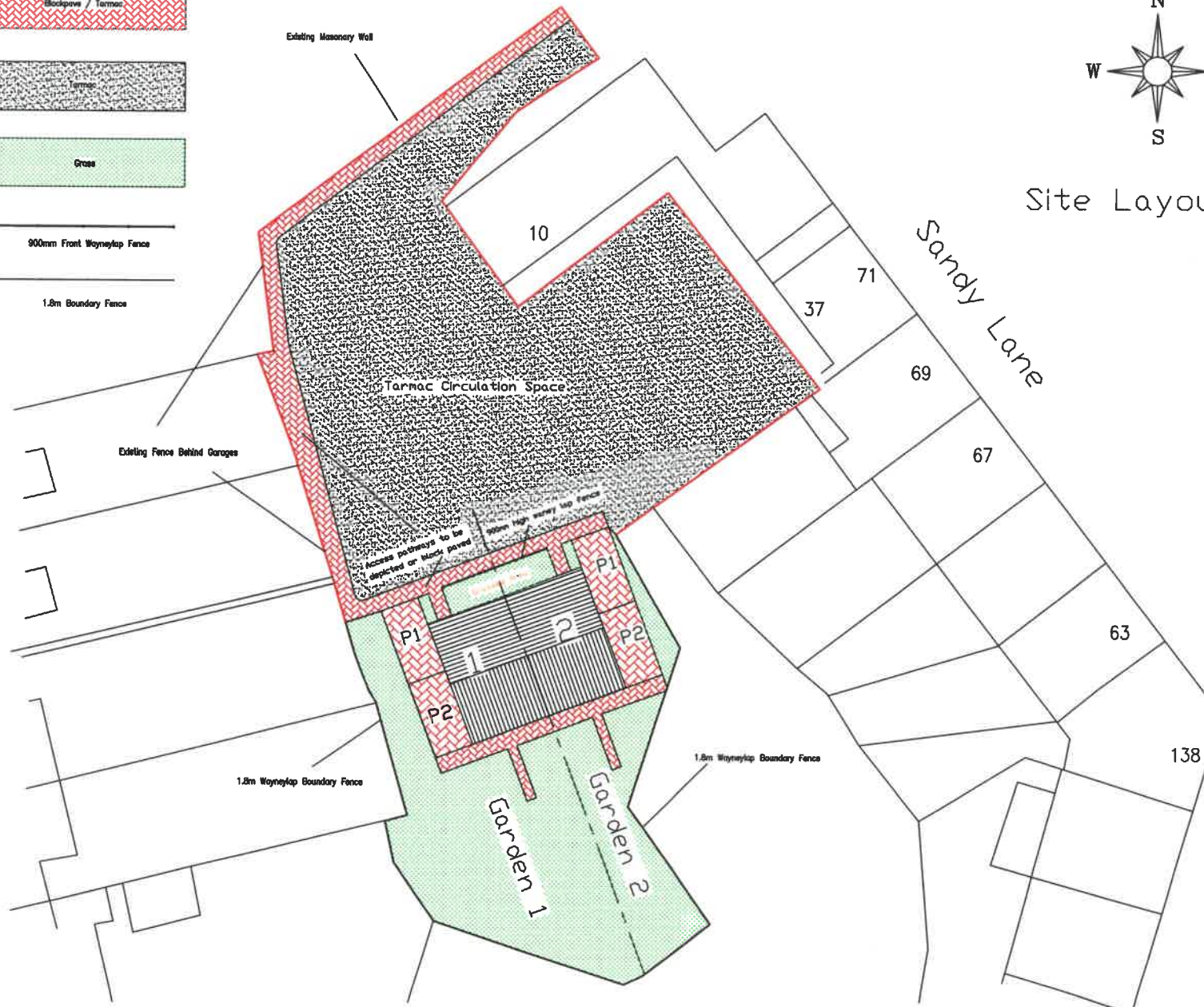
Existing Fence Behind Garages

1.2m Waynelayp Boundary Fence

1.2m Waynelayp Boundary Fence



Site Layout



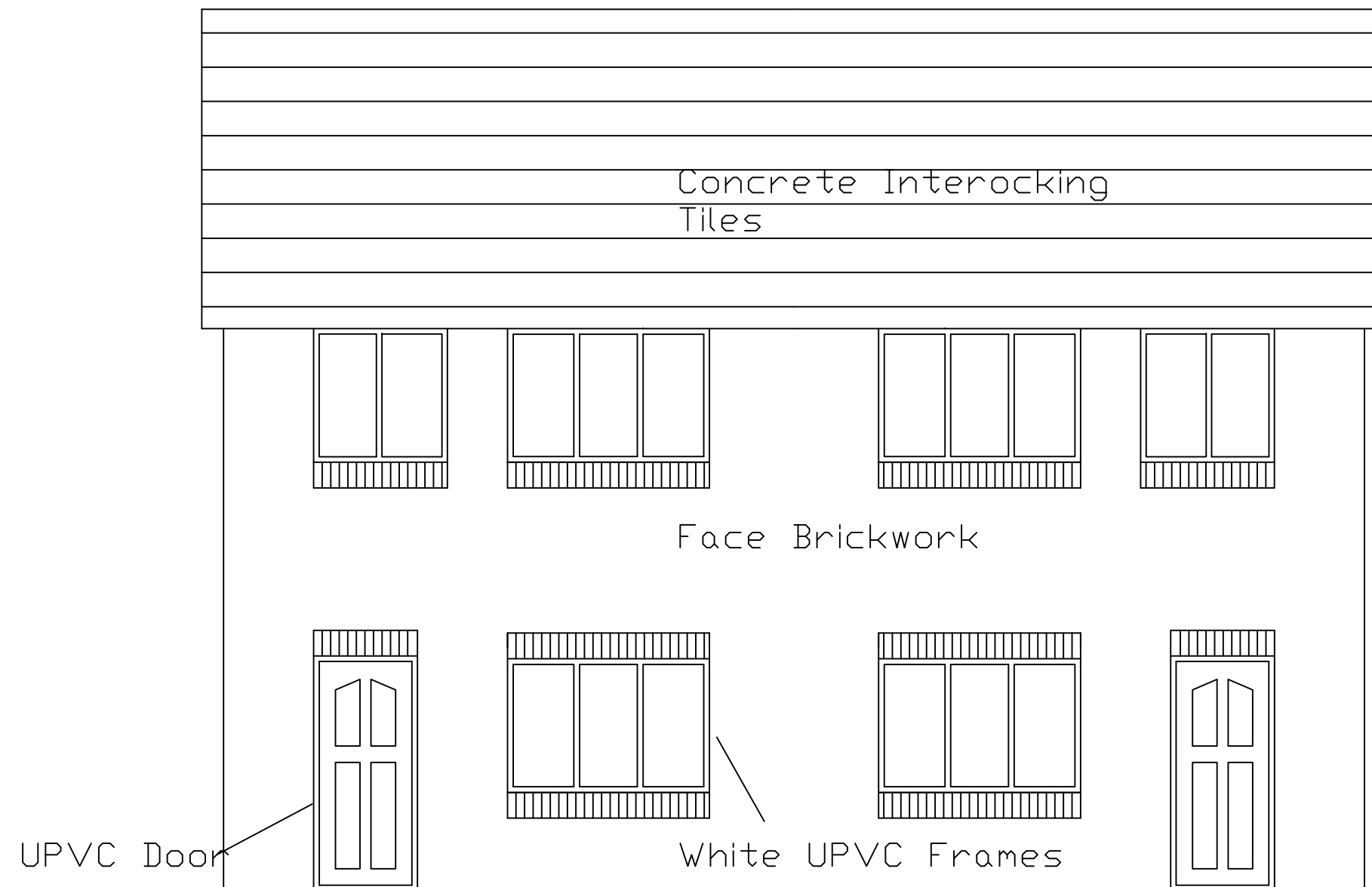
General Notes
Scale Revised 05.02.2017

No.	Revision/Issue	Date

Plot Name and Address
Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Manchester M26 1HY
Tel: 07806777251

Project Name and Address
New Build Development
Sandy Lane
Prestwich

Plot Name and Address
New Build Development
Sandy Lane Prestwich
Date 17th January 2017
Scale 1 : 250



Front Elevation

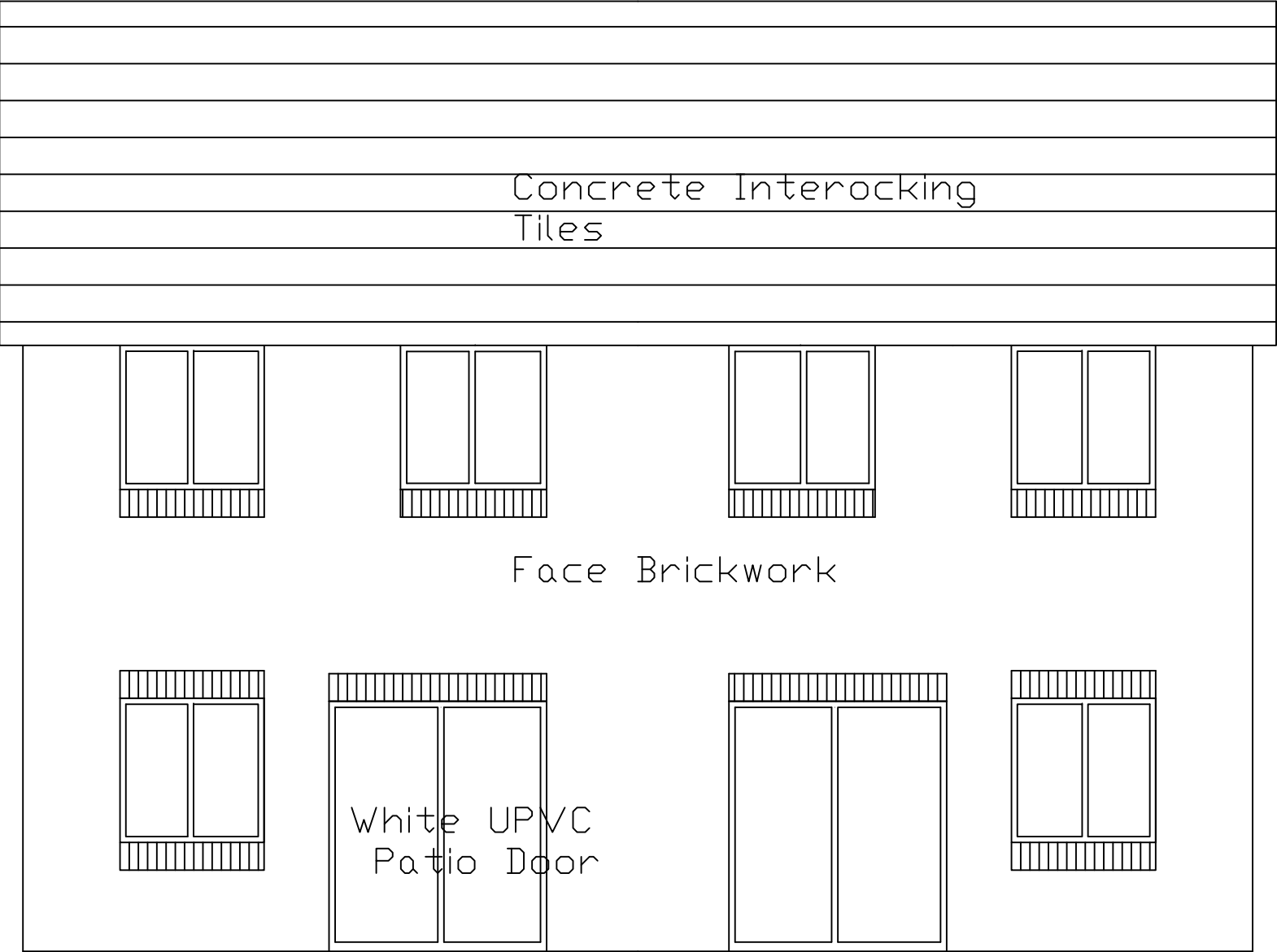
General Notes

No.	Revision/Issue	Date

Firm Name and Address
Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Radcliffe
M26 1HY

Project Name and Address
Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

Project	Sheet
1 pair Semi Detached Houses	1
Date	29.11.2016
Scale	1 : 50



Rear Elevation

General Notes

No.	Revision/Issue	Date

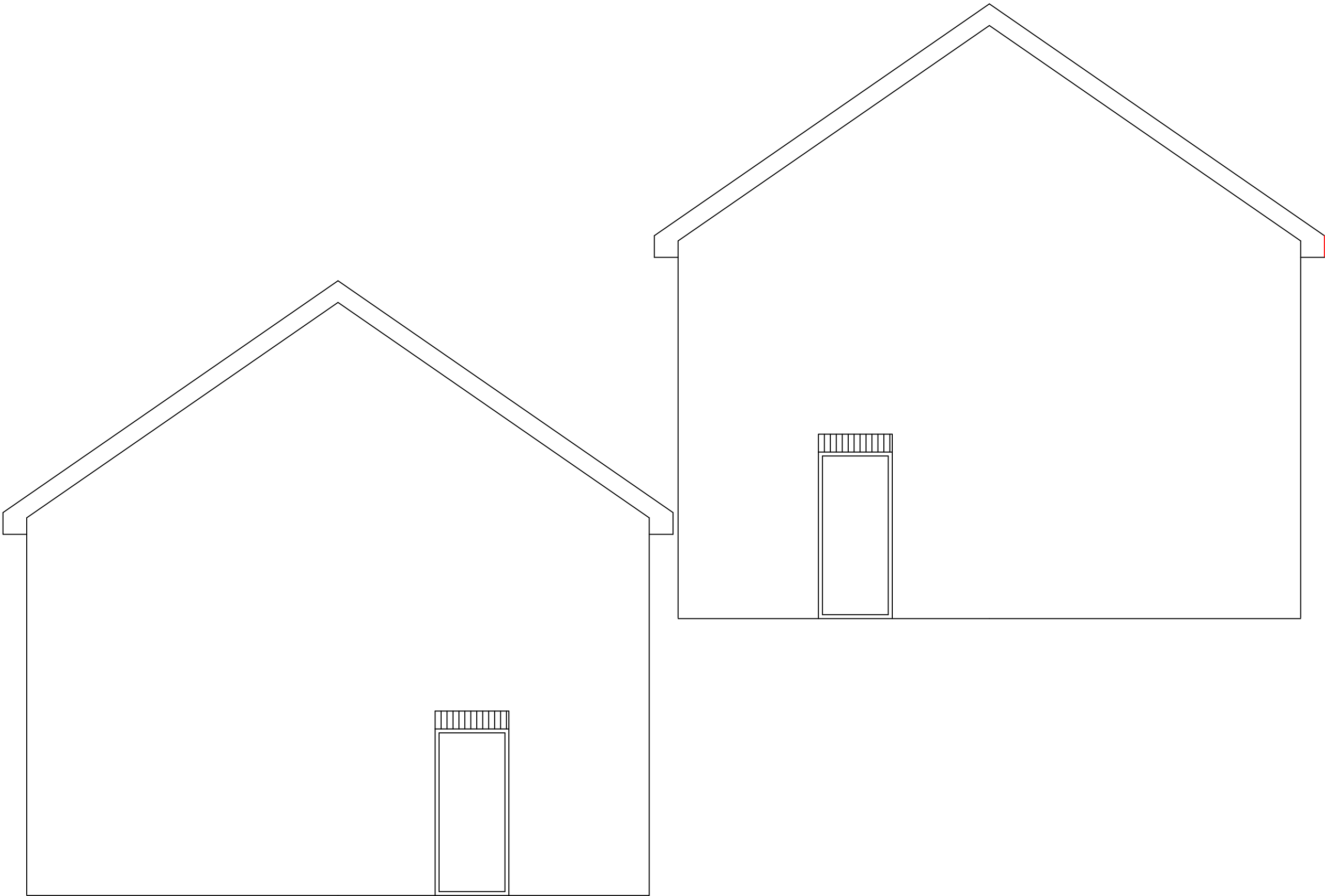
Firm Name and Address

Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Radcliffe
M26 1HY

Project Name and Address

Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

Project 1 pair Semi Detached Houses	Sheet 2
Date 29.11.2016	
Scale 1 : 50	



Side Elevation

General Notes

No.	Revision/Issue	Date

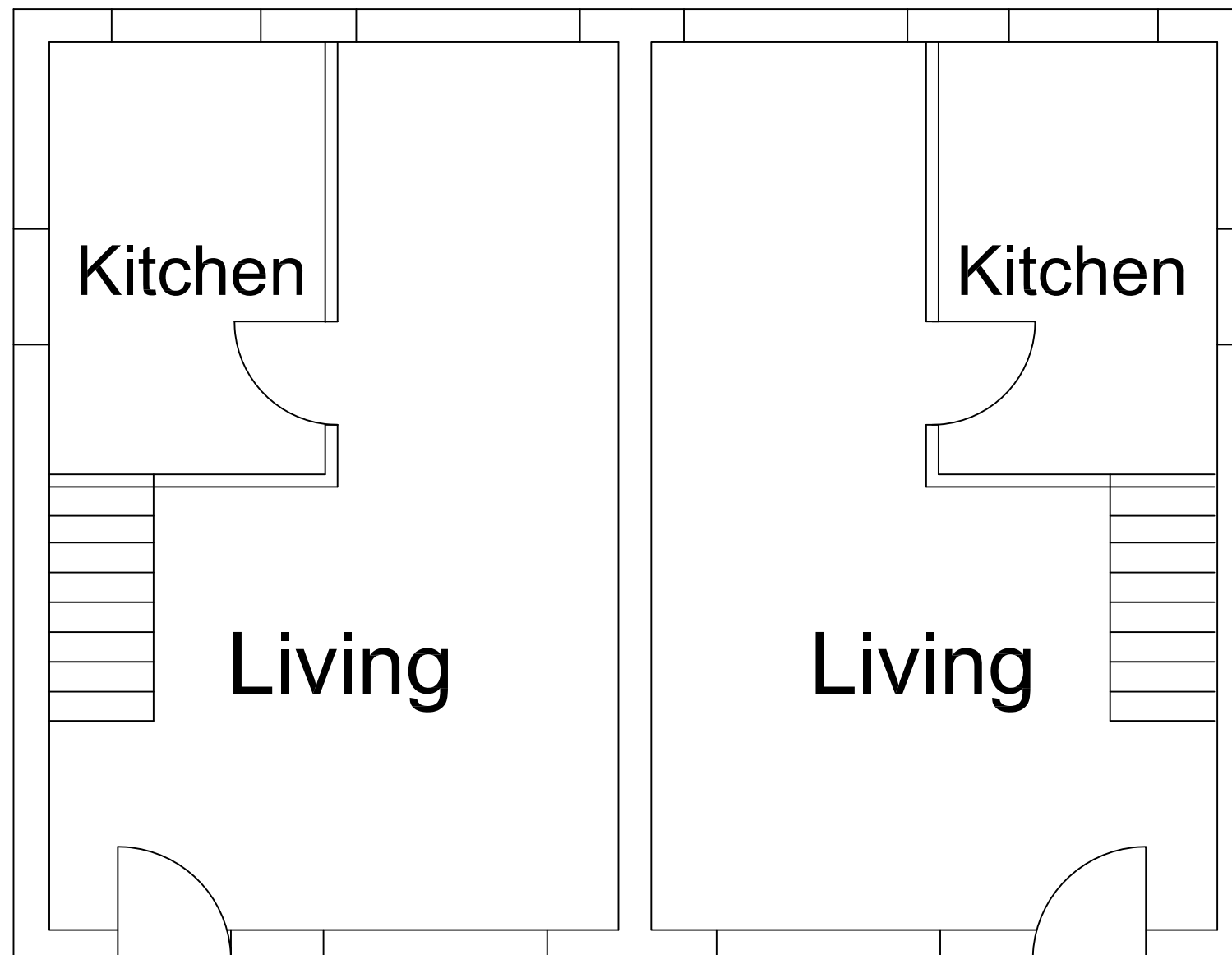
Firm Name and Address

Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Radcliffe
M26 1HY

Project Name and Address

Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

<div>Project</div> <div>1 pair Semi Detached Houses</div>	<div>Sheet</div> <div>3</div>
<div>Date</div> <div>29.11.2016</div>	
<div>Scale</div> <div>1 : 50</div>	



Ground Floor

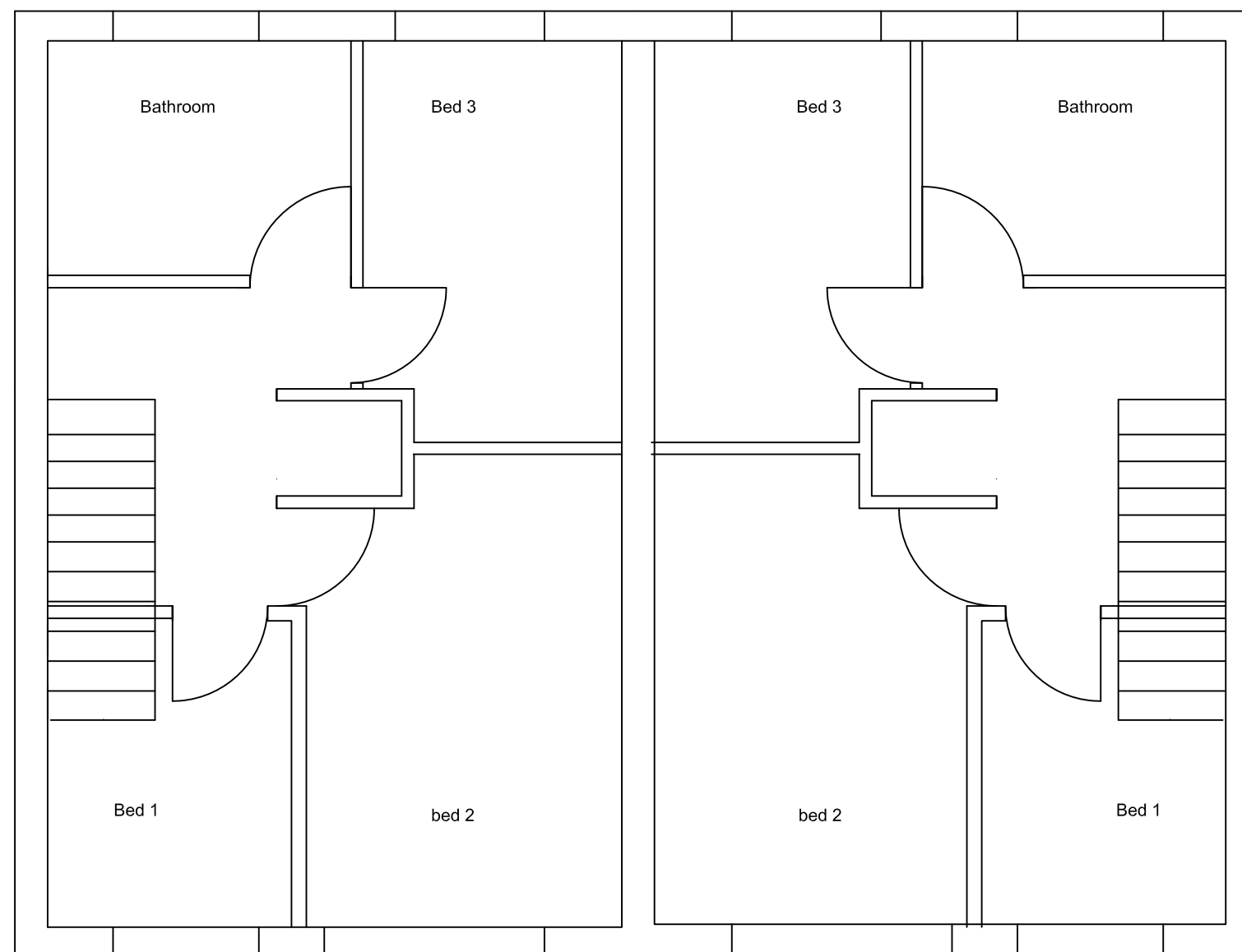
General Notes

No.	Revision/Issue	Date

Firm Name and Address
Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Radcliffe
M26 1HY

Project Name and Address
Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

Project 1 pair Semi Detached Houses	Sheet 4
Date 29.11.2016	
Scale 1 : 50	



First Floor

General Notes

No.	Revision/Issue	Date

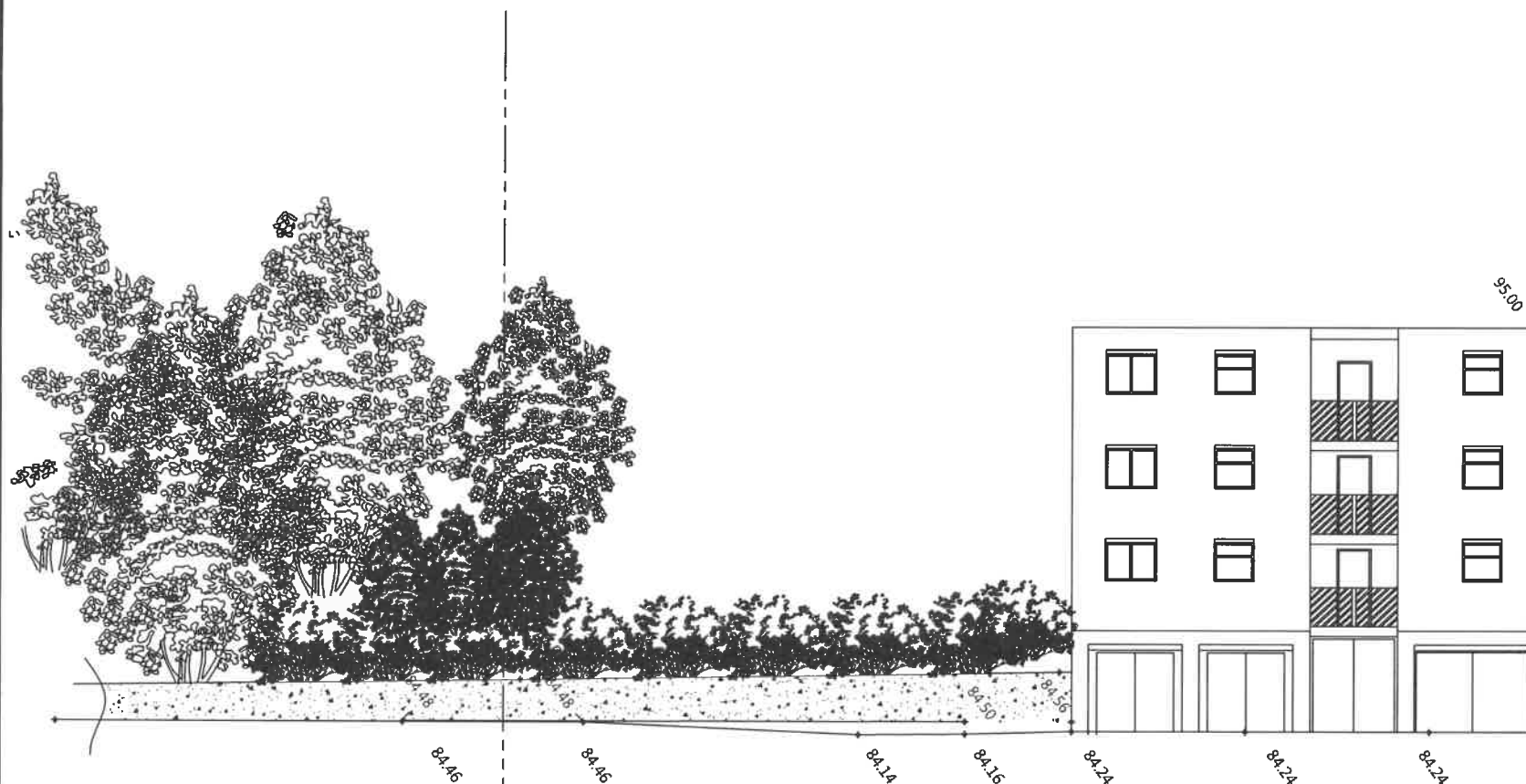
Firm Name and Address
 Steve Costello
 76 Riverside Drive
 Stoneclough Prestolee
 Radcliffe
 M26 1HY

Project Name and Address
 Property Capital Investments Ltd
 15 Mayfair
 Mayfield Road
 Salford
 M7 3WZ

Project
 1 pair Semi Detached Houses
 Date
 29.11.2016
 Scale
 1 : 50

Sheet

5



West To East Section

General Notes

No.	Revision/Issue	Date

Plan Name and Address
 Steve Costello
 76 Riverside Drive
 Stoneclough Prestolee
 Radcliffe
 M26 1HY

Project Name and Address
 Property Capital
 Investments Ltd
 15 Mayfair
 Mayfield Road
 Salford
 M7 3WZ

Project
 1 Unit Semi Detached
 HOUSE
 Date
 09.01.2017
 Scale
 1 : 1250

Sheet
2

Topographical Existing



General Notes

No.	Revision/Issue	Date

Firm Name and Address
Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Manchester 26
M26 1HY
Tel: 07806777251

Project Name and Address
Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

Project	Sheet
1 pair Semi Detached Houses	
Date	29.11.2016
Scale	1 : 500

Topographical Proposed



General Notes

No.	Revision/Issue	Date

Firm Name and Address

Steve Costello
76 Riverside Drive
Stoneclough Prestolee
Manchester M26 1HY
Tel: 07806777251

Project Name and Address

Property Capital Investments Ltd
15 Mayfair
Mayfield Road
Salford
M7 3WZ

Project

1 pair Semi Detached Houses

Sheet

Date

29.11.2016

Scale

1 : 500

Ward: Prestwich - Sedgley

Item 09

Applicant: Kollel Heichal Hatorah

Location: 45 Bury Old Road, Prestwich, Manchester, M25 0EY

Proposal: Extension and change of use to existing dwelling to form first floor self contained flat with post graduate study centre on ground floor; Formation of new vehicular access from Kings Road

Application Ref: 61022/Full

Target Date: 10/03/2017

Recommendation: Approve with Conditions

Description

The application is a resubmission following a refusal for a similar scheme in August last year - see Relevant Planning History.

The site comprises a two storey semi-detached dwellinghouse on the corner of Bury New Road and Kings Road. The existing access onto the driveway is from a point near the north east corner. The site is bounded to the south side, to the rear and to the east, across Bury New Road by houses. To the north, across Kings Road is a row of shops forming the neighbourhood centre.

The proposal comprises;

- a single storey extension to the side and rear,
- the conversion of the property to form a flat at first floor with an enclosed access stair at the rear,
- the conversion of the ground floor to a teaching/study facility, known as a Kollel, and,
- closure of the existing vehicular access at the front/corner and creation of a new vehicular access at the rear/side.

The extension would project out 5m from the side/north elevation and run back 12.5m from the front elevation, 4.5m past the existing rear elevation. The extension to the side would have a hipped roof and this would drop down to a flat roof at the rear. The extension would be finished in brick and tile to match the existing building.

The existing access on the corner of Kings Road and Bury Old Road would be closed off and a pedestrian access formed in its place with the footway and kerbs reinstated. A new access point onto Kings Road would be formed at the rear with space for parking two vehicles with a turning facility and a cycle store.

The extended ground floor would comprise a main study area (approx 70sqm), new side entrance, WC, cloakroom and small library. This element would fall within the D1 Use Class. The residential accommodation on the upper floor would be for post graduate students and comprise a bedroom, living room, kitchen diner and bathroom.

The proposed hours of opening are set out in the application form as follows:

0930 - 1930 Monday to Friday and Sundays and Bank Holidays

0900 - 1300 Saturday

It is stated in the supporting letter that at least 90% of those attending would live within walking distance of the centre. According to the agent, the Kollel Heichal Hatorah is an institute for Post Graduate Rabbinical Study established some 15 years ago. It has developed into a centre of excellence in attracting students from around the world. To date it

has functioned in rented accommodation within the area but this application is an opportunity to purchase its own premises.

Relevant Planning History

01927/E - Conversion of semi detached dwelling to ground floor study hall and first floor flat - Enquiry completed 13/12/2016

60323 - Extension to existing dwelling to form ground floor self contained flat for 3 persons and first floors to be used as post - graduate study centre - Refused 24/08/2016 for the following reasons (summarised).

- The proposed parking arrangements are sub-standard and are likely to lead to conflicts between pedestrians and vehicles at a hazardous point on the highway network.
- The proposed car parking provision of one disabled parking space for the uses proposed is inadequate and detrimental to the free flow of traffic and road safety.
- The proposed two storey extension would be seriously detrimental to the visual amenity of the streetscape by reason of its height, size and position.

Publicity

The following neighbours were notified by letter dated 18/01/17. Nos.121, 122, 123, 124,126, 128, 130 Kings Road, Nos.2-12(even) Dellcot Close, Nos.31-55(odd) and 32, 34, 36 and 48 Bury Old Road, 1 and 3 Danesway, 125-141 Albert Avenue, 1, 2A and 3 Castle Hill Road, 2 East Meade.

Objections received from residents at Nos.37 and 43(attached semi) Bury Old Road and 4 Dellcot Close. The objections are summarised below:

- The proposal doesn't differ greatly from the previous refusal and therefore should be similarly refused.
- The new access will increase risks to pedestrians and other vehicles close to what is an already busy and dangerous junction.
- There are other uses in the area that add to existing traffic problems (shops, school, day nursery, medical centre, synagogue and mosque).
- Vehicles coming and going from Kings Road have difficulties already due to visibility, traffic flow and illegal parking. The proposal would add to this.
- The proposal does not have sufficient on-site parking and there is little on-street parking or public parking nearby.
- Students using cars would end up blocking accesses and causing further problems.
- Over time, other parts of the site would be used to park cars and the front entrance would revert back to a vehicular access.
- Increase in noise levels from the proposed activity would cause further disturbance, particularly to the attached neighbour at No.43 Bury Old Road. There may be chanting, singing and debates which would increase noise.
- Over-intensive use of the site.
- The use would be better in a detached property and not a semi-detached one.
- Housing in this area is difficult to come across. This institution should go to a different location that does not impact the wider community and allow this house to be used as a home, as intended. It could be located in an existing synagogue or Shul.
- The extensions are disproportionate to other houses in the row.
- The fencing does not match the style of other nearby fences and would be a blight to the area.
- Are toilet facilities sufficient for a facility of this size and concerns about sewage treatment.
- Mixing residential purposes and religious studies may not be compatible and would be unsuitable.
- A religious building in such close proximity, would increase risk of potential anti semitic attacks.
- A group of men living together where families with young children reside could cause child endangerment in various ways.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - Concerns relating to parking. Final comments will be reported in the Supplementary Report.

Drainage Section - No objection

Environmental Health - Comments awaited

Unitary Development Plan and Policies

EN1/2	Townscape and Built Design
H2/3	Extensions and Alterations
CF2	Education Land and Buildings
HT2/4	Car Parking and New Development
SPD11	Parking Standards in Bury
SPD6	Supplementary Planning Document 6: Alterations & Extensions
NPPF	National Planning Policy Framework
CF1/1	Location of New Community Facilities
EN7/2	Noise Pollution
H3/1	Assessing Non-Conforming Uses

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

National Planning Policy Framework - Section 8 of the NPPF, relates to community and highlights the need to have good community/ cultural facilities. Section 4 relates to transport and states that developments that generate a significant amount of movement should exploit opportunities for the use of sustainable transport modes and aim for a balance of land uses so that people can be encouraged to minimise journey lengths.

UDP Policy EN1/2 - Townscape and Built Design. The Council will give favourable consideration to proposals which do not have an unacceptable adverse effect on the particular character and townscape of the Borough's towns, villages and other settlements. Factors to be considered when assessing proposals will include:

- a) the external appearance and design of the proposal in relation to its height, scale, density and layout;
- b) the relationship of the proposal to the surrounding area;
- c) the choice and use of materials;
- d) access and other design features for the mobility impaired;
- e) the design and appearance of access, parking and service provision;
- f) landscaping, including the use of natural landscape features, and open space provision;
- g) the use of lighting.

H2/3 Extensions and Alterations. Applications for house extensions and alterations will be considered with regard to the following factors:

- a) the size, shape, design and external appearance of the proposal;
- b) the character of the property in question and the surrounding area;
- c) the amenity of adjacent properties;
- d) visibility for pedestrians, cyclists and drivers of motor vehicles.

CF1/1 Location of New Community Facilities. Proposals for new and improved community facilities will be considered with regard to the following factors:

- a) impact on residential amenity and the local environment;

- b) traffic generation and car parking provision;
- c) the scale and size of the development;
- d) where applicable, access to shops and other services;
- e) if the use is intended to serve a local community, or catchment area, the suitability of the chosen location in relation to that community or catchment area;
- f) accessibility by public and private transport;
- g) the needs and requirements of the disabled;

Policy CF2 Education Land and Buildings. The Council will, where appropriate, consider favourably proposals for the provision, improvement and dual use of educational facilities.

H3/1 Assessing Non-Conforming Uses. The Council will assess proposals for the development of non-conforming uses in primarily residential areas and will not permit proposals considered to be incompatible. Factors which will be taken into account when assessing such proposals will include noise, vibration, smell, fumes, smoke, soot, ash, grit, dust, visual intrusion, traffic generation and parking arrangements, and hours of operation.

EN7/2 Noise Pollution. In seeking to limit noise pollution the Council will not permit:

- a) development which could lead to an unacceptable noise nuisance to nearby occupiers and/or amenity users;
- b) development close to a permanent source of noise.

HT2/4 Car Parking and New Development. The Council will require all applications for development to make adequate provision for their car parking and servicing requirements in accordance with the Council's car parking standards.

Need - The proposed development would be located in the heart of the community that the proposed development would serve and would be of a scale that would be appropriate. The issues of residential amenity, parking and access issues will be addressed later in the report. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy CF1/1 of the Bury Unitary Development Plan.

Visual amenity - The extension is single storey and set in from the side boundary with Kings Road. With the existing boundary hedge, the extension with the hipped roof would not appear particularly prominent within the streetscape. The flat roof extension at the rear is at least 6m from the rear boundary with Dellcot Close and again, the existing boundary hedge would help screen the new build from the road.

The proposal, in terms of size and appearance, would not have a seriously detrimental impact on the streetscape and would generally comply with UDP Policy EN1/2 Townscape and Built Design.

Residential amenity - This area of Bury Old Road is busy and relatively noisy, however undoubtedly there would be increased activity in and around the site throughout the day. From what the applicant has stated, the activities within the study centre would not be particularly noisy or cause undue disturbance to the attached neighbour. Notwithstanding this, given the noise sensitive residential use within the attached property, it is considered appropriate to attach a condition requiring the party walls of the property to be treated to reduce noise outbreak in accordance with current building regulations standards.

The hours of opening, between 9.30am and 7.30pm on most days is not particularly unneighbourly and should not cause serious harm to amenity. The proposal is to open on a Sunday, being a normal day within the Jewish faith. The concern is that the Sunday opening may well cause issues if the neighbour is not Jewish. The hours of opening are set out in an appropriate condition but can be amended should the Planning Control Committee decide to do so.

It is noted that the study use falls with the wider D1 Non-residential use class which also includes public halls, places of worship, creches etc. To prevent the space being used for

these more noisier functions, it would be reasonable to attach a condition removing the 'permitted development' rights that would normally allow changes within the same use class and restrict the ground floor use to that of 'educational/study' use only. As such the proposal, on balance, complies with the NPPF and UDP Policies EN7/2 Noise Pollution and H3/1 Assessing Non-Conforming Uses and CF2 Education.

The extensions are set away from the shared boundary with the neighbour and there are no serious residential amenity issues arising from the side and rear extensions.

Parking and Access - The Council's parking standards, set out in SPD 11 expands upon parking policies within the UDP. The guidance note indicates the maximum standards for parking, which would be 1 space per 2 full time staff for a further education use. There are minimum standards for students using bicycles and motorbikes - 1 per 10 students and 1 per 600 students respectively.

The Traffic Section has some concerns that there is limited off-street parking in an area that has ongoing parking issues. The proposed car parking provision of two parking spaces with some cycle parking is minimal to say the least.

It is stated by the applicant that at least 90% those attending the centre are local residents within walking distance and that the proposed parking is adequate. However with up to 25/30 students attending the centre at any one time, this is not a small facility within the locality. Whilst it may well be the case that many of those attending are local, they may not necessarily come on foot. Given the parking problems in and around Kings Road, there remains a need to take account of those using their car and balance out the cultural benefits of the centre to the community which it serves and weighed against the disbenefits in terms of parking.

Given the above concerns in relation to parking, it is considered appropriate to attach a condition requiring a Travel Plan to be submitted prior to commencement of development. This would set out suitable measures that can be employed to reduce private vehicular use and promote more sustainable methods of travel to and from the site - walking, cycling, public transport, car sharing etc.

On balance, the proposed development, with the inclusion of a condition relating to a travel plan, would not be detrimental to residential amenity and issues of parking and highway safety.

Objections - Most of the objections set out in the 'Publicity' section above are planning related and are dealt with in the report. Those issues relating to potential anti-semitic behaviour and the presence of 'groups of men' are not considered to sustainable reasons for objection on planning grounds.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

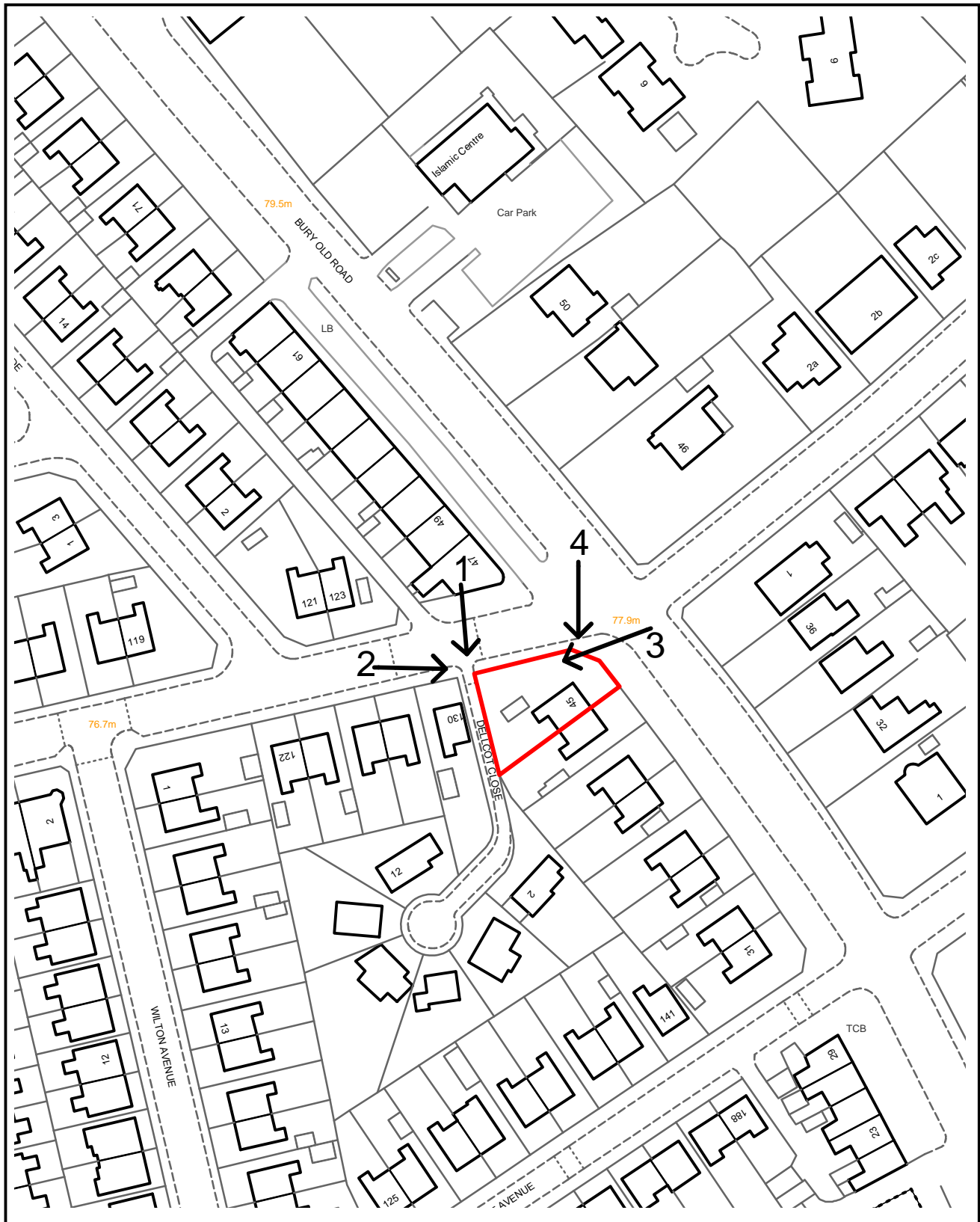
The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to location plan and drawings numbered location plan, 01, 02, 03, 04, 11/C, 12/A, 13/B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations and areas of hardstanding, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. The sound insulation of the party walls shall be improved in accordance with Building Regulations Approved Document E (2015), the details of which shall be submitted to and approved by the Local Planning Authority and implemented prior to the study centre hereby approved commencing and thereafter maintained.
Reason. Details have not been provided and to reduce nuisance from noise to the occupiers of the adjoining dwelling pursuant to NPPF and UDP Policy EN7/2 Noise Pollution.
5. The ground floor of property shall be used for educational study purposes only and not for community worship or any other use within the D1 use class.
Reason. In the interests of residential amenity pursuant to UDP Policy EN7/2 Noise Pollution.
6. The study centre hereby approved shall not be used outside the following times:
0900hrs to 1930hrs daily
Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to UDP Policies EN7/2 Noise Pollution, H3/1 Assessing Non-Conforming Uses.
7. Prior to the commencement of development, a Travel Plan, setting out measures to promote the greater use of sustainable travel to and from the site by staff and students/visitors and reduce the reliance on private vehicle use, shall be submitted to and approved in writing by the Local Planning Authority. The Travel Plan shall be implemented in full and thereafter maintained.
Reason. A Travel Plan has not been submitted and in order to deliver sustainable travel pursuant to the NPPF and UDP Policy HT4 New Development.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61022

**ADDRESS: 45 Bury Old Road
Prestwich**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61022

Photo 1



Photo 2



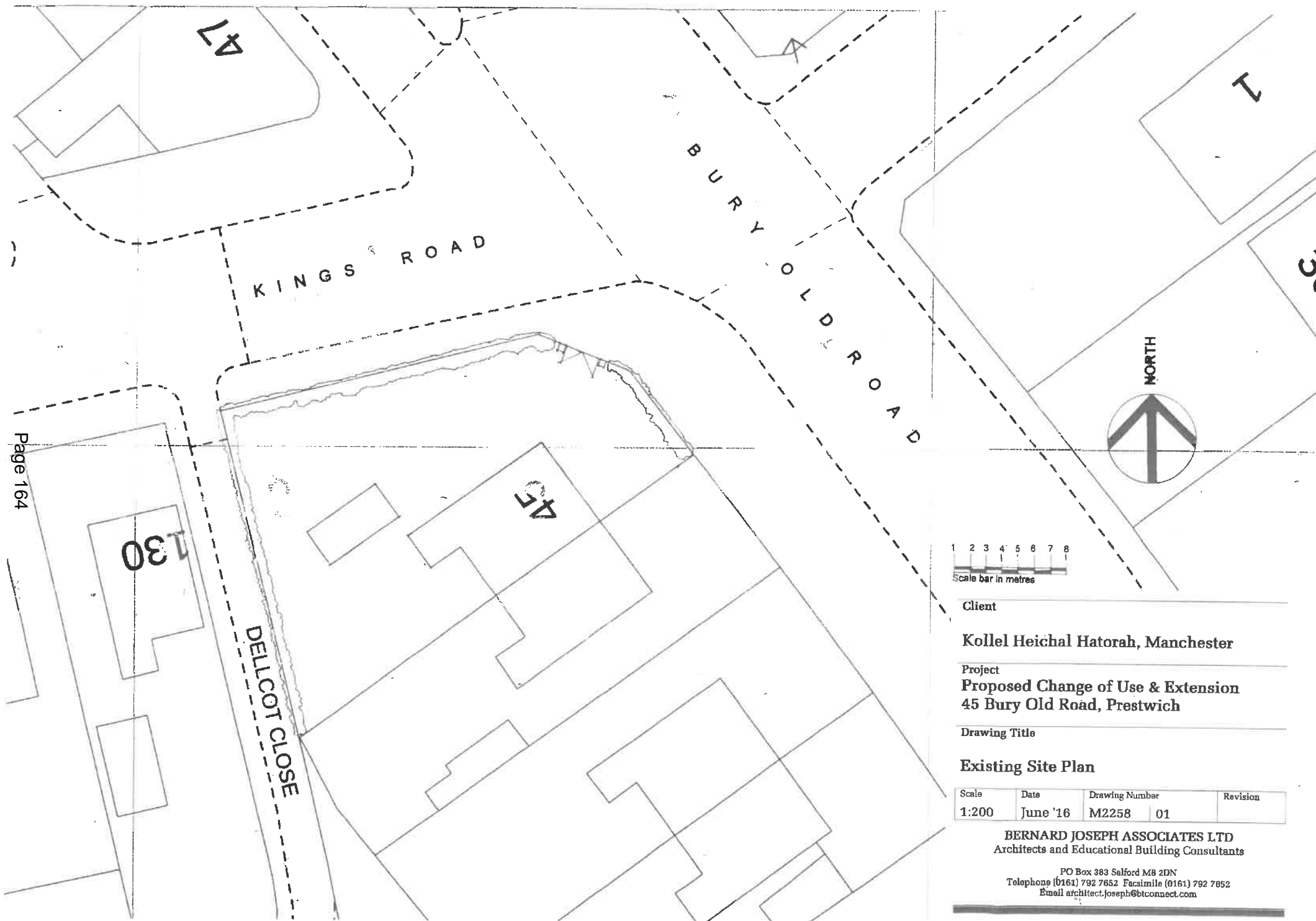
61022

Photo 3



Photo 4





Client

Kollel Heichal Hatorah, Manchester

Project

Proposed Change of Use & Extension
45 Bury Old Road, Prestwich

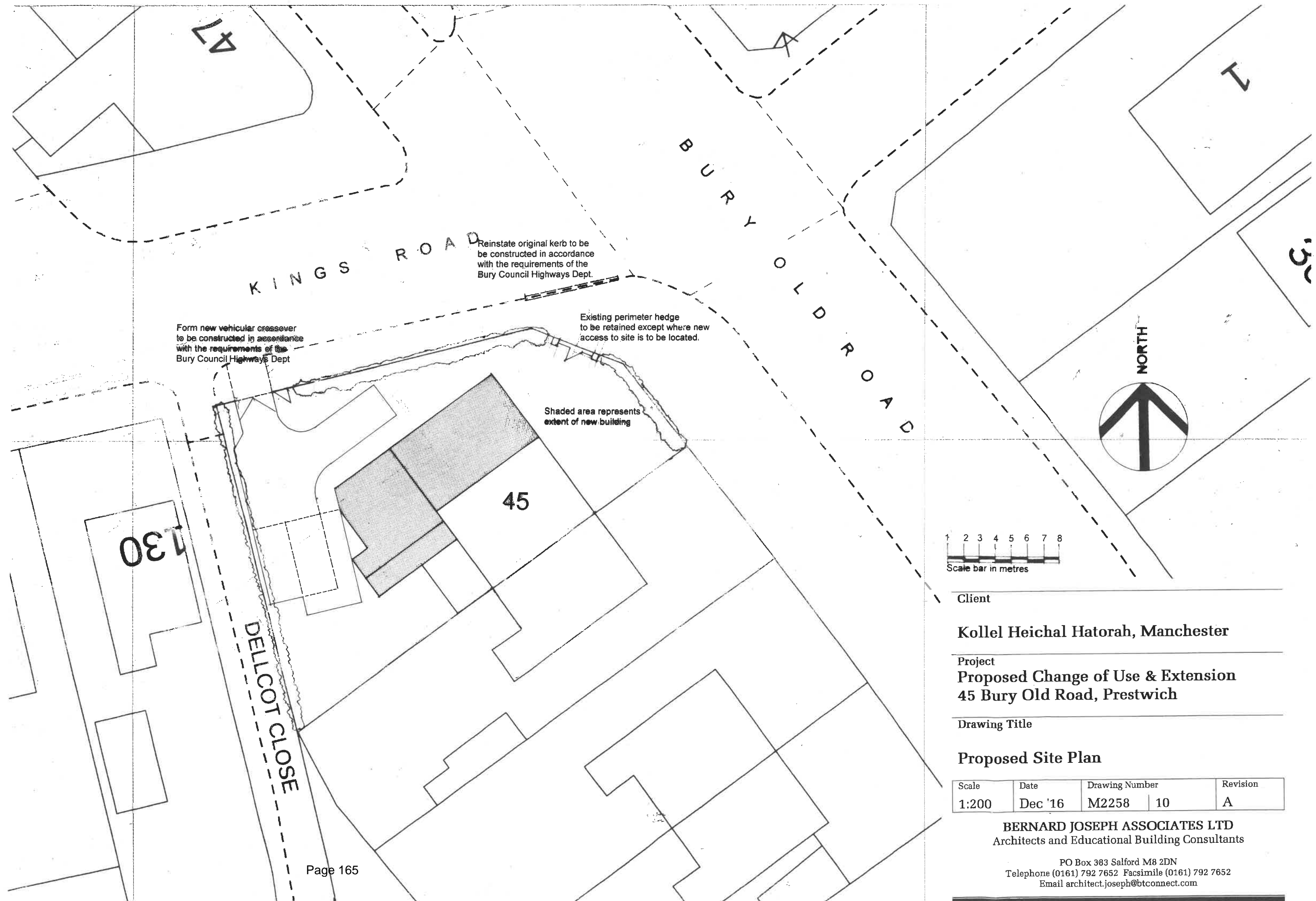
Drawing Title

Existing Site Plan

Scale	Date	Drawing Number	Revision
1:200	June '16	M2258 01	

BERNARD JOSEPH ASSOCIATES LTD
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Email architect.joseph@btconnect.com



Client

Kollel Heichal Hatorah, Manchester

Project

Proposed Change of Use & Extension
45 Bury Old Road, Prestwich

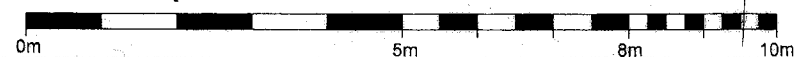
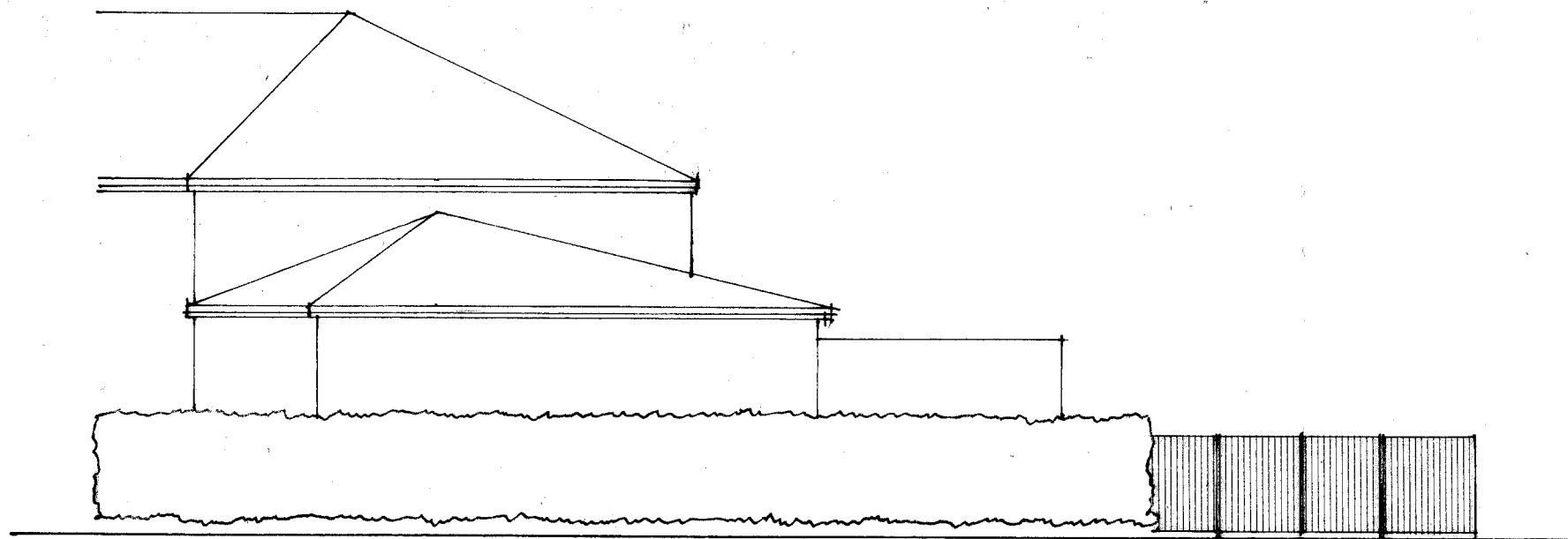
Drawing Title

Proposed Site Plan

Scale	Date	Drawing Number	Revision
1:200	Dec '16	M2258 10	A

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A3 Format @ 1:100 Scale

A Revision	Dec '16 Date	Redrawn Description
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1. Contractor is responsible for all setting out and must check dimensions on site before work is put in hand.
2. Written dimensions only to be taken. This drawing must not be scaled.
3. Architect to be immediately notified of suspected omissions or discrepancies.

Client

Kollel Heichal Hatorah, Manchester

Project

Proposed Change of Use & Extension
45 Bury Old Road, Prestwich

Drawing Title

Proposed Elevation to Kings Road

Scale 1:100	Date Jan '17	Drawing Number M2258 14	Revision
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DO NOT SCALE – IF IN DOUBT ASK

NOTES.

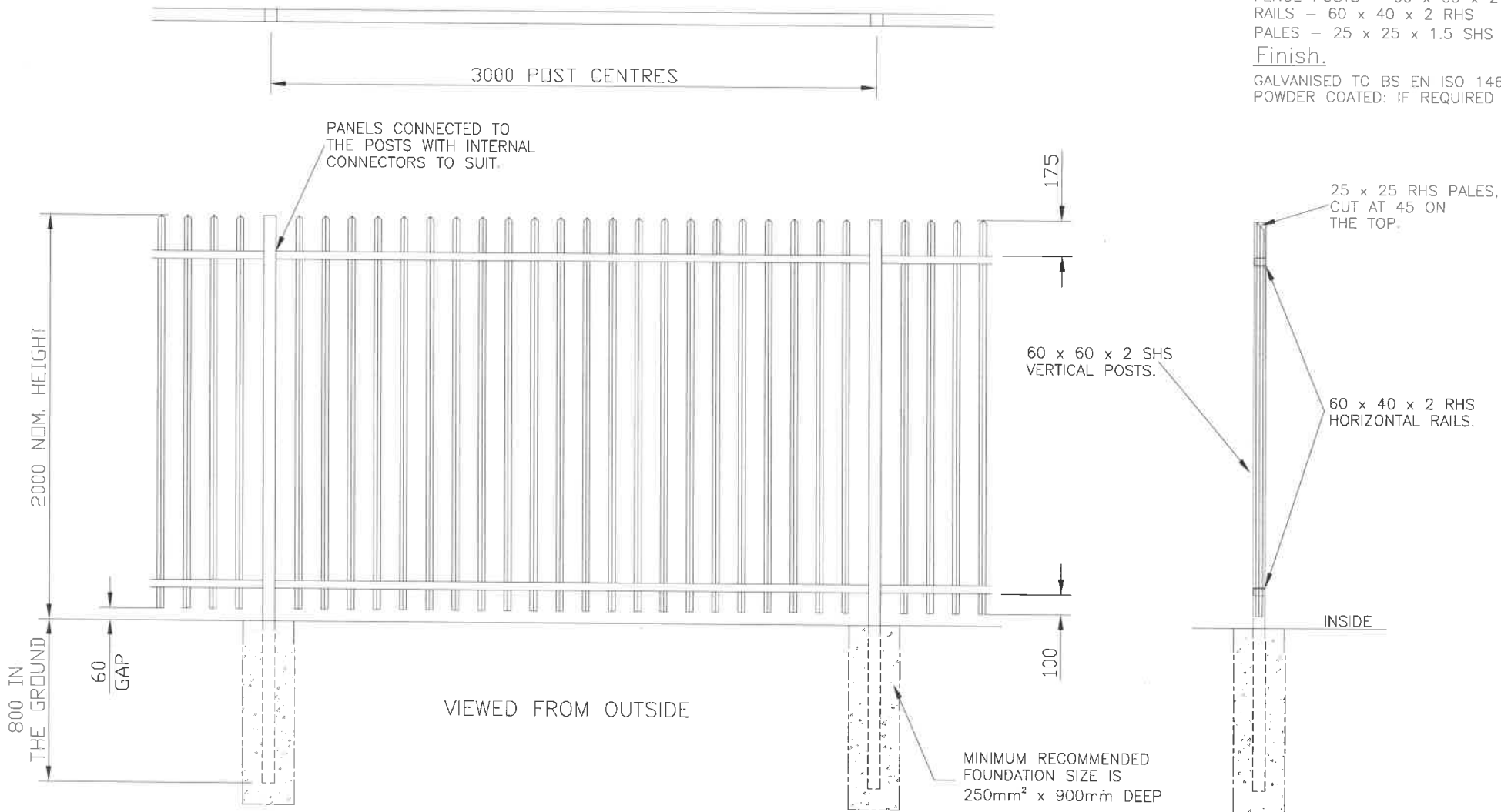
ALL DIMENSIONS ARE IN mm (U.N.O)
CAPS UNDERNEATH FENCING ARE
NOMINAL DUE TO GROUND LEVELS.

Materials.

FENCE POSTS – 60 x 60 x 2 SHS
RAILS – 60 x 40 x 2 RHS
PALES – 25 x 25 x 1.5 SHS

Finish.

GALVANISED TO BS EN ISO 1461:1999
POWDER COATED: IF REQUIRED



Jacksons
FINE FENCING

Stowting Common
Near Ashford,
KENT. TN25 6BN
Telephone : 01233 750393
Fax: 01233 750403
Int. Tel : +44 (0)1233 750393

Page 167

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27/07/06

ORIGINAL ISSUE

Drawn WB

Date 27/07/06

Checked

Size
A3

Scale 1:20
U.O.S

TITLE 2m HIGH BARBICAN FENCING WITH
DUG IN POSTS, SALES DRAWING.

CUSTOMER

ACK No.

Drawing No.

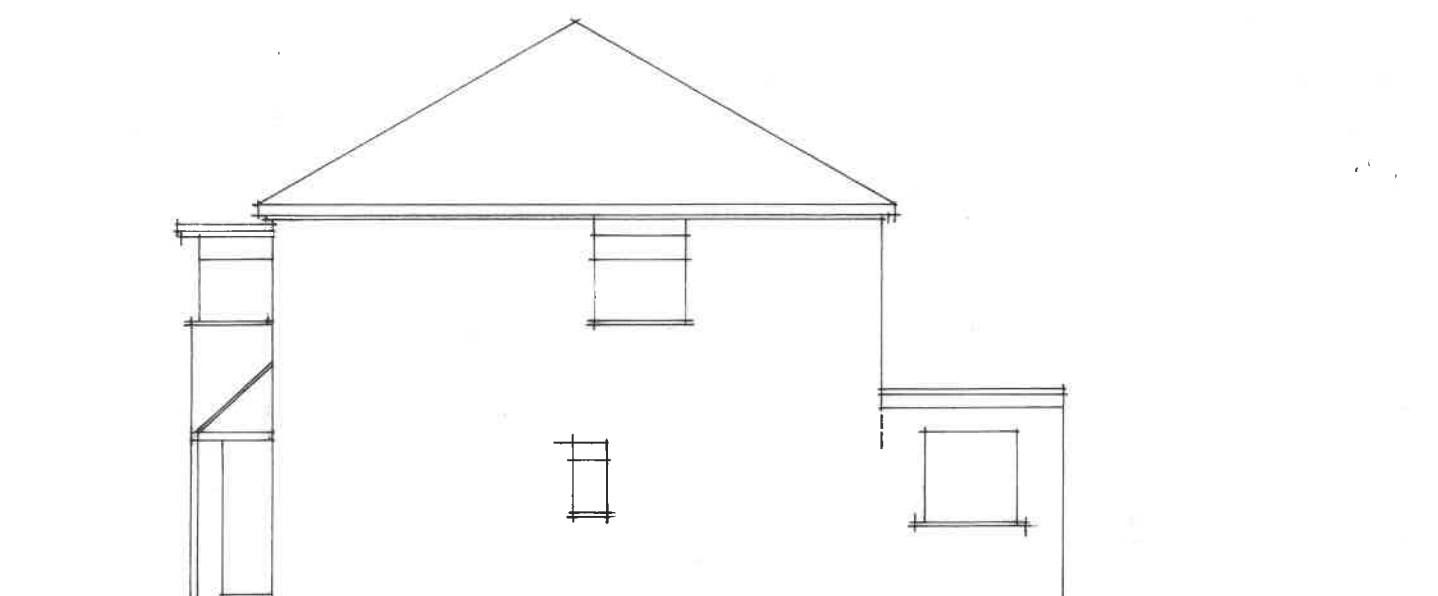
06 J1/01064

Sheet 4a of 14



WEST ELEVATION

EAST ELEVATION



NORTH ELEVATION

Scale:



Client

Kollel Heichal Hatorah, Manchester

Project

**Proposed Change of Use & Extension
45 Bury Old Road, Prestwich**

Drawing Title

Existing Elevations

Scale	Date	Drawing Number	Revision
1:100	June '16	M2258 04	

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WEST ELEVATION

EAST ELEVATION



NORTH ELEVATION

Scale:



Client

Kollel Heichal Hatorah, Manchester

Project

**Proposed Change of Use & Extension
45 Bury Old Road, Prestwich**

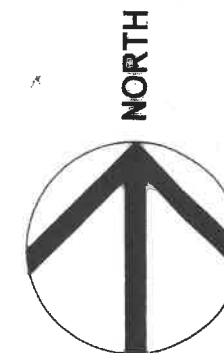
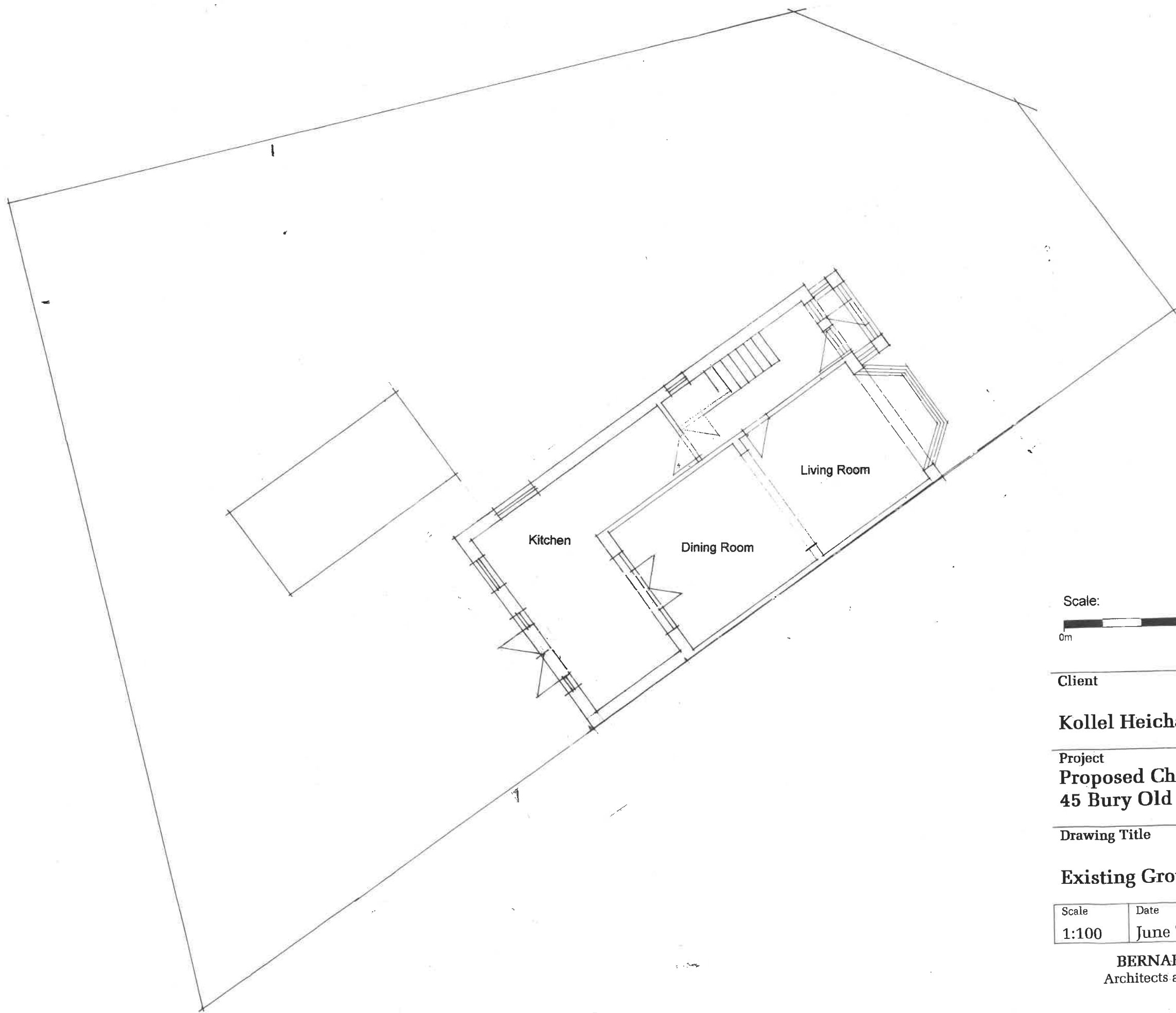
Drawing Title

Proposed Elevations

Scale	Date	Drawing Number	Revision
1:100	Dec '16	M2258 13	A

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Client

Kollel Heichal Hatorah, Manchester

Project
Proposed Change of Use & Extension
45 Bury Old Road, Prestwich

Drawing Title

Existing Ground Floor Plan

Scale	Date	Drawing Number		Revision
1:100	June '16	M2258	02	

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Form new vehicular and pedestrian access to site complete with 2 metre gates and fence as per detail.

6 no cycle spaces

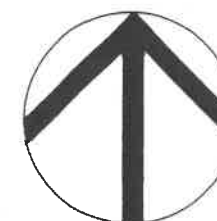
Existing gates to be removed and replaced with new 1.3 metre high metal fence and pedestrian gate as per detail.

Shaded area represents extent of new building

Study Hall

Library

NORTH



A3 Format @ 1:100 Scale

Client

Kollel Heichal Hatorah, Manchester

Project

Proposed Change of Use & Extension
45 Bury Old Road, Prestwich

Drawing Title

Proposed Ground Floor Plan

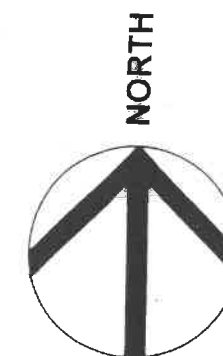
Scale	Date	Drawing Number		Revision
1:100	Dec '16	M2258	11	A

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Scale:





Client

Kollel Heichal Hatorah, Manchester

Project

**Proposed Change of Use & Extension
45 Bury Old Road, Prestwich**

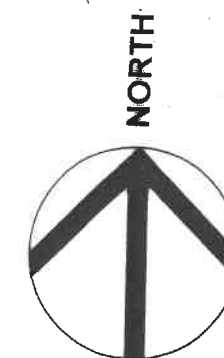
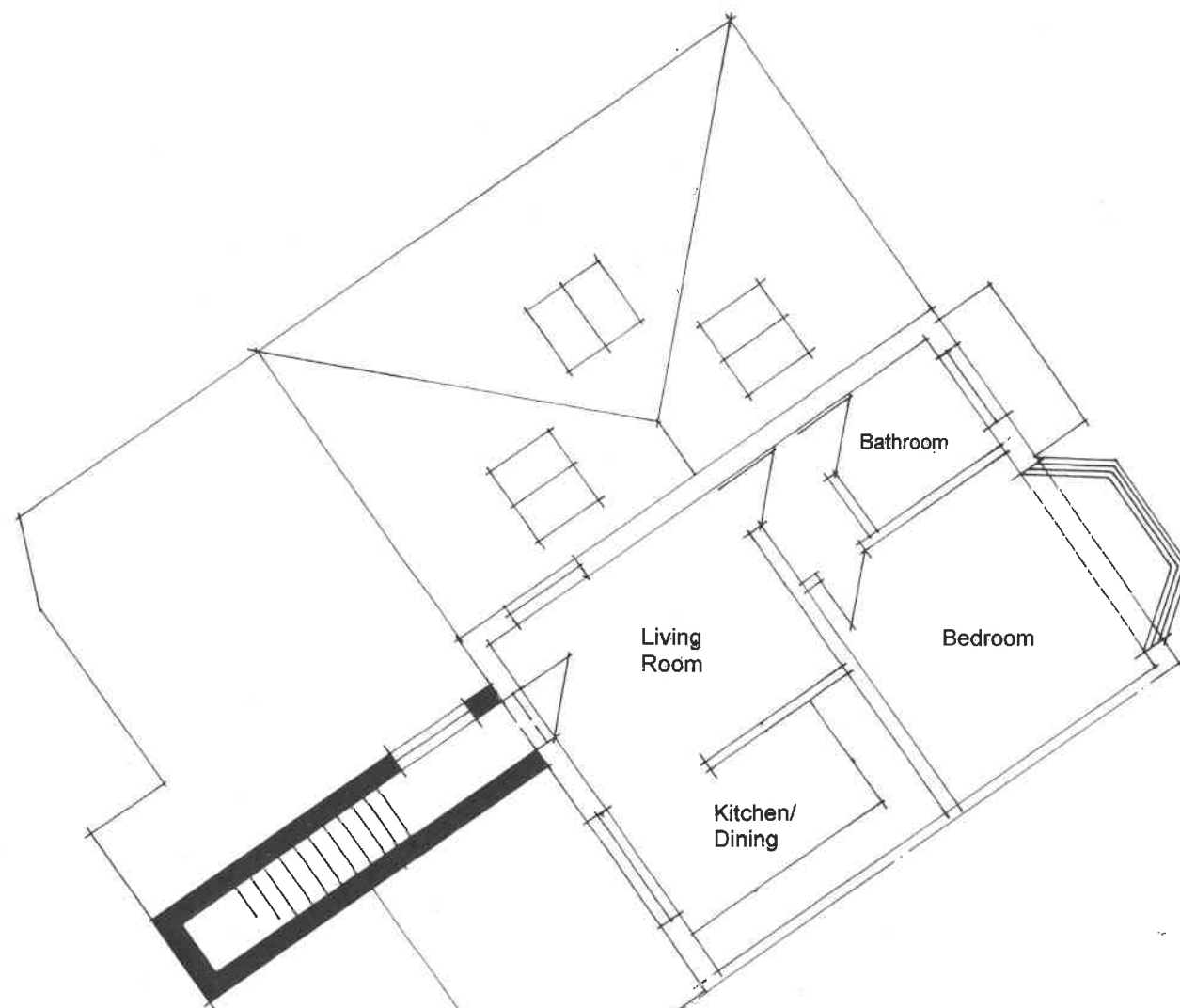
Drawing Title

Existing First Floor Plan

Scale	Date	Drawing Number	Revision
1:100	June '16	M2258 03	

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A3 Format @ 1:100 Scale

Client

Kollel Heichal Hatorah, Manchester

Project

**Proposed Change of Use & Extension
45 Bury Old Road, Prestwich**

Drawing Title

Proposed First Floor Plan

Scale	Date	Drawing Number		Revision
1:100	Dec '16	M2258	12	A

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Scale:



Ward: Whitefield + Unsworth - Unsworth

Item 10

Applicant: Unsworth Smile Clinic

Location: 57 Parr Lane, Bury, BL9 8JR

Proposal: Second floor extension to form three storey building

Application Ref: 61048/Full

Target Date: 20/03/2017

Recommendation: Approve with Conditions

Description

The application relates to an existing dental clinic situated within a designated Local Shopping Centre on Parr Lane at the top of Sunny Bank Road. The area surrounding the busy shopping centre is residential in character. The clinic is located above an existing medical centre which is also attached on the north side and there is a pharmacy attached to the south. The building is two storey with single storey properties on either side.

The proposal is to add a third storey to the building to create an additional space. There would be two additional surgery rooms, a staff room, a decontamination room, store room and toilets. The applicant states that the additional space is required as the existing space is not suitable and the additional surgeries would allow some flexibility for part time staff.

The flat roof extension would project above the existing building by 2.7m, taking the overall height to 10.6m. The extension would be set in from the existing walls and constructed with a lightweight steel structure with a mix of metal cladding and glazing to the walls.

A revised Design and Access Statement has been submitted and sets out the need for the extension:

- The practice has been operating under NHS contract since 2006. Since this time the work/patient numbers have not altered significantly. The clinic has approximately 6000 'active' patients on its register. Taking on extra patients would not mean additional income as it does with a private clinic.
- The proposal is essential to the practice to improve facilities for staff and patients. There is currently no staff room or stock room.
- Staff and patient numbers would not increase. The maximum number staff at any one time is 13 and most of the dentists are part time and work flexible hours.
- The majority of patients are local and many use the local bus service.
- There is a car park behind the Bay Horse Public House within the centre that patients can use for a charge of £1. The clinic has highlighted this to patients.

Since submitting the application, the number of staff employed has been clarified by the applicant and is as follows

- 2 full time dentist
- 4 part time dentists
- 5 full time nurses (includes those whom work as receptionists)
- 1 part time nurse
- 1 full time manager
- 1 part time manager

Relevant Planning History

60845 - Second Floor Extension - Withdrawn 19/01/17.

Publicity

The following neighbours were notified by letter dated 24/01/17 - Unsworth Medical Centre, nos.49, 51, 59-65(odd), 73, 90-98(even) Parr Lane, 71, 73 Pole Lane, 25 Bloomfield Drive, 36 Chadderton Drive, 264 Sunny Bank Road.

Objections received from residents at 25 Bloomfield Drive, 36 Chadderton Drive and 60 and 63 Parr Lane. Objections are summarised below:

- Parking is a serious problem in and around the centre and the proposal would make it worse.
- The proposed extension would mean an increase in staff and patients
- If the practice is not expanding why is there a need for the extension?
- The owner of the business park in the disabled parking bay outside the doctors or across the road on double yellow lines!! If the owner of the business cannot park his own car how does he expect others to park in the already congested area.
- Saying most people walk and use a bus to get the dentist is just speculation.
- This building is not available to wheelchair users due to no lift being installed.
- The extension is imposing, out of keeping in the area and looks like a pigeon coop!
- Loss of light/overshadowing- the proposed height will severely impact on residential housing opposite.

The medical centre has stated that the construction of new extension should not have an impact on their building.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - Concerns about parking.

Unitary Development Plan and Policies

EN1/2	Townscape and Built Design
EC4/1	Small Businesses
HT2/4	Car Parking and New Development
CF4	Healthcare Facilities
SPD16	Design and Layout of New Development in Bury

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy - UDP Policy CF4 relating to Healthcare Facilities states that improvements to existing, and proposals for new healthcare facilities will generally be looked on favourably by the Council.

Policy EN1/2 Townscape and Built Design states that the Council will give favourable consideration to proposals which do not have an unacceptable adverse effect on the particular character and townscape of the Borough's towns, villages and other settlements. Factors to be considered when assessing proposals will include:

- a) the external appearance and design of the proposal in relation to its height, scale, density and layout;
- b) the relationship of the proposal to the surrounding area;
- c) the choice and use of materials;
- d) access and other design features for the mobility impaired;

- e) the design and appearance of access, parking and service provision;
- f) landscaping, including the use of natural landscape features, and open space provision;
- g) the use of lighting.

Use - The principle of an extension is acceptable. The existing dental clinic is a valuable community facility and within a Local Shopping Centre. Expanding the existing facilities would be welcome but subject to design criteria and satisfactory amenity and parking issues.

Visual amenity - The existing dental practice sits between the single storey medical centre to the north and a row of flat roof single storey shops to the south. The existing red brick building is very functional and does not have any particular architectural merit. As such, adding an additional storey to the existing two storey building would not have a significantly detrimental impact on the appearance of the building itself. It would have an impact on the immediate streetscape, sandwiched as the building is between lower level buildings on either side, however a three storey building would not be particularly out of keeping in the wider area. The proposal is considered to be appropriate in terms of design, size and siting and would be contrary to UDP Policy EN1/2 Townscape and Built Design and guidance document SPD16 Design and Layout of New Development in Bury.

Residential amenity - The distances to the nearest residential properties to the north at 49 and 51 Parr Lane are approximately 20m. Properties across Parr Lane to the west are approximately 30m away. Separation distances across Parr Lane are acceptable and would not lead to undue overlooking. The distances to properties on the north side are considered to be acceptable subject to obscure glazing on the windows on the north elevation.

Access and Traffic - Undoubtedly, parking is a problem in and around the busy local shopping centre and concerns with regard to parking raised by residents are understandable.

The crucial factor in any assessment is whether the extension would mean increasing capacity, staff and patients. Whilst the capacity of the building is obviously increasing with the addition of the two surgeries, staff room and other rooms, the applicant has indicated that the increased space would allow the clinic to improve existing facilities and allow flexibility of working, without necessarily increasing patient or staff numbers.

Whilst this may be the case and no new patients joining the clinic, increased flexibility may mean that at busier times more surgeries would be used at the same time and patient numbers, at that particular time, may increase. In this case there may be a knock-on impact on the parking around the site. How significant this would be is difficult to determine. For example, it may be the case that patients would be attending the clinic in addition to visiting other shops and services in the centre. In this case the impact on parking would be more limited.

It should also be noted that whilst the applicant states that patients and staffing levels would remain at existing levels, it cannot be guaranteed that this would be the case in the future either with this clinic or if the building were to be occupied by another business.

The benefits of improvements to the service offered by the existing clinic need to be assessed against any possible disbenefits, in this case the potential for increased pressure on parking in and around the centre. In this case it is considered that the proposal is, on balance, just about acceptable.

Disabled Access - It is unfortunate that the existing building does not have lift. Given there is no lift from the ground floor, a lift from the second floor to third would have limited benefit.

Objections - The planning related issues raised by the objectors have been addressed in the above report. The issue of where the applicant, or any other person, parks his or her car is a matter for the parking authorities.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

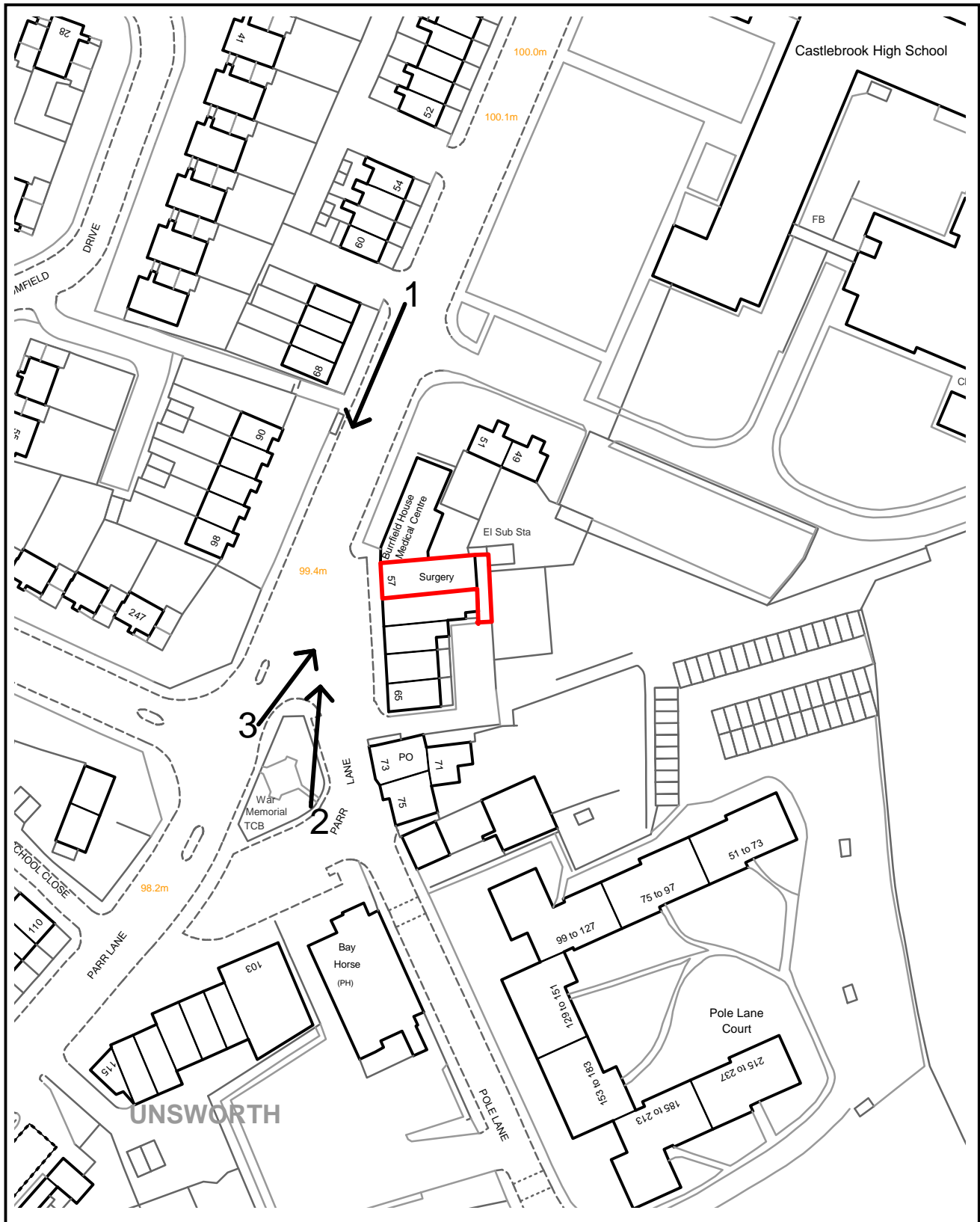
Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 9297/001/A, 011/B and 021/B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. Before the first occupation of the extension hereby permitted the windows on the north elevation of the second floor extension shall be fitted with obscured glazing (min level 3) and shall be permanently retained in that condition thereafter.
Reason. To protect the privacy of nearby occupiers in the interests of residential amenity.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61048

**ADDRESS: 57 Parr Lane
Bury**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61048

Photo 1



Photo 2



61048

Photo 3



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2002-2003



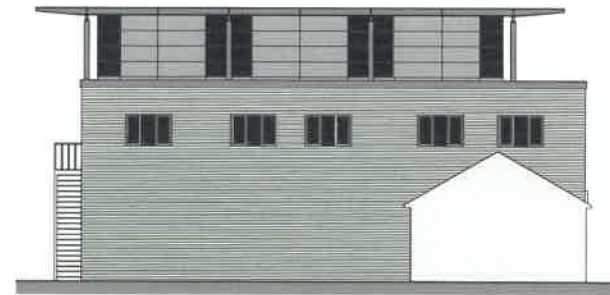
Foot

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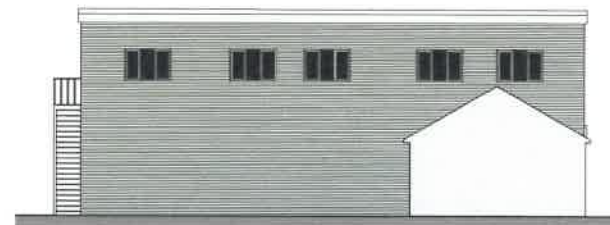
Proposed Rear Elevation 1:100



Proposed North Elevation 1:100



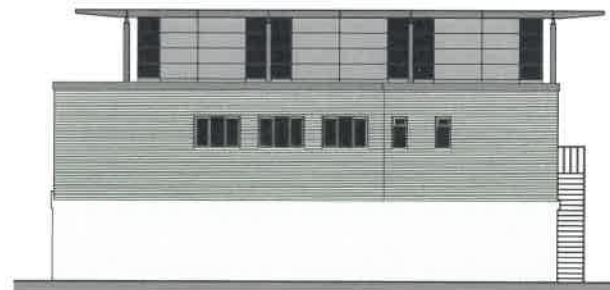
Existing Rear Elevation 1:100



Existing North Elevation 1:100



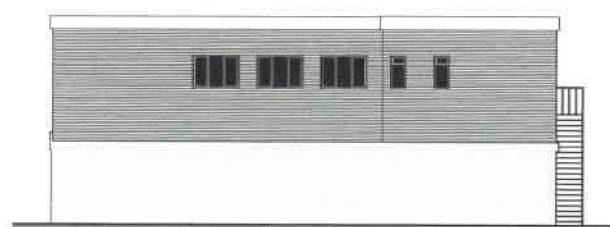
Proposed Parr Lane Elevation 1:100



Proposed South Elevation 1:100



Existing Parr Lane Elevation 1:100



Existing South Elevation 1:100

All elevations are to be structural in style, and discrepancies are to be reported to the Architect before construction commences. Do not scale from the elevations.
This drawing is to be read in conjunction with all relevant documents and specifications. Any discrepancies are to be reported to the Architect before the above work commences.
All structural components shown are indicative only. Details / calculations of structural members are to be provided by the Structural Engineer.
This drawing is copyright.

A	Final Issue	18/11/2016
B	Control amendments following comments received from LPA on 22/01/2017	22/01/2017

T+A Tang & Associates Ltd
Chartered Architects

Tel : 0145 477 8001
Email: enquiries@tangandassociates.co.uk

Cheney Hill, 1st, Stapeley Road
Stapeley, Stoke-on-Trent, Staffordshire, ST6 4JX, UK

Client:
Dr. Isa Usman

Project:
57 Parr Lane, Unsworth, BL9 6JR

Drawing Title:
Existing & Proposed Elevations

Scale:	Date:
1:100 @ A1	November 2016
Drawing No:	Revisions
9297/021	B

BURY COUNCIL
DEPARTMENT FOR RESOURCES AND REGULATION
PLANNING SERVICES

PLANNING CONTROL COMMITTEE

28 February 2017

SUPPLEMENTARY INFORMATION

Item:01 Eton Hill Business Park, Eton Hill Road, Radcliffe, Manchester, M26 2US Application No. 60673

Erection of 18 no. small industrial units for Class B1 (Business), Class B2 (general industry) and Class B8 (storage and distribution) with associated car parking and service yard

Consultations

Drainage Section - No objections, subject to the inclusion of conditions relating to drainage.

Conditions

Therefore, condition 16 should be added in relation to drainage:

16. Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority.

The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards. This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The final scheme should comply with the principles outlined in the approved Flood Risk Assessment.

Reason. To ensure a satisfactory means of drainage, promote sustainable development and to manage the risk of flooding and pollution pursuant to Policy EN5/1 - New Development and Flood Risk and Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan and Section 10 of the National Planning Policy Framework.

Item:02 Victoria Mill, High Street, Walshaw, Bury, BL8 3FS Application No. 60762

Proposed extension and conversion of existing mill to provide 13 no. 1 & 2 bedroom residential apartments with 16 no. car parking spaces and associated works

Nothing further to report

Item:03 Site adjacent to 24 Bolton Road, Bury, BL8 2QR Application No. 60767 Replacement of externally illuminated 48-sheet advertising display with 48-sheet digital display

Nothing further to report.

Item:04 Land at side of 46 Merton Road, Prestwich, Manchester, M25 1PL Application No. 60812

Demolition of existing garage and erection of new dwelling

Publicity

Additional objection letter received from No 25 Malvern Close with the following issues raised:

- The report description states the proposed dwelling would 'be designed in a

similar context to the adjacent houses and those on the row' - however the existing dwellings on Merton Road have a depth of 5.8m and the proposed dwelling has a depth of 8.2m at single and second storey - proposed dwelling would be out of scale;

- The house may follow the front building line but it is not in keeping at the rear;
- There would be no tree screening at the rear boundary as the trees have already been removed - grateful a landscaping scheme has been included as a condition - will this be along the boundary?
- Has the response from UU regarding proximity of their equipment been taken into consideration?
- Can a planning condition be included to prevent window type and size being changed post completion?
- Can the distance of the other window be increased from 19.3m to 20m as required?
- What is a difference in level defined as - No 25 Malvern is approx 1m above ground level - would this equate to requiring a further 3m of separation between properties?

In response to my objection letter of 13/2/17:

- The dwelling does not have a 2.4m single storey rear projection - this is at 1st floor and would not be permitted development;
- The proposal would not comply with aspect standards at 19.3m;
- Can it be confirmed the proximity of UU equipment would not have implications for a landscaping scheme;
- Do not believe the revised plans sufficiently reflect the objections made and the 2.4m projection at the rear should be removed from the scheme;
- The proposal would not comply with UDP Policy H2/6 - No 25 Malvern Close would lose privacy and be directly overlooked and would damage the enjoyment of occupants of No 25.

Response to objector

- Whilst the proposed dwelling would have a larger footprint at ground floor and extend further back than other properties on the row, permitted development rights would enable other properties to similarly extend at ground floor at the rear without the need for planning permission.
- The 1st floor rear projection of the proposed dwelling would not be of such a scale and massing to be considered out of character with the proposed dwelling, nor would it have a detrimental impact on No 25 Malvern Close in terms of proximity and overlooking given the separation distances and the oblique relationships of the two properties.
- The applicant has been informed of the response from UU which identifies that a public sewer crosses the site and permission would not be granted by UU to build over or within 3m of the centreline. This would be a matter for the applicant to address directly with UU with regards to location of servicing equipment on the site.
- The trees which have been removed were not protected and could be removed by the landowner at any time without the need for a planning consent. The landscaping scheme required by the condition would need to meet the approval of the Local Planning Authority prior to the commencement of development.
- The levels difference between the two properties would not necessitate or require an increase in separation distances between the proposed dwelling and No 25 Malvern Close. As stated in the main body of the report, the relationship between the two properties is considered to be acceptable due to separation distances and the positioning of the new build.
- The objections stated above have been covered in the main report.

Item:05 Bast House Farm Barns, Manchester Road, Ramsbottom, Bury, BL9 5LZ
Application No. 60870

Conversion of two redundant agricultural outbuildings to create 6 no. dwellings

Consulation.

Traffic - No objection subject to conditions.

Impact on Listed Building - The proposal, albeit in the form of the conversion, would help preserve the existing barn complex within its traditional setting. In doing so it would also help to preserve and enhance the historic setting of the listed building located immediately to the west. Allowing the building complex to fall into further dereliction with possible demolition, would leave the listed farmhouse in a more incongruous situation. As such the proposal would preserve and enhance the character and setting of the listed building and comply with UDP Policies EN2/3 Listed Buildings.

Additional conditions

15. Unless the section of Public Footpath No. 229 Ramsbottom, located within the application site, is first diverted, details of an alternative location for the parking spaces which obstruct its line shall be submitted to and approved in writing by the Local Planning Authority prior to commencement of the development. The parking spaces should be provided prior to the first occupation of the dwellings and subsequently retained.

Reason. To ensure that the Public Right of Way that crosses the site is adequately accommodated pursuant to UDP Policy RT3/3 Access to the Countryside.

17. The turning facilities indicated on the approved plans shall be provided before the development is first occupied and the areas used for the manoeuvring of vehicles shall be subsequently maintained free of obstruction at all times.

Reason. In the interests of highway safety pursuant to the NPPF and UDP Policy H2/2 The Layout of New Residential Development.

18. In the event that the section of Public Footpath No. 229 Ramsbottom, located within the application site, is to be diverted, no development shall commence unless and until full details of following have been submitted to and approved by the Local Planning Authority:

- the provision of a gate at the interface between the existing access track with the open field/diverted route of the Public Right of Way;
- details of materials and construction specifications for the diverted footpath and any fencing;

The details subsequently approved shall be implemented to the written satisfaction of the Local Planning Authority before the development is first occupied/ Public Footpath No. 229 Ramsbottom is diverted.

Reason. To ensure that the Public Right of Way that crosses the site is adequately accommodated pursuant to UDP Policy RT3/3 Access to the Countryside.

19. No development shall commence unless and until full details of the following have been submitted to and approved in writing by the Local Planning Authority:

- proposals to install residential sprinkler systems to BS 9251:2005 or equivalent standard, or other measures as required by the Greater Manchester Fire and Rescue Service to overcome the site's emergency access deficiencies;

The details subsequently approved shall be implemented before the dwelling is first

occupied.

Reason. No details have been submitted and in the interests of fire safety pursuant to The NPPF and UDP Policy OL1/4 Conversion and re-use of Buildings in the Green Belt and H2/2 The Layout of New Residential Development.

Item:06 Land at Bank Street, Whitefield, Manchester, M45 7JF Application No. 60901

Demolition of existing buildings and erection of two storey office (Class B1) building and associated car parking, hard and soft landscaping works and associated works

Nothing further to report

Item:07 Side of 10 Scotland Place, Ramsbottom, Bury, BL0 9BD Application No. 60974

Erection of 1 no. attached dwelling

Nothing further to report.

Item:08 Land to the rear of Sandy Lane, Prestwich, Manchester, M25 9PS Application No. 60997

Demolition of existing garages and erection of 2 no. semi detached houses

Conditions.

UDP Policy reasons added to conditions 5 to 9 - HT2 Highway Network and H2/2 The Layout of New Residential Development.

Item:09 45 Bury Old Road, Prestwich, Manchester, M25 0EY Application No. 61022

Extension and change of use to existing dwelling to form a mixed use comprising first floor self contained flat (Class C3) with post graduate study centre on ground floor (Class D1); Formation of new vehicular access from Kings Road.

Further Information.

Further information has been submitted:

A list of the Kollel member's addresses has been supplied 23 of the 31 members have an M25 (Prestwich area) postcode. The remaining 8 have an M7 (Broughton, Kersal areas) postcode.

Supplementary Parking Document 11 - Parking Standards in Bury indicate a maximum of 1 parking space per 2 full time staff for a D1 Higher Education Use.

Consultations

Environmental Health (Noise Pollution). In the absence of detailed information as to what constitutes 'Rabbinic study', assurances should be sought as to the nature of the proposed studies, and any consent which may be given, be conditioned such that more noisy group activities (eg.singing/chanting/stamping) are not permitted.

Traffic comments - No objection subject to the following conditions:

8. The development hereby approved shall not be brought into use unless and until the vehicular access alterations indicated on approved plan reference M2258 11

Revision C, incorporating the formation of the proposed footway crossing off Kings Road and reinstatement of the redundant footway crossing to adjacent footway levels at the junction with Bury Old Road, have been implemented to an agreed specification and to the written satisfaction of the Local Planning Authority.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant to UDP Policy HT2 Highway Network.

9. The turning facilities within the proposed car parking area indicated on approved plan reference M2258 11 Revision C shall be provided before the development is brought into use and shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to UDP Policy HT2/4 Car Parking and New Development.

10. The car and cycle parking indicated on approved plan reference M2258 11 Revision C shall be surfaced, demarcated and made available for use to the written satisfaction of the Local Planning Authority prior to the development hereby approved being brought into use and thereafter maintained at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to UDP Policy HT2/4 Car Parking and New Development.

11. The proposed boundary railings shall be powder coated black.

Reason. In the interests of visual amenity pursuant to UDP Policy EN1/2 Townscape and Built Design.

Item:10 57 Parr Lane, Bury, BL9 8JR Application No. 61048
Second floor extension to form three storey building

Consultation

Traffic Team - No objection subject to the following condition.

5. No development shall commence unless and until full details of the following have been submitted to and agreed in writing by the Local Planning Authority:

- a method statement to cover all works/activities abutting the unadopted footway on Parr Lane, including the provision, where necessary, of temporary pedestrian facilities/protection measures;
- parking on site or on land within the applicant's control of operatives' vehicles together with storage on site of construction materials.

The details subsequently approved shall be implemented to an agreed programme with the measures retained and facilities used for the intended purpose for the duration of the construction period.

Reasons. To secure the satisfactory development of the site in terms of pedestrian safety and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to H2 Highway Network.

REPORT FOR DECISION

Agenda Item 5



Agenda
Item

5

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	28 February 2017
SUBJECT:	DELEGATED DECISIONS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	The report lists: Recent delegated planning decisions since the last PCC
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A
Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS**DIRECTOR:**

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Planning Control Committee of the delegated planning decisions made by the officers of the Council.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-None**Contact Details:-**

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Planning applications decided using Delegated Powers

Between 16/01/2017 and 19/02/2017



Ward: **Bury East**

Application No.: 60755 **App. Type:** FUL 18/01/2017 Approve with Conditions
Location: 6 Market Street, Bury, BL9 0AJ

Proposal: Change of use from hairdressers (Class A1) to cafe bar (Class A3) with flue at rear

Application No.: 60864 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: 80 Spring Street, Bury, BL9 0RW

Proposal: First floor extension at rear

Application No.: 60888 **App. Type:** ADV 19/01/2017 Approve with Conditions
Location: 1 Tithebarn Street, Bury, BL9 0JR

Proposal: 2 No. externally illuminated fascia signs

Application No.: 60964 **App. Type:** ADV 07/02/2017 Approve with Conditions
Location: 2-4 Market Street, Bury, BL9 0AN

Proposal: Replacement signs to include 2 no. internally illuminated fascia signs, 2 no. internally illuminated projecting signs and 7 no. non-illuminated signs

Application No.: 60965 **App. Type:** LBC 07/02/2017 Approve with Conditions
Location: 2-4 Market Street, Bury, BL9 0AN

Proposal: Listed Building Consent for replacement signs to include 2 no. internally illuminated fascia signs, 2 no. internally illuminated projecting signs and 7 no. non-illuminated signs

Application No.: 60983 **App. Type:** LBC 08/02/2017 Approve with Conditions
Location: 2-4 Market Street, Bury, BL9 0AN

Proposal: Listed Building Consent for internal alterations comprising of relocation of existing ATM's and installation of new ATM's with signs, redecoration and alterations to machine walls, glazed partition and suspended ceiling

Application No.: 61003 **App. Type:** CON 15/02/2017 Raise No Objection
Location: Land bounded by Smith Street to South, John Street to East, rear of Yorkshire to north, Penn Street and Baillie Street to north west and Constantine Road to West, Rochdale
Proposal: Article 18 consultation from Rochdale council (ref: 16/01383/HYBR) for hybrid application comprising comprehensive phased regeneration of Rochdale A) Detailed application for phase one for erection of 4 commercial buildings, decked car park to provide retail, food & drink uses (Use classes A1, A3, & A4), maximum 22,511 sq.m), cinema (use Class D2, maximum 1,797 sq.m), gym (use class D2 maximum 2,018sq.m) together with new service road and junction at John Street, ancillary uses, servicing and common areas, landscaping and infrastructure; and for B) Outline application (all matters reserved) for phase two for erection of four buildings to provide retail, food and drink (use classes A1, A3, A4 & A5 1,000 sq. m max); offices / commercial (use Classes A2 & B1, 4,000 sq. m max), residential uses (max 100 dwellings), hotel use (Class C1, max 120 bed) and leisure (Class D1, max 500 sq. m) together with associated car parking, ancillary uses, landscaping and infrastructure

Application No.: 61034 **App. Type:** CON 15/02/2017 Raise No Objection
Location: Heap Fold Farm, Bury Old Road, Heywood, BL9 7JA
Proposal: Article 18 consultation from Rochdale Council (ref: 16/01541/HYBR): Hybrid application for the redevelopment of land at Heap Fold Farm. A) Detailed application for the conversion of the existing school to 12 apartments including the creation of a new access from the proposed residential development, car parking and landscaping; B) Detailed application for the construction of a new primary school together with the creation of a new access, landscaping, play area, sports pitch and multi use games area; C) Outline application (including details of access) for residential development on the remainder of the land including associated landscaping, open space, green corridor with cycleway and footpath links, ecological enhancements and the provision of community allotments

Ward: **Bury East - Moorside**

Application No.: 60853 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: McDonalds, Woodfields Retail Park, Peel Way, Bury, BL9 5BY
Proposal: Alterations to elevations to include the installation of a new "Folded Roof" concept, comprising of aluminium cladding to the roof, the construction of extensions totalling 15.8sqm and the installation of a third drive thru booth. The reconfiguration of the drive thru lane to provide side by side ordering and associated works to the site. Relocation of existing and installation of new fascia signage.

Application No.: 60854 **App. Type:** ADV 19/01/2017 Approve with Conditions
Location: McDonalds, Woodfields Retail Park, Peel Way, Bury, BL9 5BY
Proposal: Relocation of 4 no. existing fascia signs and the installation of 3 no. new fascia signs.

Application No.: 60855 **App. Type:** ADV 19/01/2017 Approve with Conditions
Location: McDonalds, Woodfields Retail Park, Peel Way, Bury, BL9 5BY
Proposal: Installation of 10 no. signs to accommodate the new drive thru layout suite to comprise: 8 No freestanding signs, 1 no. relocated banner sign and 1 no. side-by-side directional.

Application No.: 60897 **App. Type:** FUL 15/02/2017 Approve with Conditions
Location: Birchin Bower Farm, 1 Castle Hill Road, Bury, BL9 6UN
Proposal: Conversion from 1 no. dwelling into 2 no. dwellings

Application No.: 60926 **App. Type:** FUL 31/01/2017 Approve with Conditions
Location: 65 Sefton Drive, Bury, BL9 6PL
Proposal: First floor extension at side

Application No.: 61001 **App. Type:** GPDE 08/02/2017 Prior Approval Not Required - Extension
Location: 62 Mather Road, Bury, BL9 6RA
Proposal: Prior notification for proposed single storey rear extension

Application No.: 61006 **App. Type:** FUL 14/02/2017 Approve with Conditions
Location: 1 Mafeking Avenue, Bury, BL9 6NF
Proposal: First floor extension including conversion of car port and re-roofing to whole property with single storey extension at side; Render to existing and proposed elevations

Application No.: 61012 **App. Type:** ADV 13/02/2017 Approve with Conditions
Location: Unit 5, Woodfields Retail Park, Peel Way, Bury, BL9 5BY
Proposal: 2 no. non-illuminated fascia signs and 1 no. internally illuminated fascia sign

Ward: Bury East - Redvales

Application No.: 60742 **App. Type:** FUL 16/01/2017 Approve with Conditions
Location: 45 Brierley Street, Bury, BL9 9BP
Proposal: Single storey granny annexe at rear

Application No.: 60797 **App. Type:** OUT 19/01/2017 Refused
Location: Land adjacent to 1 Ribchester Drive, Bury, BL9 9JT
Proposal: Outline application for 4 no. semi detached dwellings with all matters reserved

Application No.: 60916 **App. Type:** FUL 30/01/2017 Approve with Conditions
Location: 154 Parkhills Road, Bury, BL9 9AW
Proposal: Single storey front extension and first floor side extension

Application No.: 60991 **App. Type:** FUL 14/02/2017 Approve with Conditions
Location: 5 Parkhills Close, Bury, BL9 9HB
Proposal: Single storey extension at rear and ramped access to front

Application No.: 61029 **App. Type:** FUL 16/02/2017 Approve with Conditions
Location: 19 Somerset Drive, Bury, BL9 9DG
Proposal: Two storey side extension

Ward: Bury West - Church

Application No.: 60438 **App. Type:** FUL 09/02/2017 Approve with Conditions
Location: Greater Manchester Fire OTC, Wellington Street, Bury, BL8 2AX
Proposal: The reinstatement of existing access point onto Wellington Street and the provision of a new hard standing to be used as fire service vehicle parking

Application No.: 60647 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: Scholes House, Leigh Lane, Bury, BL8 1RJ
Proposal: Change of use of kennels to domestic use including swimming pool

Application No.: 60852 **App. Type:** FUL 16/01/2017 Approve with Conditions
Location: 215 Ainsworth Road, Bury, BL8 2RU
Proposal: Creation of new vehicular access

Application No.:	60945	App. Type:	FUL	02/02/2017	Approve with Conditions
Location:	474 Bolton Road, Bury, BL8 2DU				
Proposal:	Two storey extension to both sides; Single storey extension at front; Dormers to front; Alterations to existing roof with increase of ridge height at side and pitched roofs to flat roof bays at front/side and replacement garden wall and gates				

Application No.:	61146	App. Type:	GPDE	15/02/2017	Prior Approval Required Refused - Ext
Location:	28 Kenmor Avenue, Bury, BL8 2DY				
Proposal:	Prior notification for proposed single storey rear extension				

Ward: Bury West - Elton

Application No.:	60831	App. Type:	FUL	06/02/2017	Approve with Conditions
Location:	Higher Woodhill Farm, Woodhill Road, Bury, BL8 1BS				
Proposal:	Extension to two existing agricultural buildings				

Application No.:	60913	App. Type:	FUL	19/01/2017	Approve with Conditions
Location:	309 Brandlesholme Road, Bury, BL8 1EY				
Proposal:	Two storey and single storey extensions at side and rear				

Application No.:	60955	App. Type:	FUL	30/01/2017	Approve with Conditions
Location:	6 Springside View, Bury, BL8 4LU				
Proposal:	Porch at front				

Application No.:	60960	App. Type:	FUL	06/02/2017	Approve with Conditions
Location:	Higher Woodhill Farm, Woodhill Road, Bury, BL8 1BS				
Proposal:	Demolition of existing poultry shed and erection of agricultural livestock building				

Application No.:	60968	App. Type:	FUL	01/02/2017	Approve with Conditions
Location:	1 Dereham Close, Bury, BL8 1JP				
Proposal:	Demolition of existing single storey side attached garage; Erection of two storey pitched roof side extension				

Application No.:	60977	App. Type:	FUL	02/02/2017	Approve with Conditions
Location:	31 Garside Hey Road, Bury, BL8 1HZ				
Proposal:	Single storey extension at rear				

Ward: North Manor

Application No.:	60858	App. Type:	FUL	19/01/2017	Approve with Conditions
Location:	8 Kendal Road, Ramsbottom, Bury, BL0 9SP				
Proposal:	Single storey extension at side/rear				

Application No.:	60902	App. Type:	FUL	26/01/2017	Approve with Conditions
Location:	Underhill, Redisher Lane, Ramsbottom, Bury, BL8 4HX				
Proposal:	Variation of condition no. 2 following grant of planning permission 54878 for new entrance/drive to be moved over by 5.5 metres				
Application No.:	60914	App. Type:	FUL	17/01/2017	Approve with Conditions
Location:	44A Newcombe Road, Ramsbottom, Bury, BLO 9UT				
Proposal:	External alterations to front elevation and garage and enlargement of driveway/pavement crossing to create extra parking space				
Application No.:	60915	App. Type:	FUL	08/02/2017	Approve with Conditions
Location:	Land to west of Hawkshaw Lane, Tottington, Bury, BL8 4LB				
Proposal:	Creation of stables and riding manege				
Application No.:	60932	App. Type:	FUL	14/02/2017	Approve with Conditions
Location:	Lower Hill Top Barn, Hollymount Lane, Tottington, Bury, BL8 4HP				
Proposal:	Two storey extension at side				
Application No.:	60938	App. Type:	FUL	09/02/2017	Approve with Conditions
Location:	67 Longsight Road, Ramsbottom, Bury, BLO 9TA				
Proposal:	First floor extension at rear and conversion of existing flat roof to pitched; Loft conversion with front and rear dormers				
Application No.:	60954	App. Type:	FUL	10/02/2017	Approve with Conditions
Location:	201 Holcombe Road, Tottington, Bury, BL8 4BQ				
Proposal:	Two storey extension at side				
Application No.:	61009	App. Type:	LDGP	17/02/2017	Lawful Development
Location:	19 Larkfield Close, Tottington, Bury, BL8 4QJ				
Proposal:	Certificate of lawfulness for proposed boundary garden wall with a height of 925mm				
Application No.:	61025	App. Type:	FUL	09/02/2017	Approve with Conditions
Location:	8 Mayfield Road, Ramsbottom, Bury, BLO 9TB				
Proposal:	Part single storey and part two storey extension at side/rear				

Ward: Prestwich - Holyrood

Application No.:	60903	App. Type:	LDGP	26/01/2017	Lawful Development
Location:	285 Heywood Road, Prestwich, Manchester, M25 2RA				
Proposal:	Certificate of lawfulness for proposed hip to gable roof extension				
Application No.:	60935	App. Type:	FUL	27/01/2017	Refused
Location:	245 Bury Old Road, Prestwich, Manchester, M25 1JE				
Proposal:	Two storey rear extension and rear dormer extension				

Application No.: 61005 **App. Type:** FUL 13/02/2017 Approve with Conditions
Location: 46 Poppythorn Lane, Prestwich, Manchester, M25 3BY
Proposal: Conversion of existing basement into a habitable space with new plant/store room areas and installation of external light well with railings at side

Ward: **Prestwich - Sedgley**

Application No.: 60182 **App. Type:** FUL 07/02/2017 Approve with Conditions
Location: Land adjacent to Rico House, George Street, Prestwich, Manchester, M25 9WS
Proposal: Erection of synagogue (Class D1) and offices (Class B1) together with associated vehicular access, car parking and landscaping

Application No.: 60749 **App. Type:** FUL 16/01/2017 Approve with Conditions
Location: 4 Portugal Road, Prestwich, Manchester, M25 9WP
Proposal: Loft conversion with dormers at side/rear and front and works to basement.

Application No.: 60790 **App. Type:** FUL 06/02/2017 Refused
Location: 43 Watkins Drive, Prestwich, Manchester, M25 0DR
Proposal: Single storey extension at side

Application No.: 60794 **App. Type:** FUL 27/01/2017 Approve with Conditions
Location: 3 The Terrace, Prestwich, Manchester, M25 1FD
Proposal: Building of retaining walls and steps in rear garden and levelling out areas to provide flat garden areas

Application No.: 60834 **App. Type:** FUL 10/02/2017 Approve with Conditions
Location: 42-44 Fairway, Prestwich, Manchester, M25 0JH
Proposal: Roof extension/raise ridge height of roof and loft conversion with pitched roof dormers at front and flat roof dormers at rear.

Application No.: 60880 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 193 Bury Old Road, Prestwich, Manchester, M25 1JF
Proposal: Change of use from shop (A1) to cafe (A3) with alterations to building including loft conversion; 4 no. roof lights to rear elevation and new entrance door to Park Street elevation.

Application No.: 60910 **App. Type:** FUL 20/01/2017 Approve with Conditions
Location: 9 Colchester Avenue, Prestwich, Manchester, M25 0LL
Proposal: Dormer extensions at front and rear

Application No.: 60920 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 47 Tewkesbury Drive, Prestwich, Manchester, M25 0JN
Proposal: Two storey side and rear extension; First floor rear extension and single storey front and rear extensions

Application No.: 60925 **App. Type:** FUL 31/01/2017 Approve with Conditions
Location: 6 George Street, Prestwich, Manchester, M25 9WS
Proposal: Single storey extension at rear; Loft conversion with front and rear dormers

Application No.: 60933 **App. Type:** FUL 02/02/2017 Approve with Conditions
Location: 90 Kings Road, Prestwich, Manchester, M25 0FY
Proposal: Single storey front extension

Application No.: 60951 **App. Type:** FUL 24/01/2017 Approve with Conditions
Location: 4 Fairway, Prestwich, Manchester, M25 0JH
Proposal: Single storey rear extension

Application No.: 60956 **App. Type:** FUL 10/02/2017 Approve with Conditions
Location: Land at rear of 9 Park Hill, Bury Old Road, Prestwich, Manchester, M25 0HH
Proposal: Relocation of storage container

Application No.: 60975 **App. Type:** FUL 25/01/2017 Approve with Conditions
Location: 30 Hilton Crescent, Prestwich, Manchester, M25 9NQ
Proposal: Part single/part two storey rear extension

Application No.: 60976 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 23 Watkins Drive, Prestwich, Manchester, M25 0DR
Proposal: First floor extension to the side and garage conversion

Application No.: 60982 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 17 Lichfield Drive, Prestwich, Manchester, M25 0HX
Proposal: Single storey rear extension

Application No.: 61008 **App. Type:** FUL 06/02/2017 Approve with Conditions
Location: 23 Sheepfoot Lane, Prestwich, Manchester, M25 0BN
Proposal: New front porch; Extensions to sides and rear; Addition of basement to rear

Ward: Prestwich - St Mary's

Application No.: 60558 **App. Type:** TEL 27/01/2017 Prior Approval Required and Granted
Location: Creative Living, Bury New Road, Prestwich, Manchester, M25 3BL
Proposal: Prior notification of proposed demolition of existing building

Application No.: 60759 **App. Type:** FUL 07/02/2017 Approve with Conditions
Location: Prestwich Hospital, Bury New Road, Prestwich, Manchester, M25 3BL
Proposal: Demolition of temporary building to build proposed two storey pharmacy and office building with landscaped areas.

Application No.: 60873 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 9 Hollyedge Drive, Prestwich, Manchester, M25 9RG
Proposal: Single storey extension at front/side and new front porch

Application No.: 60877 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 1 Vale Drive, Prestwich, Manchester, M25 9RX
Proposal: Extension to existing detached garage and conversion to annexe

Application No.: 60941 **App. Type:** FUL 10/02/2017 Approve with Conditions
Location: Go Local Extra, 49 Rainsough Brow, Prestwich, Manchester, M25 9XW
Proposal: Retention of ATM and alterations to shop front

Application No.: 60942 **App. Type:** ADV 10/02/2017 Approve with Conditions
Location: Go Local Extra, 49 Rainsough Brow, Prestwich, Manchester, M25 9XW
Proposal: 3 no. internally illuminated fascia signs to ATM

Application No.: 60952 **App. Type:** FUL 24/01/2017 Approve with Conditions
Location: 3 Shirehills, Prestwich, Manchester, M25 9QB
Proposal: First floor/two storey extension at side

Application No.: 60966 **App. Type:** FUL 06/02/2017 Approve with Conditions
Location: 459 Bury New Road, Prestwich, Manchester, M25 1AF
Proposal: Erection of covered area on existing first floor balcony

Ward: Radcliffe - East

Application No.: 60689 **App. Type:** FUL 18/01/2017 Approve with Conditions
Location: Land to south of Lower Spen Moor Farm and north of Elton Reservoir, Bury and Bolton Road, Radcliffe, Manchester, M26 4JZ
Proposal: Improvement and extension of bridleway 115 to link with access track to Lower Spen Moor.

Application No.: 60832 **App. Type:** FUL 20/01/2017 Approve with Conditions
Location: 72 Higher Ainsworth Road, Radcliffe, Manchester, M26 4JF
Proposal: First floor extension at side and rear with juliet balcony

Application No.: 60909 **App. Type:** FUL 01/02/2017 Approve with Conditions
Location: 48 Blackburn Street, Radcliffe, Manchester, M26 1NY
Proposal: Change of use from office (Class B1) to restaurant (Class A3)

Application No.: 60947 **App. Type:** FUL 06/02/2017 Approve with Conditions
Location: 1 Ripon Close, Radcliffe, Manchester, M26 2QL
Proposal: Two storey side extension

Ward: Radcliffe - North

Application No.: 60875 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 56 Chelsea Avenue, Radcliffe, Manchester, M26 3NF
Proposal: Change of use from shop (A1) to dwelling (C3a)

Application No.: 60876 **App. Type:** FUL 19/01/2017 Approve with Conditions
Location: 54 Chelsea Avenue, Radcliffe, Manchester, M26 3NF
Proposal: Change of use from office (B1) to dwelling (C3)

Application No.: 60912 **App. Type:** LDCP 25/01/2017 Lawful Development
Location: 197 Bury New Road, Radcliffe, Bolton, BL2 6QQ
Proposal: Certificate of lawfulness for proposed single storey rear extension

Ward: Radcliffe - West

Application No.: 60821 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: Outwood Motorhouse Industrial Estate, Outwood Road, Radcliffe, Manchester, M26 1AQ
Proposal: Change of use of existing storage container to taxi booking/dispatch office

Application No.: 60919 **App. Type:** FUL 31/01/2017 Approve with Conditions
Location: 30 Ringley Road West, Radcliffe, Manchester, M26 1DJ
Proposal: Single storey extension at rear with canopy and terrace

Application No.: 60928 **App. Type:** FUL 07/02/2017 Approve with Conditions
Location: Land at rear of Harper Fold, Lavender Street/36 Churchill Drive, Radcliffe, Manchester, M26 3TJ
Proposal: Erection of stable block comprising of 3 no. stables and 1 no. tack room/store with associated muck heap

Application No.: 60948 **App. Type:** FUL 23/01/2017 Approve with Conditions
Location: 5 Wood Hey Close, Radcliffe, Manchester, M26 3XN
Proposal: Single storey extension at rear, roof canopy at front and conversion of existing flat roof to pitched at side with part conversion of existing garage to habitable room

Ward: Ramsbottom + Tottington - Tottington

Application No.: 60894 **App. Type:** FUL 02/02/2017 Approve with Conditions
Location: 24 Market Street, Tottington, Bury, BL8 4AD
Proposal: Change of use from internet business to residential dwelling (Class C3)

Application No.: 60906 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: 11E Kirklees Street, Tottington, Bury, BL8 3NE
Proposal: Single storey extension at rear and garden store with decking over; Loft conversion with velux windows to front and rear

Application No.: 60949 **App. Type:** FUL 02/02/2017 Approve with Conditions
Location: 2 Five Acre Barn, Watling Street, Tottington, Bury, BL8 3QW
Proposal: Erection of detached double garage

Application No.: 60959 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: Woodberry House, Wesley Street, Tottington, Bury, BL8 3SD
Proposal: Variation of condition no. 2 (approved plans) of planning permission 59606 to amend levels and position of plots 5-8

Application No.: 60996 **App. Type:** FUL 02/02/2017 Approve with Conditions
Location: 9 Rhine Close, Tottington, Bury, BL8 3NP
Proposal: Replacement of flat roof at front with tiled pitched roof

Ward: Ramsbottom and Tottington - Ramsbottom

Application No.: 60809 **App. Type:** FUL 07/02/2017 Approve with Conditions
Location: 63 Whalley Road, Shuttleworth, Ramsbottom, Bury, BL0 0DP
Proposal: Creation of new vehicular access and driveway at front

Application No.: 60866 **App. Type:** PMBPA 19/01/2017 Prior Approval Required and Refused
Location: Lower Dickfield Farm, Lower Dickfield, Helmshore Road, Ramsbottom, Bury, BL8 4PD
Proposal: Prior approval for the proposed change of 2 no. agricultural buildings to 2 no. dwellinghouses (Class C3) under part 3 class Q(a) of general permitted development order

Application No.: 60868 **App. Type:** FUL 24/01/2017 Approve with Conditions
Location: Gate House, Holcombe Old Road, Ramsbottom, Bury, BL8 4NG
Proposal: Single storey extension at side and replacement of existing flat roof over existing extension with pitched roof

Application No.: 60891 **App. Type:** FUL 20/01/2017 Approve with Conditions
Location: 15 Heatherside Road, Ramsbottom, Bury, BL0 9BX
Proposal: Two storey extension at front/side; Front porch; Widening of existing driveway

Application No.: 60899 **App. Type:** FUL 24/01/2017 Approve with Conditions
Location: 66 Woodhey Road, Ramsbottom, Bury, BL0 9RB
Proposal: Erection of garage at side with solar panels to roof

Application No.: 60917 **App. Type:** FUL 23/01/2017 Approve with Conditions
Location: Hillcrest, Helmshore Road, Ramsbottom, Bury, BL8 4PD
Proposal: First floor extension at side

Application No.: 60944 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: 1 Henwick Hall Avenue, Ramsbottom, Bury, BL0 9YH
Proposal: Erection of custom-made slimline steel-supported canopy to protect motorhome on existing hardstanding

Application No.: 60978 **App. Type:** FUL 10/02/2017 Approve with Conditions
Location: 188 Bolton Street, Ramsbottom, Bury, BL0 9JE
Proposal: Erection of single storey rear extension, re-roof of existing car port and replacement sliding vehicular access gates

Application No.: 61013 **App. Type:** FUL 06/02/2017 Approve with Conditions
Location: 2 Strathmore Close, Ramsbottom, Bury, BL0 9YW
Proposal: Single storey side extension

Ward: Whitefield + Unsworth - Besses

Application No.: 60967 **App. Type:** LDGP 10/02/2017 Lawful Development
Location: 26 Swinton Crescent, Bury, BL9 8PA
Proposal: Lawful development certificate for proposed single storey extensions at side and rear

Application No.: 61014 **App. Type:** FUL 16/02/2017 Approve with Conditions
Location: 20 Stanway Road, Whitefield, Manchester, M45 8EG
Proposal: Two storey extension at side/rear

Ward: Whitefield + Unsworth - Pilkington Park

Application No.: 60865 **App. Type:** FUL 27/01/2017 Approve with Conditions
Location: 1 Ringley Chase, Whitefield, Manchester, M45 7UA
Proposal: First floor extension at side and external alterations including balcony at rear

Application No.: 60889 **App. Type:** FUL 20/01/2017 Approve with Conditions
Location: 184 -188 Bury New Road, Whitefield, Manchester, M45 6QF
Proposal: Variation of condition 2 (approved plans) of planning permission 58913 relating to extensions and alterations to 184-188 Bury New Road. Alteration involves changing side window to new entrance door

Application No.: 60898 **App. Type:** FUL 26/01/2017 Approve with Conditions
Location: 13 Marle Croft, Whitefield, Manchester, M45 7NB
Proposal: Two storey extension at front; first floor extension at rear and single storey sun room extension at the rear following removal of existing conservatory.

Application No.: 60946 **App. Type:** FUL 16/01/2017 Approve with Conditions
Location: 12 Old Hall Road, Whitefield, Manchester, M45 7QW
Proposal: Garage conversion and modification of roof on side elevation including 2 no. rooflights

Application No.: 60986 **App. Type:** LDGP 13/02/2017 Split Decision
Location: 11 Park Lane, Whitefield, Manchester, M45 7JJ
Proposal: Certificate of Lawful Development for:

A - a proposed single storey side extension and;

B - a proposed two storey rear extension

Application No.: 60994 **App. Type:** FUL 27/01/2017 Approve with Conditions
Location: 30 Philips Park Road West, Whitefield, Manchester, M45 7GJ
Proposal: Porch at front/side with pitched roof to existing flat roof at side and single storey extension at rear

Ward: **Whitefield + Unsworth - Unsworth**

Application No.: 60532 **App. Type:** FUL 27/01/2017 Approve with Conditions
Location: Thurston Fold Farm, Griffe Lane, Bury, BL9 8QS
Proposal: Change of use and extension of existing barn building to 5 no. self contained dwellings, together with removal of existing double-height garage and reinstatement of windows and erection of new porch to existing farmhouse

Application No.: 60625 **App. Type:** FUL 16/01/2017 Approve with Conditions
Location: 344 Hollins Lane, Bury, BL9 8BS
Proposal: Erection of balcony/raised decking

Application No.: 60922 **App. Type:** FUL 27/01/2017 Approve with Conditions
Location: McDonald's Restaurant, Park 66, Pilsworth Road, Bury, BL9 8RD
Proposal: Alterations to elevations to include the installation of a new "Folded Roof" concept, comprising of aluminium cladding to the roof and the construction of extensions totalling 27.3sqm and the installation of an additional drive thru booth. The reconfiguration and extension to the corral to accommodate a new freezer /chiller. Alterations to the external seating incorporating new furniture and layout and retractable awnings over the seating area and associated works

Application No.: 60923 **App. Type:** ADV 27/01/2017 Approve with Conditions
Location: McDonald's Restaurant, Pilsworth Road, Bury, BL9 8RD
Proposal: Relocation of 4 no. existing fascia signs and the installation of 3 no. new fascia signs and 2 no. non-illuminated logo signs to awnings

Application No.: 60969 **App. Type:** CON 10/02/2017 Raise No Objection
Location: Barn at former Coal Pits Farm, Moss Hall Road, Heywood, OL10 2RE
Proposal: Article 18 consultation from Rochdale Council (ref.16/00465/FUL) for conversion of existing barn to a single dwelling together with associated landscaping and access road

Total Number of Applications Decided: **106**

REPORT FOR DECISION

Agenda Item 6

Bury
COUNCIL

Agenda
Item

6

DECISION OF:	PLANNING CONTROL COMMITTEE
DATE:	28 February 2017
SUBJECT:	PLANNING APPEALS
REPORT FROM:	HEAD OF DEVELOPMENT MANAGEMENT
CONTACT OFFICER:	DAVID MARNO
TYPE OF DECISION:	COUNCIL
FREEDOM OF INFORMATION/STATUS:	This paper is within the public domain
SUMMARY:	<p>Planning Appeals:</p> <ul style="list-style-type: none"> - Lodged - Determined <p>Enforcement Appeals</p> <ul style="list-style-type: none"> - Lodged - Determined
OPTIONS & RECOMMENDED OPTION	The Committee is recommended to the note the report and appendices
IMPLICATIONS:	
Corporate Aims/Policy Framework:	Do the proposals accord with the Policy Framework? Yes
Statement by the S151 Officer: Financial Implications and Risk Considerations:	Executive Director of Resources to advise regarding risk management
Statement by Executive Director of Resources:	N/A
Equality/Diversity implications:	No
Considered by Monitoring Officer:	N/A
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Wards Affected:	All listed
Scrutiny Interest:	N/A

TRACKING/PROCESS

DIRECTOR:

Chief Executive/ Strategic Leadership Team	Executive Member/Chair	Ward Members	Partners
Scrutiny Committee	Committee	Council	

1.0 BACKGROUND

This is a monthly report to the Committee of the Planning Appeals lodged against decisions of the authority and against Enforcement Notices served and those that have been subsequently determined by the Planning Inspectorate.

Attached to the report are the Inspectors Decisions and a verbal report will be presented to the Committee on the implications of the decisions on the Appeals that were upheld.

2.0 CONCLUSION

That the item be noted.

List of Background Papers:-

Contact Details:-

David Marno, Head of Development Management
Planning Services, Department for Resources and Regulation,
3 Knowsley Place ,Bury BL9 0EJ

Tel: 0161 253 5291

Email: d.marno@bury.gov.uk

**Planning Appeals Lodged
between 16/01/2017 and 19/02/2017**



Application No.: 60445/FUL

Decision level: DEL

Recommended Decision: Refuse

Applicant: Mrs N Thurstans Ferreira

Location The Gatehouse, Bridgefield Drive, Bury, BL9 7PE

Appeal lodged: 30/01/2017

Appeal Type: Written Representations

Proposal Erection of boundary fence

Application No.: 60524/ADV

Decision level: DEL

Recommended Decision: Refuse

Applicant: Bury Roofing Services

Location Bury Roofing Services, Peter Street, Bury, BL9 6AB

Appeal lodged: 13/02/2017

Appeal Type: Written Representations

Proposal Retrospective application for 2 no. non-illuminated external signs

Total Number of Appeals Lodged: 2

**Planning Appeals Decided
between 16/01/2017 and 19/02/2017**



Application No.: 59918/FUL

Decision level: DEL

Recommended Decision: Refuse

Applicant: Eventmore Limited

Location: George Hotel, Market Street, Bury, BL9 0BL

Proposal: Retrospective application to subdivide the ground floor into two shops and elevational alterations to create two shop fronts

Appeal Decision: Dismissed

Date: 09/02/2017

Appeal type: Written Representations

Appeal Decision

Site visit made on 18 January 2017

by Jason Whitfield BA (Hons) DipTP MRTPI

an Inspector appointed by the Secretary of State for Communities and Local Government

Decision date: 9 February 2017

Appeal Ref: APP/T4210/W/16/3160333

George Hotel, Market Street, Bury BL9 0BL

- The appeal is made under section 78 of the Town and Country Planning Act 1990 (the Act) against a refusal to grant planning permission.
 - The appeal is made by Eventmore Limited against the decision of Bury Metropolitan Borough Council.
 - The application Ref 59918, dated 22 March 2016, was refused by notice dated 13 June 2016.
 - The development proposed is to subdivide the ground floor into two shops and elevational alterations to create two shop fronts.
-

Decision

1. The appeal is dismissed.

Procedural Matter

2. The development has been carried out. The description of the development on the application form omits reference to the elevational alterations to create two shop fronts as shown on the proposed plans. The Council amended the description of development to include reference to the elevational alterations and this description was used on the decision notice and in Section E of the appeal form. I have therefore made my determination on this basis, having removed the phrase "retrospective application" as this does not constitute development. This description is reflected in the heading above.

Main Issue

3. The main issue is whether the proposal would preserve or enhance the character or appearance of the Bury Town Centre Conservation Area.

Reasons

4. The appeal site is a three storey building located within the Bury Town Centre Conservation Area (CA). The CA is centred on the historic core of central Bury which developed as a medieval market town. The street pattern reflects the early stages of the town's history. Buildings around Market Street post-date 1850 when the construction of buildings, such as the Grade II Listed Derby Hall, brought about a wave of Victorian redevelopment which resulted in one of the town's grandest civic streets.
5. With a dense grain, the irregular street pattern of the area is predominately characterised by a mix of shops, restaurants, bars and other commercial uses. The architectural interest of the CA is principally in the fine grouping of

Victorian and Edwardian buildings. The significance of the CA is generally derived from the use of locally sourced sandstone, the high quality of masonry which has resulted in buildings of high architectural merit and the fact that many of the most important buildings were constructed in a short period between 1850 and the early 20th century.

6. The appeal property fronts onto Market Street and the public area around Kay Gardens. Located at the end of a terrace, the building is a particularly prominent feature in this part of the CA. The stone parapet rises above the roof of the building and the masonry is intricately detailed in places. Aside from the works subject to this appeal, the building is largely unaltered since it was re-fronted in stone in the late 19th century when the footprints of two houses, which were unchanged since around 1845, were covered.
7. The building has pairs of projecting bay windows at first and second floor levels. The windows are set back with pilasters either side. Photographs indicate that prior to the works subject of this appeal the ground floor contained similarly paired bay windows above a continuous plinth and sited either side of a decorative central entrance door with a deep stone hood and fanlight. Indeed, it is evident that the building had Art Nouveau-style glazing bars to the windows of the ground floor. Consequently, the building has a pleasing symmetry, consistency and a distinct vertical emphasis. Notwithstanding the alterations subject to this appeal, the building and its architectural merit makes an important, positive contribution to the significance of the CA. Indeed, the Bury Town Centre Conservation Area Character Appraisal 2007 (CACA) recognises the appeal property as a significant unlisted building that makes a positive contribution to the character of the CA.
8. Permission is sought for the sub-division of the ground floor of the building into two retail units. The Council has raised no objections to this in principle and on the evidence before me I have no reason to disagree. Nevertheless, the development also includes external works to facilitate the subdivision. The works comprise the installation of fully glazed shop fronts on the front elevation of the building either side of the central entrance door. The glazed openings contain doors so that the central entrance is retained for the residential uses above. In addition, pilasters to both sides of the shop fronts have been constructed in concrete and roughly finished.
9. I note that the works have ensured that the stone hood to the central entrance door has been retained and that the upper floors remain intact. However, the ground floor bay windows and a significant proportion of the plinth have been removed. Moreover, the stall risers below the ground floor bays have been removed completely. The loss of such significant architectural features has significantly diminished the historical merits of the building.
10. Whilst the glass shop fronts provide clean lines and uncluttered entrances to each unit, they pay little attention to the original character of the building and appear in stark contrast to the prevailing use of stone in both this building and the wider CA. Furthermore, the appeal property is a particularly prominent feature and the alterations are visible in several vantage points from both within and outside of the CA. The design of the alterations and the materials used fail to complement the host property and diminish the historic interest of the building. Consequently, the development is not of a high standard of design and has introduced a discordant feature into the CA.

11. I note that modern, glazed shop fronts have been fitted at neighbouring No 6 Market Street and the Bury District Co-Operative Society Emporium building. However, the CACA states that such alterations have greatly diminished their historic merits. Indeed, I agree with the Council that to allow replication of such features would be unduly harmful to the significance of the CA.
12. The appellant has indicated that vinyl films to replicate the frames of the original building could be added to the building through a suitably worded condition. However, the use of vinyl films would fail to replicate the sense of depth or the intricacy of the original masonry on the building and would not adequately mitigate the harm resulting from the alterations. Such a condition would not therefore be necessary.
13. I conclude, therefore, that the proposal would fail to preserve or enhance the character or appearance of the Bury Town Centre Conservation Area. As a consequence, it would conflict with Policies EN2/1 and EN2/2 of the Bury Unitary Development Plan 1997 which state that development will only be acceptable if it will preserve or enhance the character or appearance of the Borough's Conservation Areas. It would also conflict with paragraph 132 of the Framework which states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation, and Paragraph 138 of the Framework which states that harm to a building which makes a positive contribution to the significance of the Conservation Area should be treated either as substantial harm under paragraph 133 or less than substantial harm under paragraph 134.
14. For the reasons above I have found that the proposal would be harmful to the CA. That harm would be less than substantial and in accordance with paragraph 134 of the Framework, it should be weighed against any public benefits of the proposal.
15. The appellant indicates that the subdivision of the unit is essential to ensure the building remains in commercial use and I note that the external works provide separate accesses for the two units. I also note the original windows were not appropriate for retail use, giving little space for display areas.
16. However, whilst I afford such public benefits moderate weight, the harm identified to the significance of the heritage asset in this instance weighs more heavily than the public benefit. Thus, the proposal would not accord with the provisions of paragraph 134 of the Framework.

Other Matters

17. Planning permission was granted in 2016 at the appeal property for the change of use of first and second floors from offices (Class B1a) to 5 no self-contained apartments. (Council Ref: 59702) The appellant has made representations to the effect that the Council has already approved the development subject to this appeal under that planning permission.
18. Condition 2 of that permission requires the approved works to be carried out in accordance with a list of plans, one of which illustrates the ground floor subdivision and external alterations which are subject to this appeal. However, a condition requiring approved works to be carried out in accordance with a list of plans does not require all works on the plans to be carried out. Rather, it

requires that where such permitted works are carried out they are done so in accordance with the approved plans.

19. Furthermore, a planning permission should stand by itself and the meaning be clear within the four corners of the document. The description of the approved development did not make reference to the sub-division of the ground floor or the ground floor external alterations. Whilst it is not unusual for the description of the development to refer only to the major part of a development for the sake of simplicity, a planning permission cannot authorise development beyond which that is applied for or stated in the decision notice. Moreover, the application was explicit in referring to the change of use of the first and second floors of the building. Had the development included works at ground floor, it would have said so. In addition, the subdivision of the ground floor and the external works subject to this appeal are shown as existing features as well as proposed on the approved plans under the 2016 permission.
20. As a result, I consider it has not been demonstrated that a reasonable and lawful fall back position exists in this case. In any event, it is not for me, under a section 78 appeal, to determine whether or not that is the case. To that end it is open to the appellant to apply for a determination under sections 191/192 of the Act and my determination of this appeal under s78 does not affect the issuing of a determination under s191/192.
21. The appellant and the occupiers of the shop units indicate that the alterations required to return the building to its previous appearance would have a significantly negative impact upon the ongoing financial viability of the existing businesses. However, I have been provided with no substantive evidence which demonstrates how such changes would result in the businesses being unable to continue to trade from the building or indeed how it would have a financial impact on the businesses. I therefore afford such statements little weight.
22. I note that no objections have been received from local residents regarding the development and that the appellant purchased the property with the works already undertaken. However, such matters would not outweigh the failure of the proposal to preserve or enhance the character or appearance of the CA.
23. The appellant considers the proposal should be considered in the context of the presumption in favour of sustainable development. However, in this case, as the proposal conflicts with paragraph 134 of the Framework, a restrictive policy under Footnote 9 would apply and the presumption in favour of sustainable development under Paragraph 14 would therefore be dis-applied.

Conclusion

24. For the reasons given above I conclude that the appeal should be dismissed.

Jason Whitfield

INSPECTOR

REPORT FOR NOTING

Agenda Item 7



Agenda
Item

7

MEETING: PLANNING CONTROL COMMITTEE

DATE: 28th FEBRUARY 2017

SUBJECT: PLANNING OUTCOMES REPORT

REPORT FROM: HEAD OF DEVELOPMENT MANAGEMENT

CONTACT OFFICER: DAVID MARNÓ – DEVELOPMENT MANAGER

TYPE OF DECISION: NONE

FREEDOM OF INFORMATION/STATUS: This paper is within the public domain

SUMMARY: The report provides summary on the visits undertaken and analysis provided by Members on the outcomes tour undertaken on 5th August 2016.

OPTIONS & RECOMMENDED OPTION The Committee is recommended to note the report.

IMPLICATIONS:

Corporate Aims/Policy Framework: Do the proposals accord with the Policy Framework?
N/A

Financial Implications and Risk Considerations: N/A

Statement by Director of Finance and E-Government: N/A

Equality/Diversity implications: No

Considered by Monitoring Officer: N/A

Are there any legal implications? N/A

Staffing/ICT/Property: N/A

Wards Affected: ALL

Scrutiny Interest: N/A

TRACKING/PROCESS**DIRECTOR:**

Chief Executive/ Management Board	Executive Member/Chair	Ward Members	Partners
Scrutiny Commission	Executive	Committee	Council

1.0 BACKGROUND

- 1.1 This report presents a brief analysis of the views of the members of the Planning Control Committee who, as part of the on-going training programme, undertook an outcomes visit to a number of sites in the Borough where development had been implemented.
- 1.2 In all, eight sites were visited and each site was scored on the basis of perceived quality of the decision, implementation and an overall general assessment of the scheme.
- 1.3 The outcomes tour is an annual assessment programme and training initiative to enable both Members and Officers to visit sites upon completion and to provide a view upon the success of the development assessed against policy, the surrounding environment and context and to determine any lessons that could be learned in future proposals.
- 1.4 This year, the visits took place on 5th August 2016 and a total of 8 sites were visited. 10 Planning Control Councillors attended the tour this year together with a number of officers. A standardised questionnaire was devised to enable marking/scoring to take place and to enable strengths and weaknesses of individual developments to be identified.

2.0 SITES VISITED AND ANALYSIS

- 2.1 The sites inspected by Members were:
- Tulle Court, Ramsbottom Row, Prestwich
 - Land at 12-22 Warwick Street, Prestwich
 - Solita, Bury New Road, Prestwich
 - Land adjacent to Prestwich Post Office, Bury New Road
 - Former Gasworks, Victoria Street, Bury
 - Water Tower, Land Off Bradshaw Road, Affetside
 - Ramsbottom Cottage Hospital, Nuttall Lane
 - Holcombe Brook Sports Club

Each of the sites were considered on the basis of -

- Visual Amenity – Scale, mass, appearance and quality of finish
- Visual Amenity – design, appearance
- Relationship to neighbours
- Highways issues – access and parking
- Regeneration
- Overall assessment

1. Tulle Court, Ramsbottom Row, Prestwich

	1	2	3	4	5	N/A	Total Score
Visual Amenity/ Scale/mass		1	2	11	5		77/95
Visual Amenity/Design /appearance			5	9	5		76/95
Neighbours			3	11	4	1	73/90
Highway Access			3	10	6		79/95
Parking			3	10	6		79/95
Regeneration			3	7	7	1	72/90
Overall View			2	12	4		74/90
							530/650
							81.5%

Comments made include:

- Residents have told me they are very happy with the site a good example scheme.
- Scheme is an improvement on what was there before. Housing partner has worked well with the council.
- Ample Parking for residents and visitors.
- Inward looking so has helped create neighbourliness.
- A well thought out development which has improved the area.
- Successful scheme overall, devoid of planting.
- Good replacement of 1970's flats and good re-cladding.

Overall this was a well received scheme. The site had taken some considerable time to progress due to the economic slowdown from 2008. However, the site was acquired by a registered landlord and quickly progressed to completion in 2016.

The development of this site completed the final phase of regeneration in this area and provides a welcome development to this part of Prestwich.

2. Land at 12-22 Warwick Street, Prestwich

	1	2	3	4	5	N/A	Total Score
Visual Amenity/ Scale/mass		1	6	13			72/100
Visual Amenity/Design /appearance	1	1	5	13			70/100
Neighbours			12	8			68/100
Highway Access		3	6	4		6	40/65
Parking	1	1	5	2		11	26/45
Regeneration		2	4	9	1	1	57/80
Overall View		1	6	8			52/75
							385/565 68%

Comments made include:

- Lessons to learn about modern design e.g. there are two window styles that are set into wooden clad walls, one has weathered well but the other is letting in damp.
- Like the design but a little out of place in the area.
- Area a bit cramped and overbearing to neighbouring homes.
- A good development in the space available.
- Good stylish contemporary design & removed fire damaged building.
- No allocated parking.
- Looks like a cheap build with flimsy windows and stained cladding.

Mixed response to this scheme from the assessments. Contemporary development approach was won on appeal and therefore had no local intervention in designing to be more locally responsive to design. There appears to still be reluctance to cladding using cedar. However, it has performed better in some parts than in more exposed areas.

The site was always constrained and replaced a burnt down factory. However, the close relationship to the terraced properties at the rear appears to have worked well and is acknowledged that the development is a good one in the space available.

3. Solita, Bury New Road, Prestwich

	1	2	3	4	5	N/A	Total Score
Visual Amenity/ Scale/mass			3	13	4		81/100
Visual Amenity/Design /appearance			4	13	2		74/95
Neighbours			3	10	3	3	64/80
Highway Access		2	4	6	1	6	45/65
Parking	1	2	2	5	1	7	36/55
Regeneration			3	8	3	4	56/70
Overall View		1	2	10	3		63/80
							419/545 77%

Comments:

- Rejuvenated vacant building and is bringing next door also vacant into use.
- No parking provision all on street in a busy town centre location.
- Could do much better lets the row opposite down, does nothing to enhance the area aesthetically.
- Appears to have fit in well, has good trade and has boosted local economy.
- I'd like to see better parking arrangements for people coming into Prestwich in the evening.
- Bury New Road improvement plans will also help the future development of sites like this one.
- First floor windows could be more in keeping with those on church lane.
- A good reconstruction, has improved that corner of Prestwich.

A well received conversion and restoration scheme on the main road frontage within Prestwich Town Centre. The venue is popular and also expanding, reflecting high demand for the venue. The conversion is within the Conservation Area and therefore was required to be sensitive on how changes were proposed.

The building is a historic one and therefore changes needed to be sensitive with its own period, hence the differences with other buildings in the area. Overall, the views of this property's conversion are a welcome investment to the approach into Prestwich and the overall economy.

4. Land adjacent to Prestwich Post Office, Bury New Road

	1	2	3	4	5	N/A	Total Score *
Visual Amenity/ Scale/mass	1	1	5	9	5		79/105
Visual Amenity/Design /appearance			4	10	6		82/100
Neighbours			7	8	3	2	68/90
Highway Access	2	4	5	7	2		63/100
Parking			6	8	7		85/105
Regeneration			5	7	6	2	73/90
Overall View		2	5	12	1		72/100
							522/690 76%

Comments:

- Continued enforcement needed for traffic and transport.
- Access in hindsight could be improved.
- Despite the objections the development has worked well. The predicted traffic chaos has not occurred and complaints are merely token.
- Landscaping sparse and needs to mature.

Despite the local concern of this development being approved in terms of impact upon local highways and traffic movement, the development has assimilated into the main road approach into Prestwich appropriately and following some early issues of non-compliance, the scheme is now compliant with the planning permission and is a welcome addition to the town.

5. Former Gasworks, Victoria Street, Bury

	1	2	3	4	5	N/A	Total Score*
Visual Amenity/ Scale/mass			5	12	4		83/105
Visual Amenity/Design /appearance			7	11	3		80/105
Neighbours			1	3	4	13	35/40
Highway Access		1	1	10	8		83/100
Parking			2	9	9		87/100
Regeneration			6	8	5		75/95
Overall View		1	5	13	3		84/110
							527/655 80%

Comments:

- Implications for use of employment land in future.
- Excellent disabled car spaces.
- Well used and a smart use of a rundown corner.
- Very easy to access the site.
- Good access parking is no problem.
- Good scheme and positive regeneration. Disappointing employment uses not delivered.
- New junction layout works well.
- Refurbished a piece of scrub land.
- Excellent planting all around the site.
- A much improved area for gateway to town centre.
- Has improved the area for small retailers and businesses on Bolton Road.

This scheme has been well received and is considered to have been a good regeneration scheme on a site that was previously run down. There is a clear acknowledgement concerning the loss of the previous employment site and this is further compounded that the site has not delivered in all of its original promises. However, the value of the development in retail terms is acknowledged and is considered to have provided a positive benefit to the local environment.

6. Water Tower, Land Off Bradshaw Road, Affetside

	1	2	3	4	5	N/A	Total Score *
Visual Amenity/ Scale/mass			2	15	4		86/105
Visual Amenity/Design /appearance			2	15	4		86/105
Neighbours			1	6	7	7	62/70
Highway Access		9	8	4			58/105
Parking	3	5	9	3		1	52/100
Regeneration	1	1	8	7	1	2	60/90
Overall View		1	3	15	2	1	81/105
							485/680 71%

Comments:

- It looks good. Extension at back has no planning permission. Suspect they have never intended it to be a bunkhouse.
- Not enough visibility big gate is a problem.
- Extension at the rear doesn't look as in-keeping with the rest of the property.
- Extension to rear looks poor.
- Only real concern is the road access.
- No need for the big gate.
- Roadway passing precarious.
- Looks a reasonable scheme but thin end of wedge.
- Aesthetically pleasing, issues about development not stuck to original plans.
- Building not being used as originally planned.
- Unique and unusual

This development was acknowledged as an unusual one in that the development comprised the conversion of a former water tower. There were concerns in relation to the adjoining highway, being a rural road, which meant that access/egress/passing traffic needed to demonstrate particular care. Caution is flagged up so as not to become the 'thin end of the wedge' and become a use that was not originally part of the reasons for allowing the unusual scheme to be granted in the first place. Parking provision was an obvious issue on this scheme and the large gate (unauthorised) is considered to be unnecessary.

7. Ramsbottom Cottage Hospital, Nuttall Lane

	1	2	3	4	5	N/A	Total Score*
Visual Amenity/ Scale/mass			3	10	8		89/105
Visual Amenity/Design /appearance			1	10	9		88/100
Neighbours		1	6	5	9		85/105
Highway Access		2	5	8	6		81/105
Parking			7	8	5	1	78/100
Regeneration		1	1	12	6	1	83/100
Overall View			3	12	6		87/105
							591/720 82%

Comments:

- The streetlights look very cheap in relation to the quality of the development.
- A good quality development which has brought an old site back into use.
- Arguably over developed. Not bad enough to refuse but needs finishing off.
- Road unfinished, no real pavements, no real parking areas for visitors etc...
- Feels a little cramped in towards the bottom of the site.
- Really attractive range of styles, appropriate materials and colours.
- No pavements. Concerning as these appear to be aimed at families.
- Needs dropped curbs and disabled pavements.
- Good to see stonework from original facade retained on site.
- Dereliction cleared. Historic elements retained. More landscape needed.
- Would like better streetlights perhaps coloured black to go with railings at entrance of development.

The scheme had not quite been finished at the time of the visit with road surfacing requiring completion. However, the overall layout and relationship of the site to the surrounding property was considered to be successfully achieved. The heritage issue with the site has provided an unusual marker insofar as the building date stone being placed on the site's entrance and was praised. There were concerns about 'shared highway' spaces but overall this did not heavily go against the development as a whole.

8. Holcombe Brook Sports Club

	1	2	3	4	5	N/A	Total Score*
Visual Amenity/ Scale/mass	1	11	6	2	1		66/105
Visual Amenity/Design /appearance	3	13	4				41/100
Neighbours		1	4	6	4	5	58/75
Highway Access		3	5	10	3		76/105
Parking		1	6	6	7		79/100
Regeneration	3	3	6	3		6	45/75
Overall View	2	6	12	2			58/110
							423/670 63%

Comments:

- Done on the cheap, disappointing design and landscaping could have been much better with more thought and money.
- Good community impact seems evident.
- Roof has failed to meet 'green' definitions.
- Very poor landscaping an opportunity seems to have been missed.
- Disappointing, great community facility unfortunately design is poor.
- The finished materials outside including car park appear to have been done quite cheaply. The car park looks unfinished.
- Roof looks tacked on mix of materials looks old.
- Building has been cheapened a reduction in quality.
- Roof does not fit with surroundings a seeded roof would have blended beautifully and reflected hills.
- Pavement to road rough and unfinished.
- It seems good very useful for community space.
- A poor building visually an eyesore. Only saving grace is the solar panels, however it is not what was promised.
- Building looks out of place cladding needs painting or something.
- Bad design not obvious what the use is, wood cladding looks shabby.
- Chances to deliver something special lost.
- Design appears compromised, elements that made the scheme good have disappeared.
- Roof different to original approved plans and no planting.
- Crushed stone car park looks awful but adequate parking.
- The roof is an eyesore in this area.
- The problem is the materials don't lend themselves to painting so in its present state it's poor.
- A great facility for bury residents although design is very lazy could have done better.

This scheme originally started as a particularly green development in sustainability terms through softer/increased levels of planting. Through time and in its development, certain elements were replaced. Whilst still sustainable in approach, visually, the adoption of solar panels and elevational changes resulted in a strong view that the development has not delivered. The standard of finish, design, landscaping and appearance all came in for some levels of criticism and the opportunity to deliver something special was lost. Overall, the scheme was not considered to be as successful as it could have been.

Overall assessment Scores		
7	Ramsbottom Cottage Hospital, Ramsbottom	82%
1	Tulle Court, Prestwich	81.5%
5	Former Gas Works Victoria Street (ALDI etc), Bury	80%
3	Solita Bury New Road, Prestwich	77%
4	Land adjacent to Prestwich Post Office (KFC), Prestwich	76%
6	Water Tower Bradshaw Road, Walshaw	71%
2	Land at Warwick Street, Prestwich	68%
8	Holcombe Brook Sports Club, Holcombe Brook	63%

3.0 CONCLUSION

- 3.1 The outcomes tour provides an insight to schemes that have been considered by Members, how they have been carried out and their integration into the surrounding context in which they are located.
- 3.2 The scoring of the sites visited this year demonstrates that development is of a very good standard, shows successful implementation and integration. The site of least success was still considered to have not delivered the original intentions of the scheme and cost cutting/financial constraints have had an impact. 5 of the schemes were in the upper quartile in terms of standard and success and both of the highest scoring schemes had important issues around heritage to consider, which may have contributed to the overall quality of finish.
- 3.3 Overall the valuable lessons learnt from the exercise are that the issues assessed by officers and duly considered in the respective reports demonstrate that the planning process is working well and that feedback from this exercise continues to guide how future proposals are considered.

List of Background Papers: - The respective planning applications

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